



Technical Reports

Interstate Highway (IH) 635 LBJ East Ultimate Project

CSJ: 2374-01-137, 2374-01-180, 2374-01-183,
2374-02-053, & 2374-02-144

From United States Highway (US) 75
To IH 30

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT

Project Description

Introduction

Re-Evaluation consultation is being conducted for the proposed reconstruction and improvement to an 11.2-mile section of IH 635 in Dallas County, Texas. The proposed project extends along IH 635 through portions of the cities of Dallas, Garland, and Mesquite. The original IH 635 Environmental Assessment (EA) received environmental clearance through a Federal Highway Administration (FHWA) Finding of No Significant Impact (FONSI) issued on January 30, 2003.

A project-level carbon monoxide (CO) traffic air quality analysis (TAQA) was previously performed in the 2003 EA-FONSI. Since the approval of the 2003 EA-FONSI, updates to emissions rates, new traffic projections and schematic design changes require a new CO TAQA. The purpose of this CO TAQA technical report is to satisfy the current procedural requirements of the U.S. Environmental Protection Agency (EPA) regarding the assessment of project-level impacts on ambient levels of CO. The CO TAQA discussed in this document follows the September 2015 TxDOT Standard Operating Procedure for Complying with a CO TAQA Requirements (hereinafter 'CO TAQA SOP'), and the Documentation Standard for a CO TAQA Technical Report. The estimated time of completion (ETC) year of the project is 2022 and the design year of project is 2042.

Existing Facility

Within the project limits, the existing IH 635 facility is an eight lane highway with one managed high occupancy vehicle (HOV) lane in each direction, various auxiliary lanes, and discontinuous frontage roads. The existing right-of-way (ROW) width ranges from 330 to 892 feet.

Also within the project limits, the existing IH 30 facility is a six lane highway with two reversible managed lanes and discontinuous frontage roads. The existing ROW width ranges from 300 to 1,028 feet.

Proposed Design

The proposed IH 635 improvements from east of US 75 to Miller Road would include constructing five 12-foot-wide general-purpose lanes (one additional) in each direction, two 12-foot-wide tolled-managed lanes in each direction, and two to three frontage road lanes in each direction. From near Royal Lane/Miller Road to La Prada Drive, the proposed project would provide five general-purpose lanes (one additional) in each direction, two non-tolled express lanes in each direction, and two to three frontage road lanes in each direction. From La Prada Drive to south of IH 30, the project would reconstruct IH 635 to provide lane balance transitions between the general purpose lanes, proposed express lanes, and IH 30 interchange. Auxiliary lanes would be provided as needed to accommodate traffic demand volumes associated with ramp movements. The general purpose lanes would include 10-foot-wide outside and inside shoulders. The tolled managed/express lanes would include 10-foot-wide outside shoulders and 4-foot wide inside shoulders. The proposed project would also include the construction of general purpose lane ramps, tolled managed/express lane ramps, and direct-connecting ramps between IH 30 and IH 635. The proposed frontage roads along IH 635 would typically consist of 11-foot wide inside lane(s) and a 14-foot-wide outside shared use lane in each direction. Inside curb offsets would vary from 1 to 2-feet wide. Outside curb offsets adjacent to shared use lanes would be 2-feet wide. Continuous sidewalks would be provided along the proposed frontage roads.

The proposed IH 30 improvements (from west of Gus Thomasson Road to east of Galloway Avenue) would include constructing three to four 12-foot-wide general purpose lanes. Auxiliary lanes would be constructed as needed to accommodate traffic demand volumes associated with ramp movements. The general purpose lanes would include 10-foot-wide outside and inside shoulders. Two to three continuous frontage road lanes would be constructed in each direction, which would typically consist of 11-foot wide inside lane(s) and a 14-foot wide outside shared use lane in each direction. Inside curb offsets would vary from 1 to 2-feet wide and outside curb offsets adjacent to shared use lanes would be 2-feet wide. Continuous sidewalks would be provided along the proposed frontage roads.

The proposed project includes the construction of multiple noise walls located along the project corridor, where reasonable and feasible. The project would require approximately

16.3 acres of proposed ROW as well as 0.5 acres of temporary construction easements and 9.1 acres of drainage easements. Proposed ROW acquisition would be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

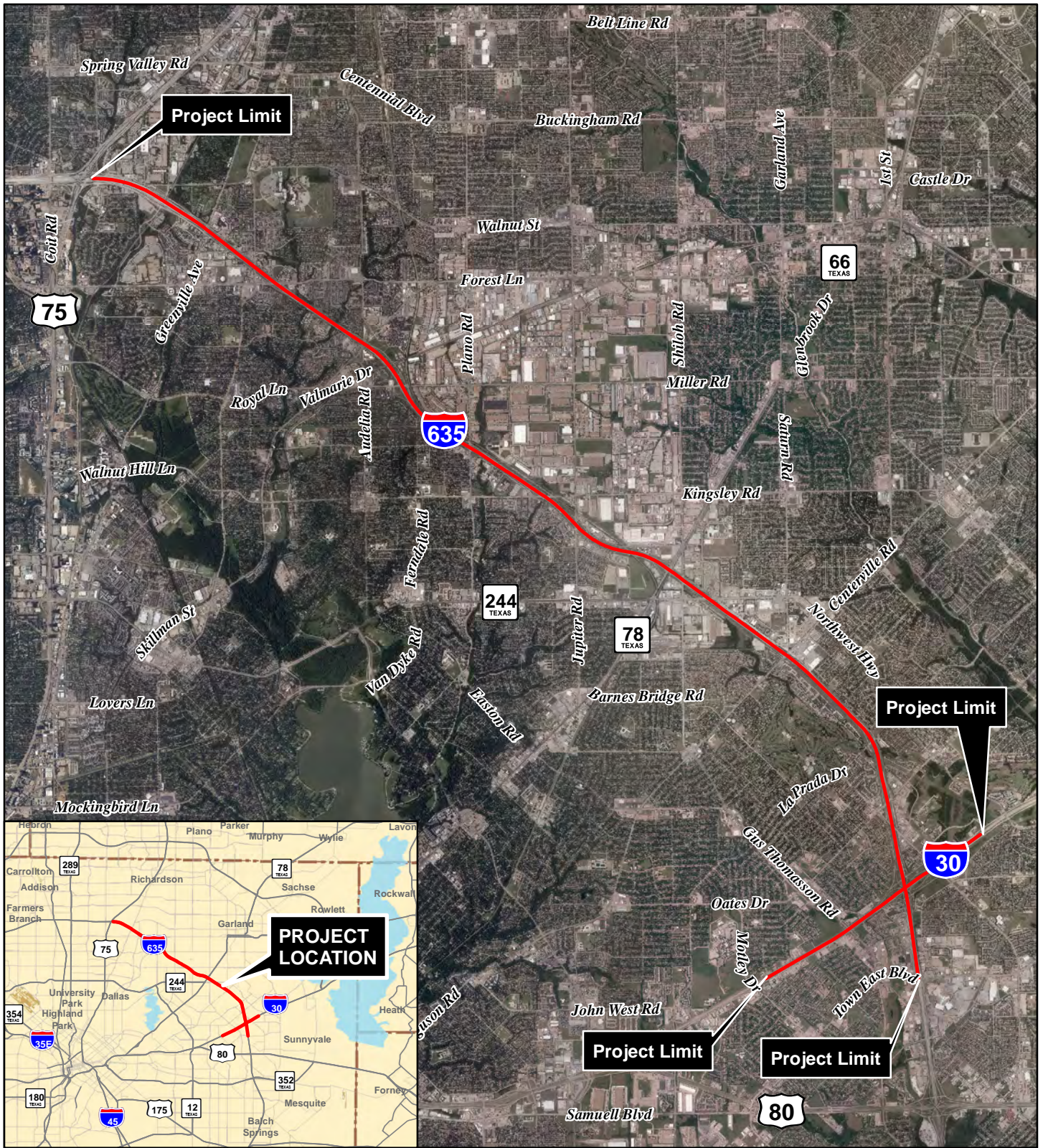


Table of Contents

- 1: ARCHEOLOGICAL BACKGROUND STUDY**
- 2: PROJECT COORDINATION REQUEST FOR HISTORICAL STUDIES PROJECT**
- 3: REPORT FOR HISTORICAL STUDIES SURVEY**
- 4: QUANTITATIVE MOBILE SOURCE AIR TOXICS ANALYSIS TECHNICAL REPORT**
- 5: CARBON MONOXIDE TRAFFIC AIR QUALITY ANALYSIS TECHNICAL REPORT**
- 6: WATER RESOURCES TECHNICAL REPORT**
- 7: BIOLOGICAL EVALUATION FORM**
- 8: HAZARDOUS MATERIALS INITIAL SITE ASSESSMENT REPORT**
- 9: COMMUNITY IMPACTS ASSESSMENT REPORT**
- 10: TRAFFIC NOISE IMPACT ANALYSIS**
- 11: INDIRECT AND CUMULATIVE IMPACTS ANALYSIS**

Archaeological Background Study



Archeological Background Study

Interstate Highway (IH) 635 LBJ East Ultimate Project

From United States Highway (US) 75

TO IH 30

CSJ: 2374-01-137, 2374-01-180, 2374-01-183,
2374-02-053, & 2374-02-144

Cities of Dallas, Garland, and Mesquite; Dallas County, Texas

November 2016

Leonard Voellinger, RPA

Introduction

Re-Evaluation consultation is being conducted for the proposed reconstruction and improvement to an 11.2-mile section of IH 635 in Dallas County, Texas. The proposed project extends along IH 635 through portions of the cities of Dallas, Garland, and Mesquite (see attached **Project Location on Aerial Photograph Map** and **USGS Topographic Map**). The original IH 635 Environmental Assessment (EA) received environmental clearance through a Federal Highway Administration (FHWA) Finding of No Significant Impact (FONSI) issued on January 30, 2003. The purpose of this archeological background study is to update the previous coordination that occurred in 2001.

This archeological background study is intended to identify known archeological sites, as well as the physiographic conditions that could contain intact archeological materials. The area of potential effect (APE) for archeological sites in this study is the existing and proposed IH 635 right of way (ROW) and easements, between the project termini. The study area, which was researched for previously-recorded sites, includes a 1-kilometer-wide buffer around the APE (see the attached **Archeological Resources and Study Area Map**). The vertical APE is generally four feet for utilities and maximum of 30 feet for bridges, as described below. The project footprint has changed since the previous archeological coordination from 2002. The attached **Archeological Resources Coordination Map** shows the project footprint that was approved in the 2003 IH 635 EA-FONSI and previously coordinated for archeological resources as well as the areas of new ROW and easements that were not previously coordinated.

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

Project Description

The proposed project would reconstruct IH 635 to provide five general-purpose lanes (one additional general purpose lane in each direction), two tolled-managed lanes, and two to three frontage road lanes in each direction from US 75 to near Royal Lane/Miller Road. From near Royal Lane/Miller Road to La Prada Drive, the proposed project would reconstruct IH 635 to provide five general-purpose lanes (one additional general purpose lane in each direction), two non-tolled express lanes, and two to three frontage road lanes in each direction. From La Prada Drive to south of IH 30, the project would reconstruct IH 635 to provide lane balance transitions between the general purpose lanes, proposed express lanes, and IH 30 interchange. Additionally, two to three frontage road lanes would be constructed in each direction from La Prada to Town East Boulevard.

Improvements to IH 30 would include reconstructing the main lanes to provide at least three lanes in each direction through the IH 635 interchange with allowances provided for potential future managed/express lanes. Auxiliary lanes would also be constructed for the IH 635 interchange. The IH 30 improvements would include two to three lane frontage roads in each direction (see the attached **Project Plan View Map** and **Project Typical Sections**). The schematic files, including the project plan and profiles, can be viewed in ECOS under the Archeology tab.

The proposed project includes the construction of multiple noise walls located along the project corridor, where reasonable and feasible. The project would require approximately 16.3 acres of proposed ROW as well as 0.5 acres of temporary construction easements and 9.1 acres of drainage easements. Proposed ROW acquisition would be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

Utility relocation needs are being evaluated and will be coordinated with the respective utility owners. All of these proposed improvements would take place within or adjacent to the existing ROW. The maximum depth for utilities would be approximately four feet and subsurface impacts at the underpasses would be approximately 30 feet deep.

Physiography, Geography, and Soils

The proposed APE is located on the western edge of the Blackland Prairie ecoregion. According to Texas Parks and Wildlife, pre-settlement conditions of this region were that of a true prairie grassland community dominated by a diverse assortment of perennial and annual grasses and forbs. Early settlers described the Blackland Prairie as a vast endless sea of grasses and wildflowers with sparsely scattered trees or mottes of oaks on uplands. Forested or wooded areas were restricted to bottomlands along major rivers and streams, ravines, protected areas, or on certain soil types.

The topography of the project area is generally flat, while the adjacent terrain exhibits some gently rolling topography. Elevations range from about 500 to 600 feet above mean sea level. IH 635 between US 75 and IH 30 crosses five waterways including: Cottonwood Creek, Floyd Branch, Jackson Branch, Dixon Branch and Long Branch as well as multiple associated Unnamed Tributaries to these major streams. Within the project limits, IH 30 crosses South Mesquite Creek.

According to the USGS Geologic Database of Texas, the Project APE traverses upper Cretaceous-age sediments classified as Austin Chalk (Kau) and Lower Taylor Marl or Ozon (Ko) geologic formations. Austin Chalk, present in the southeastern portion of the project is mostly composed of micro-angular calcite interbedded with calcareous clay. The Ozon formation is composed of calcareous clay, silt and sand. It is blocky and may exhibit concoidal fracture and contain pyrite nodules and marine mega-fossils.

A review of the Natural Resource Conservation Service's Web Soil Survey indicates that the soils in the Project APE are from five series: Austin, Dalco, Ferris, Heiden and Houston Black. Austin soils are well-drained clayey soils. Dalco soils are moderately deep, moderately well-drained clayey soils in shallow valleys; Ferris soils are deep, well-drained, sloping and strongly sloping clayey soils on uplands; Heiden soils are well-drained, clayey soils on uplands; and Houston Black are deep, moderately well-drained clayey soils on uplands. These clay soils are residual and exhibit blocky or sub-angular blocky structure, have high shrink-swell capacities and tend to move vertically through large cracks during dry spells.

Archeological Background

Archaeologists from Jacobs Engineering, Inc. conducted a background research online through the Texas Historical Commission's (THC) Archeological Sites Atlas. That research was done to identify previously-recorded sites on or eligible for listing in the National Register of Historic Places (NRHP), or designated as State Antiquities Landmarks (SALs) within the immediate vicinity of the project's APE. This research revealed a total of one previously-recorded site, 41DL45, near but outside of the one-kilometer search area for the proposed IH 635 improvements; however, there is no record of the site other than its location. Site 41DL45 is located along White Rock Creek, close to the IH 635 ROW west of the project's western terminus. This site can be viewed on the attached **Archeological Resources and Study Area Map**.

Several archeological surveys have been conducted in the project vicinity. In 1998 Dallas Area Rapid Transit (DART) conducted a survey between Skillman Avenue and the Missouri-Kansas-Texas Railroad. In 1996, an archeological survey was conducted for DART along the Missouri-Kansas-Texas Railroad (Green and James 1996). No archeological sites were identified during that survey. Also in 1996, an archeological survey was conducted for DART's North Central Corridor along the Southern Pacific Railroad (Green et. al 1996). That survey recorded two historic sites. These two sites are not within the current project's study area.

There have also been several surveys by TxDOT or its preceding organization, the Texas Department of Highways and Public Transportation. These include a survey along Skillman Avenue 1980, an undated survey along IH 635 at Greenville Avenue, and a survey along IH 635 at US 75 in 1991. No sites were recorded during any of the surveys (THC Atlas 2016).

The current environmental document is a Re-Evaluation of the IH 635 EA that received a FONSI in January 2003. The EA was completed in 2002; however, there is no record in the THC's Archeological Sites Atlas that an archeological survey of the proposed ROW was conducted at that time. Moreover, the 2002 EA states that, "An archeological impact evaluation was conducted" and "as a result of previous disturbance, it is unlikely that the

area of potential effects contains archeological sites with sufficient integrity to be eligible for inclusion in the NRHP” (TxDOT 2002).

Cemeteries

Restland Memorial Park Cemetery is the only cemetery located in the project vicinity (see page 1 of the attached **Archeological Resources and Study Area Map**). The cemetery is located along Greenville Avenue northeast of the IH 635/US75 interchange. Restland Memorial is a private cemetery on 375 acres and was founded in 1925. According to the cemetery’s website, it has over 125,000 interments. Restland Memorial Park is within the project’s study area, but outside of the APE.

Conclusions and Recommendations

Background research conducted online through the THC’s Archeological Sites Atlas identified no sites listed on or determined eligible for listing on the NRHP or designated as SALs. One known site, 41DL45, is located along White Rock Creek at IH 635 just east of the project study area. There is no site form on record so information on the site is limited to its location. Site 41DL45 borders the IH 635 ROW northwest of US 75. It is outside of the project APE and study area.

Previous cultural resources consultation which included an archeological impact evaluation concluded that intact archeological deposits are not likely to exist within the proposed ROW because of previous disturbance (TxDOT 2002). Most of the current APE exhibits residual soils with similar disturbance offering little potential to contain buried archeological material. Unrecorded sites, however, could be present within the APE in areas with colluvial sediments at the base of slopes or along minor drainages and creeks. Sites located in residual soils are not likely to have retained the aspects of integrity (location, design, setting, materials, workmanship, feeling, and association) whereas buried sites preserved under sediment layers could have integrity. Buried archeological deposits could be present in colluvial sediments, if such sediments are present.

In light of the findings, this researcher recommends conducting archeological survey of the APE at the stream crossings where additional ROW/easements would be acquired with shovel testing in suspected colluvial and any unmapped alluvial deposits that could be present. These stream crossings are presented in the attached **Archeological Resources and Study Area Map** and listed below:

- Cottonwood Creek – North terminus of IH 635 APE
- Floyd Branch – Westbound and eastbound sides of IH 635
- Jackson Branch – Westbound and eastbound sides of IH 635
- Dixon Branch – Westbound side of IH 635
- Long Branch Creek (at Northwest Highway) - Westbound and eastbound sides of IH 635

- Long Branch Creek (at La Prada Drive) - Westbound and eastbound sides of IH 635
- South Mesquite Creek - Westbound and eastbound sides of IH 30

Pursuant to Stipulation VI of the Programmatic Agreement and 43 TAC 2.24(f)(1)(C) of the MOU, TxDOT finds that the proposed undertaking could affect yet unknown archeological historic properties on or eligible for listing in the NRHP (36 CFR 800.16(l)) or as SALs. Further investigations are warranted to minimize the risk that unanticipated archeological deposits are encountered during construction.

References Cited

Green, Melissa, Frances M. James and Steven M. Hunt
Archeological Survey of the North Central Corridor, Dallas Area Rapid Transit Light Rail System, Dallas and Collin Counties, Texas. Miscellaneous Report of Investigations Number 135. 1997.

Green, Melissa and Frances James
Archeological Survey of the Northeast Corridor, Dallas Area Rapid Transit Light Rail System, Dallas County, Texas. Miscellaneous Report of Investigations Number 137. Geo-Marine. 1996.

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<http://www.restlandfuneralhome.com/who-we-are>. Accessed May 16, 2016./

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Geologic Database of Texas. <https://txpub.usgs.gov/DSS/texasgeology/>, Accessed October 26, 2016.

ATTACHMENTS

PROJECT LOCATION ON AERIAL PHOTOGRAPH MAP

USGS TOPOGRAPHIC MAP

ARCHEOLOGICAL RESOURCES COORDINATION MAP (11 PAGES)

PROJECT PLAN VIEW MAP (10 PAGES)

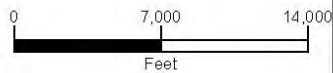
PROJECT TYPICAL SECTIONS (8 PAGES)

ARCHEOLOGICAL RESOURCES AND STUDY AREA MAP (3 PAGES)



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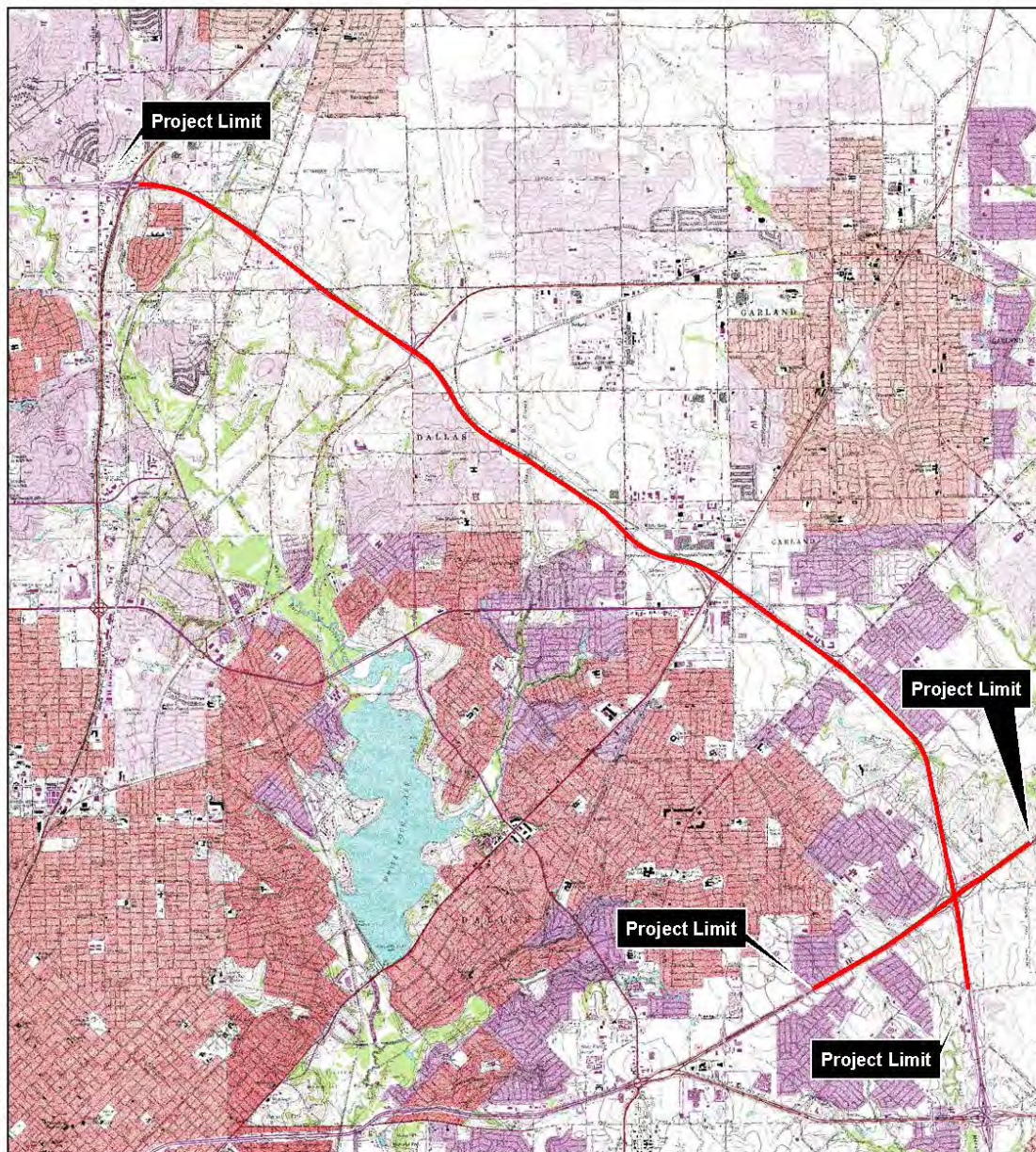
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Source/Year of Aerial Photograph: NCTCOG/2015

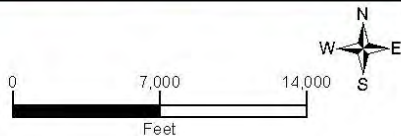
Project Location Map on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
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Legend

— Project Limits



USGS Topographic Map:
 Addison Quadrangle, made in 1959, photorevised in 1981;
 Garland Quadrangle, made in 1959, photorevised in 1967 and 1973;
 Mesquite Quadrangle, made in 1959, photorevised in 1968 and 1973;
 White Rock Lake Quadrangle, made in 1958, photorevised in 1968 and 1973.

USGS Topographic Map

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
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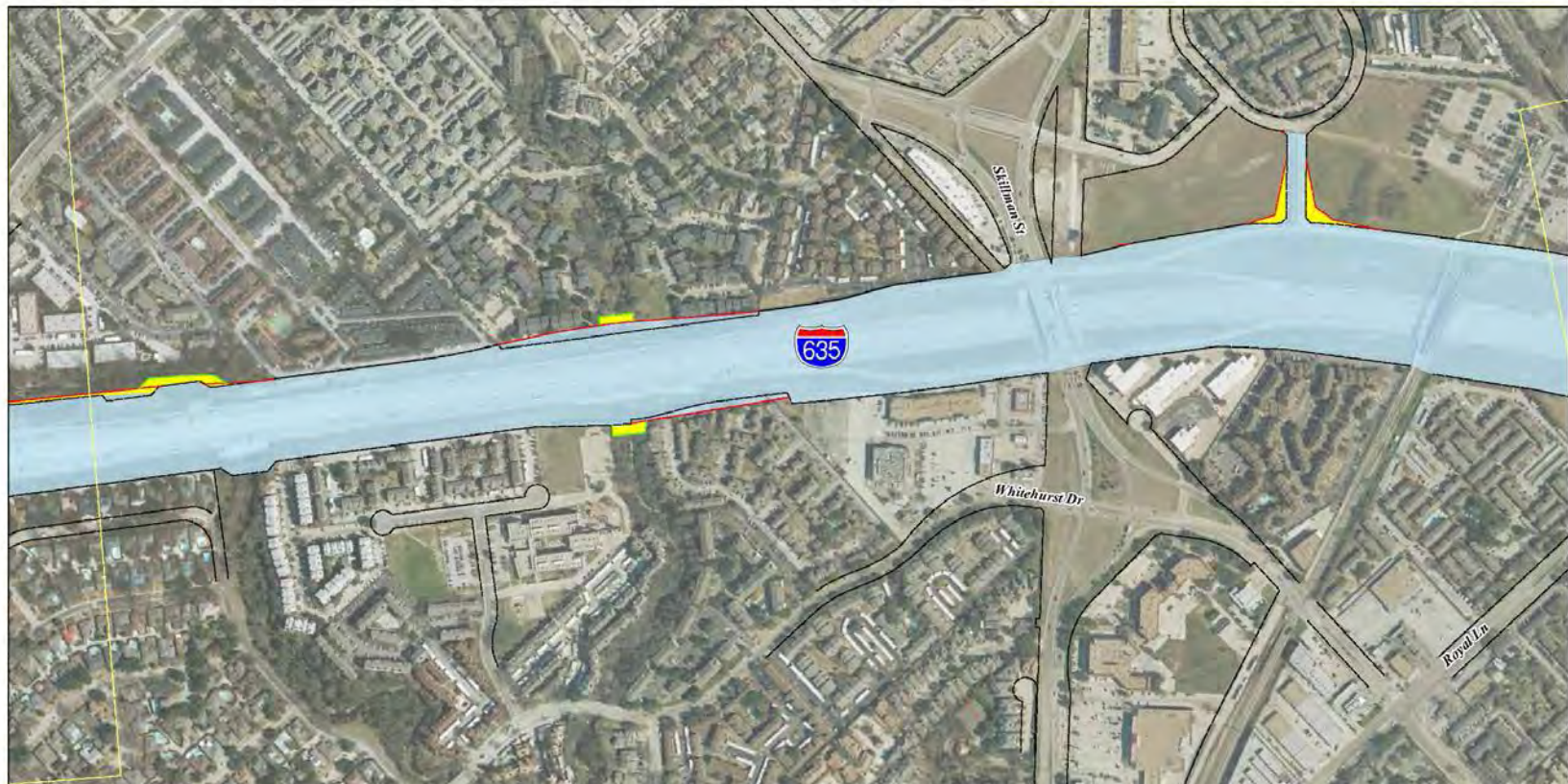
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Archeological Resources
Coordination Map
Page 1 of 11

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183,
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Source / Year of Aerial Photograph: NCTCOG / 2015



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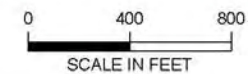


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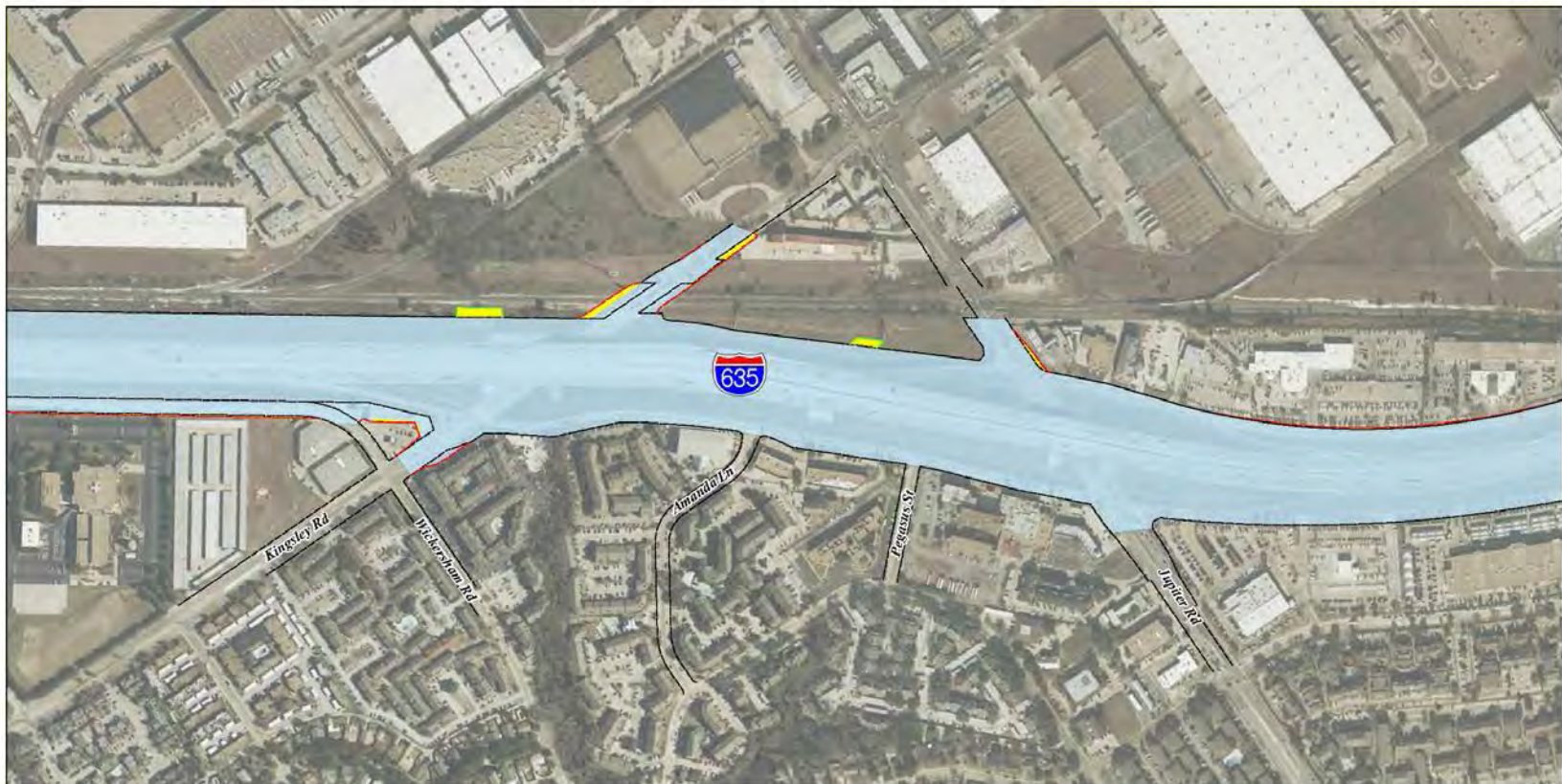
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Archeological Resources Coordination Map Page 3 of 11

IH 635 from US 75 to IH 30
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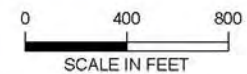
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Archeological Resources

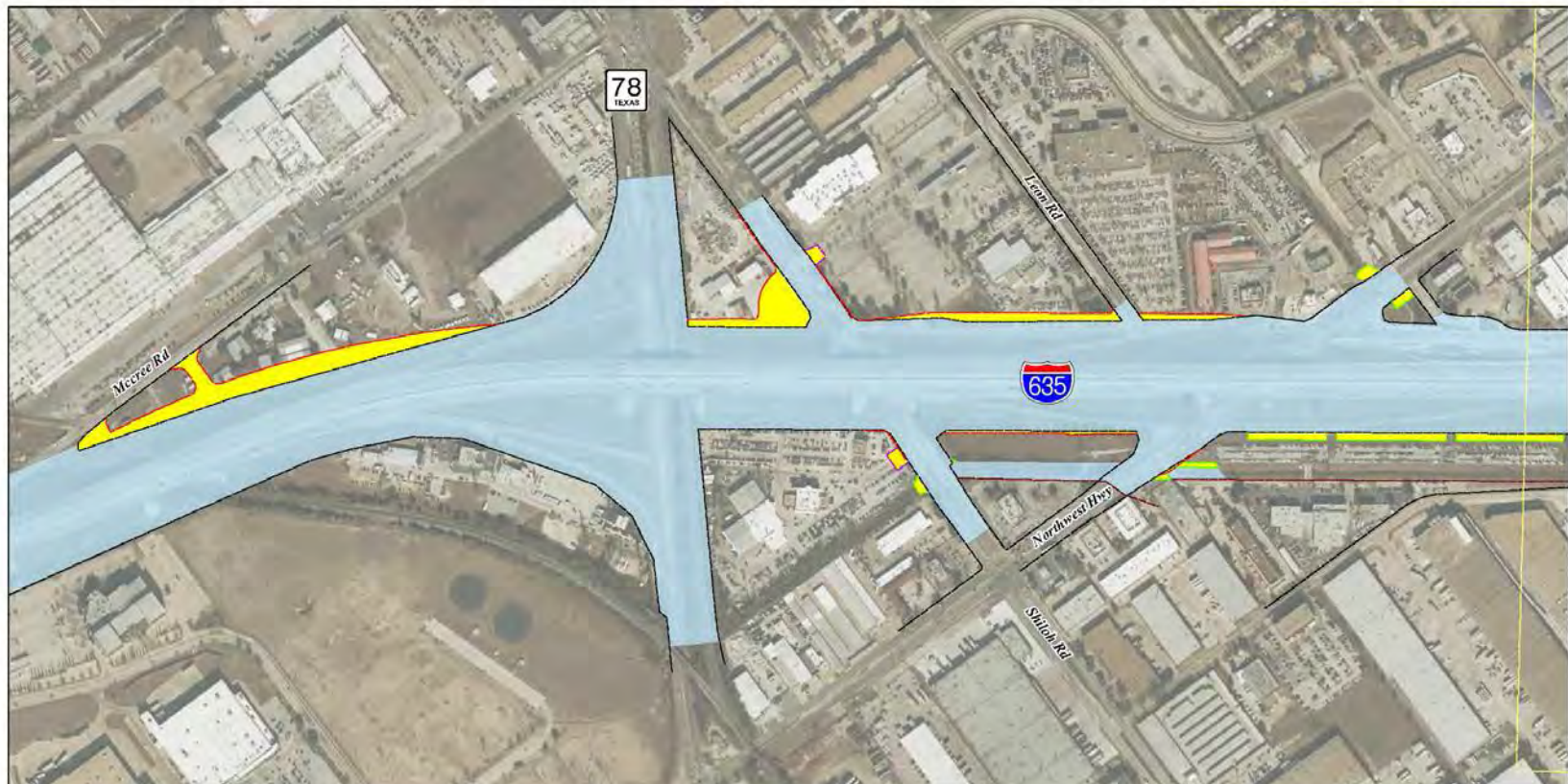
Coordination Map

Page 4 of 11

IH 635 from US 75 to IH 30
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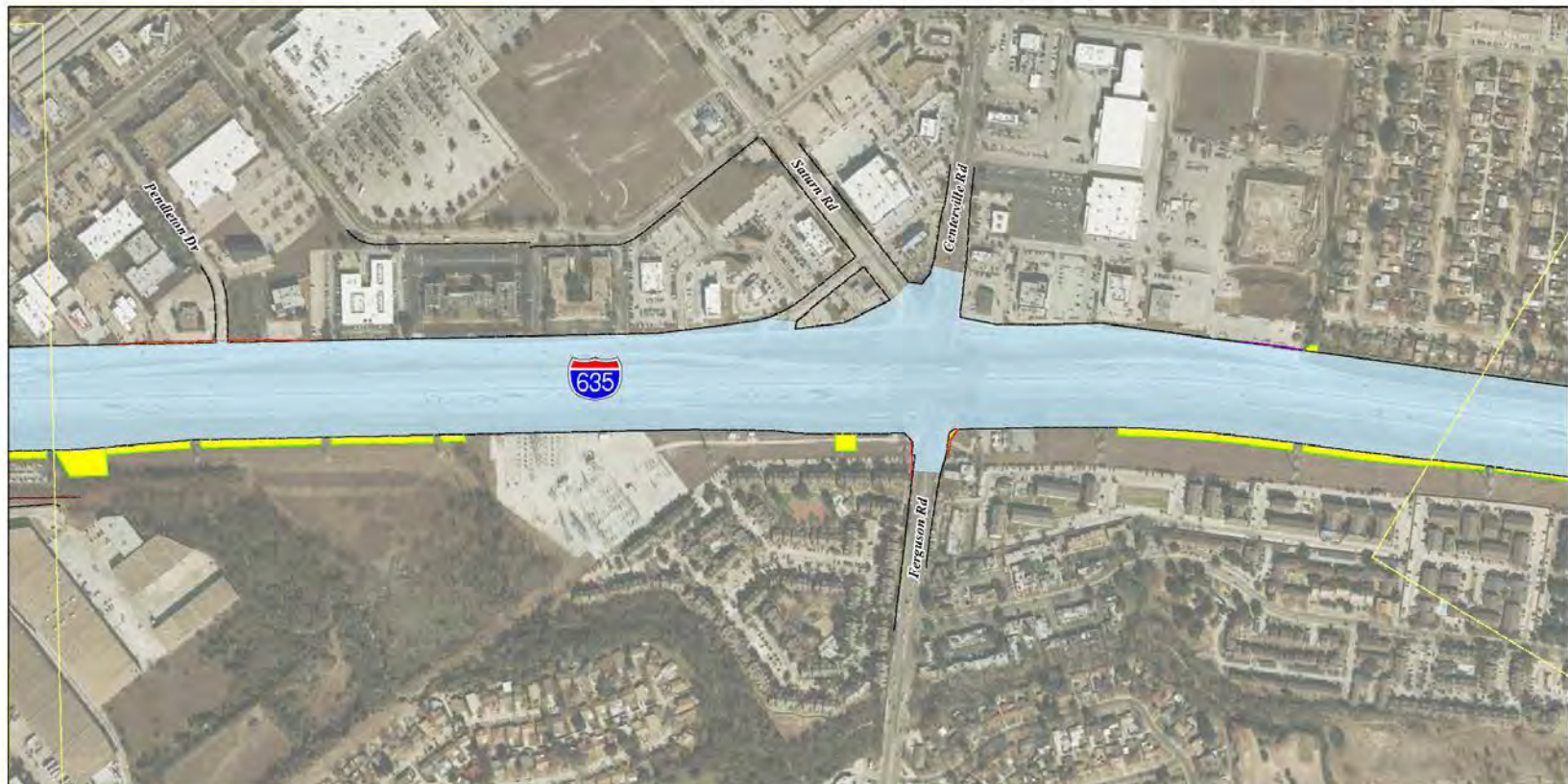
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Archeological Resources Coordination Map Page 5 of 11

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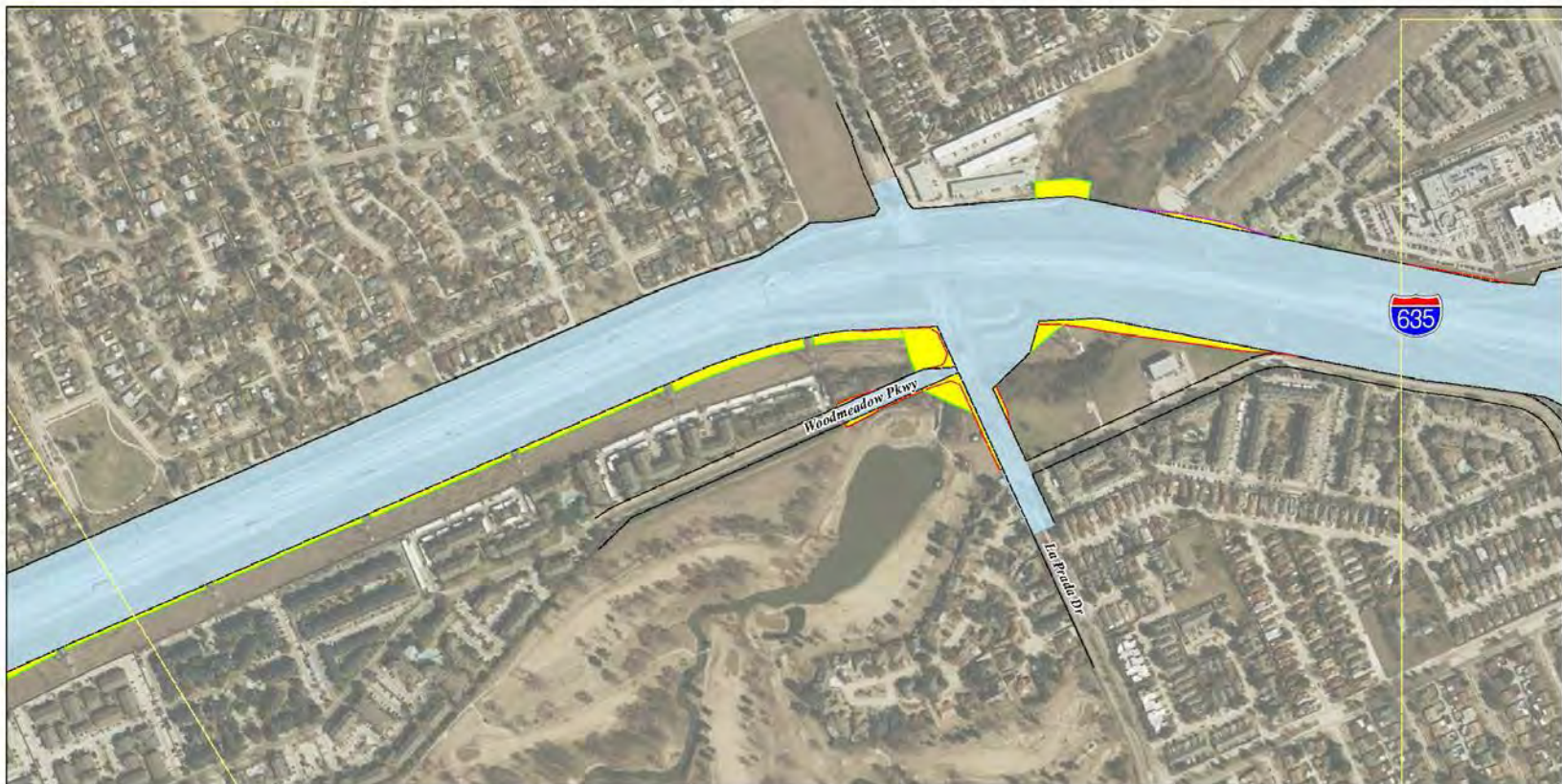
Archeological Resources Coordination Map Page 6 of 11

IH 635 from US 75 to IH 30
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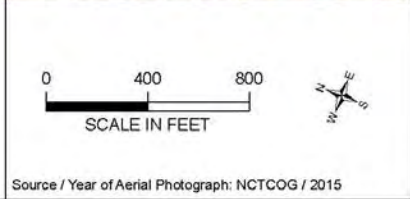
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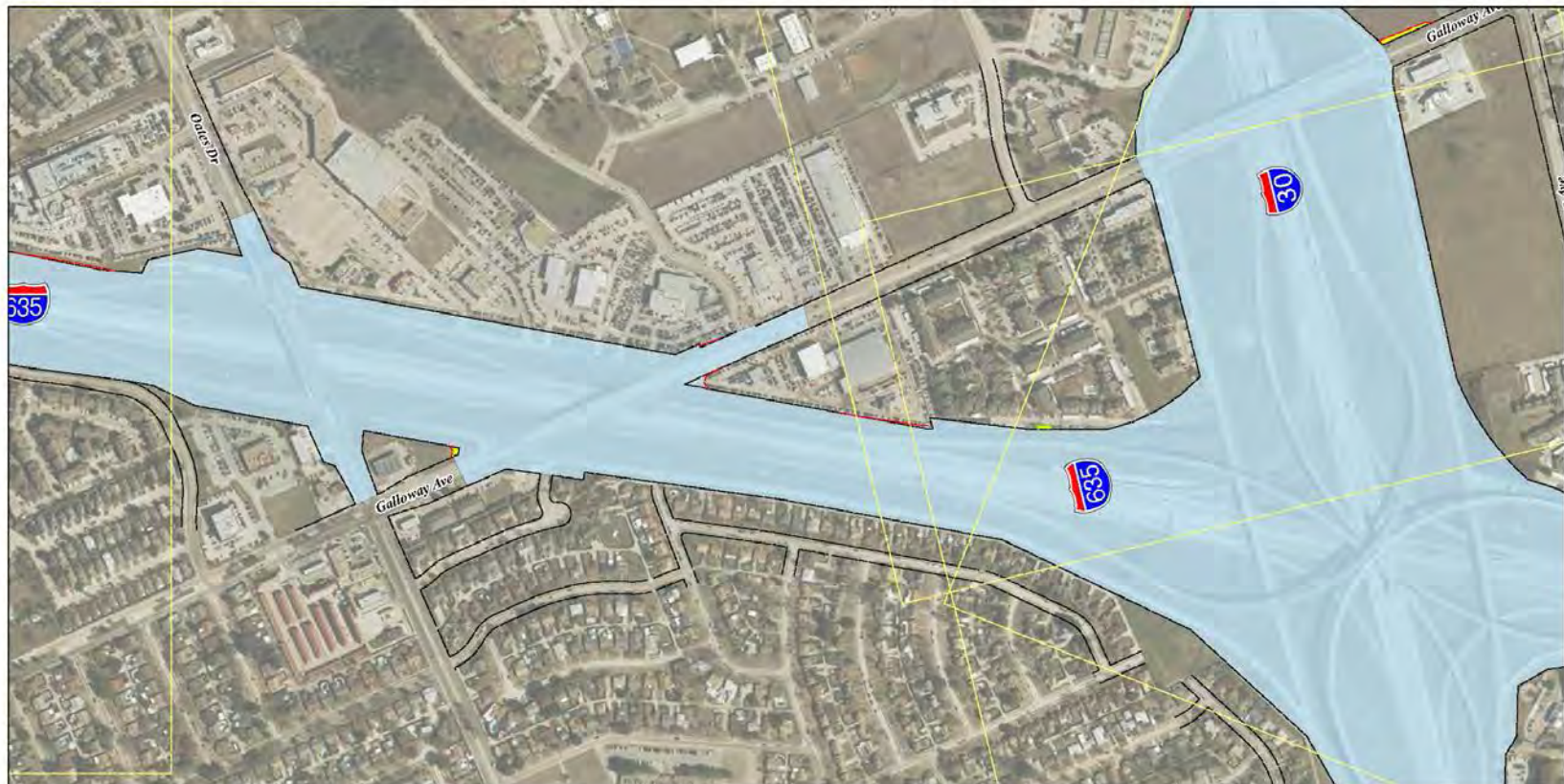


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**Archeological Resources
Coordination Map
Page 7 of 11**

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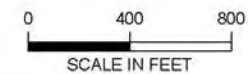


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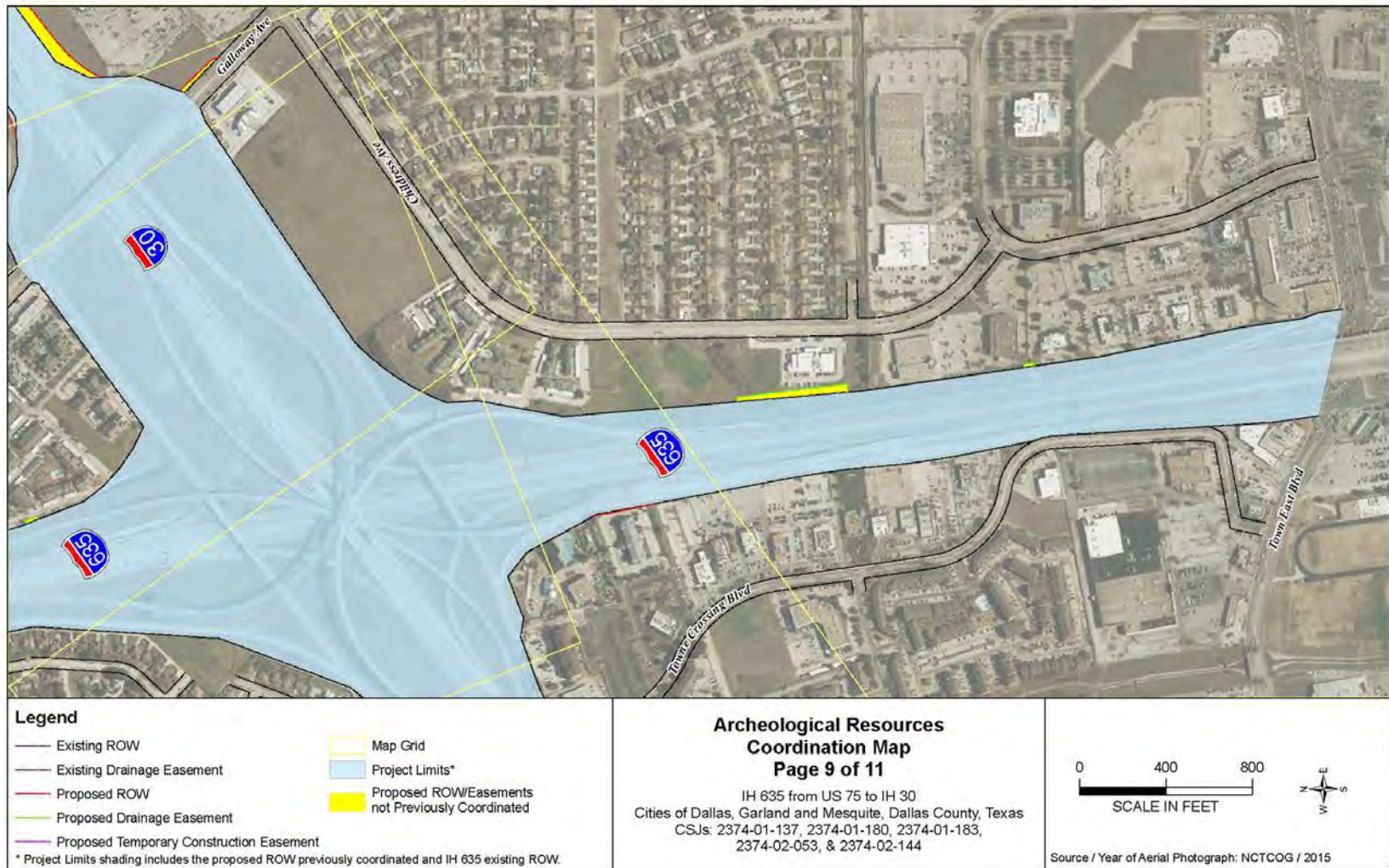
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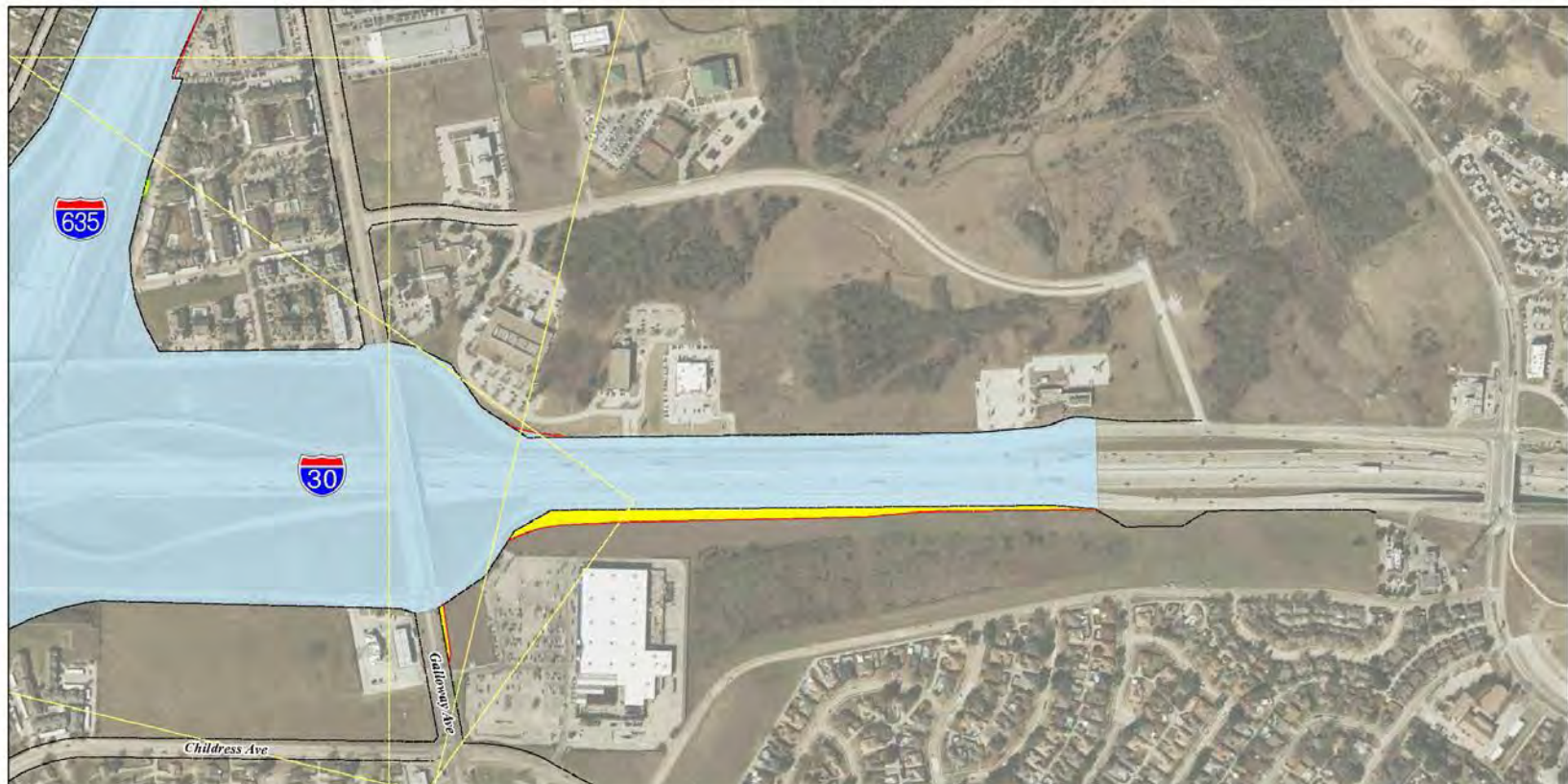
Archeological Resources Coordination Map Page 8 of 11

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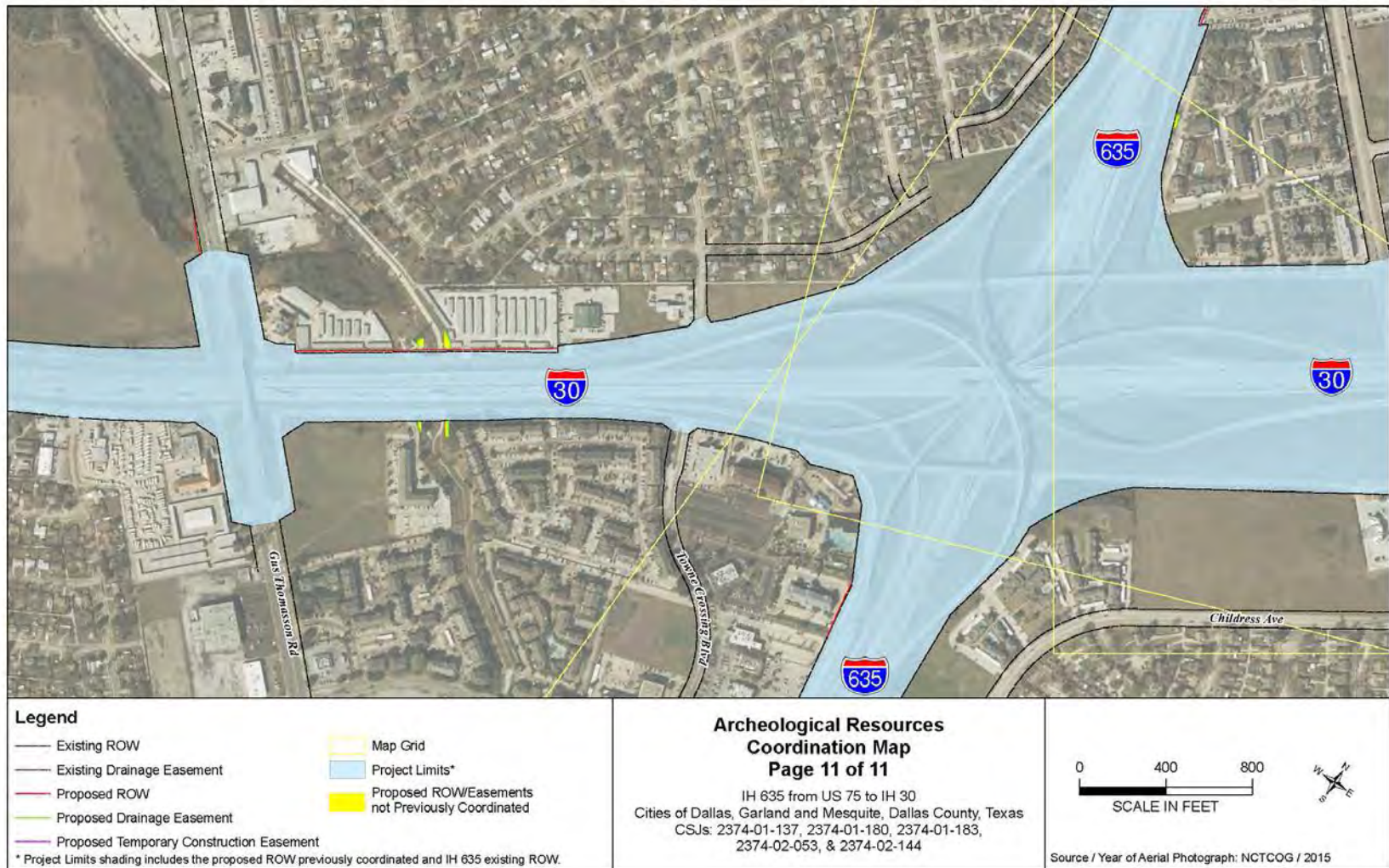
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Archeological Resources
Coordination Map
Page 10 of 11

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 SCALE IN FEET

Source / Year of Aerial Photograph: NCTCOG / 2015



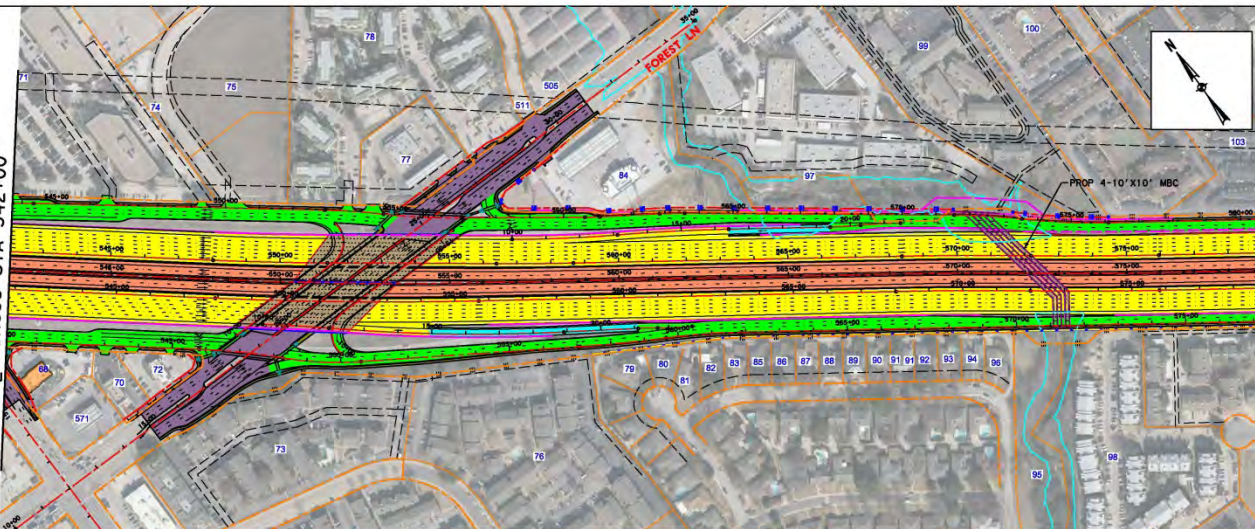
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DATE:

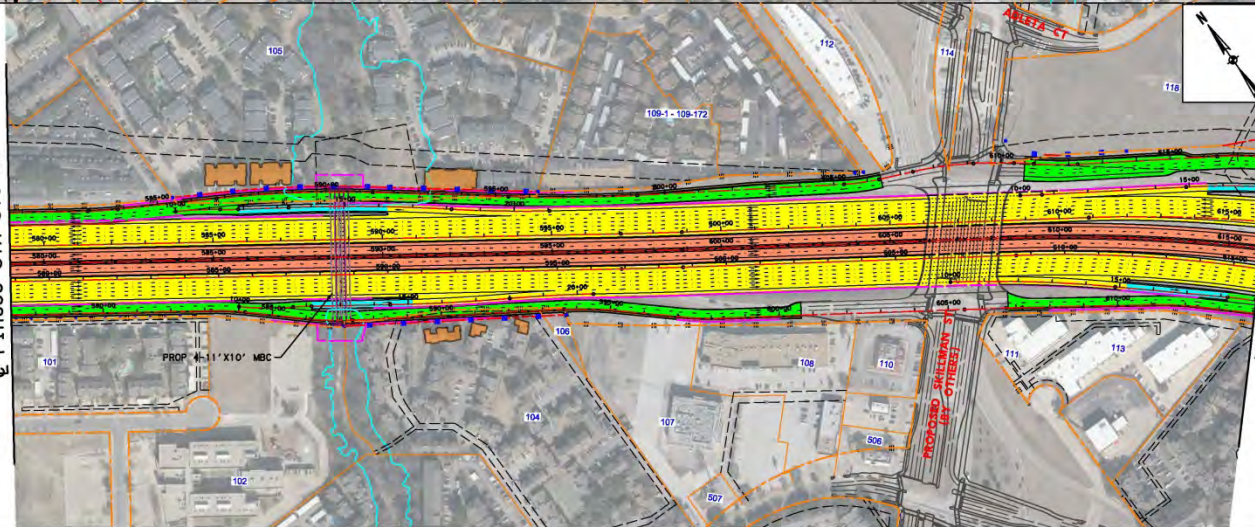
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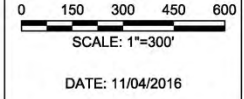
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C PIH635 STA 542+00



MATCH LINE SHEET 2
C PIH635 STA 579+00



MATCH LINE SHEET 3
C PIH635 STA 616+00



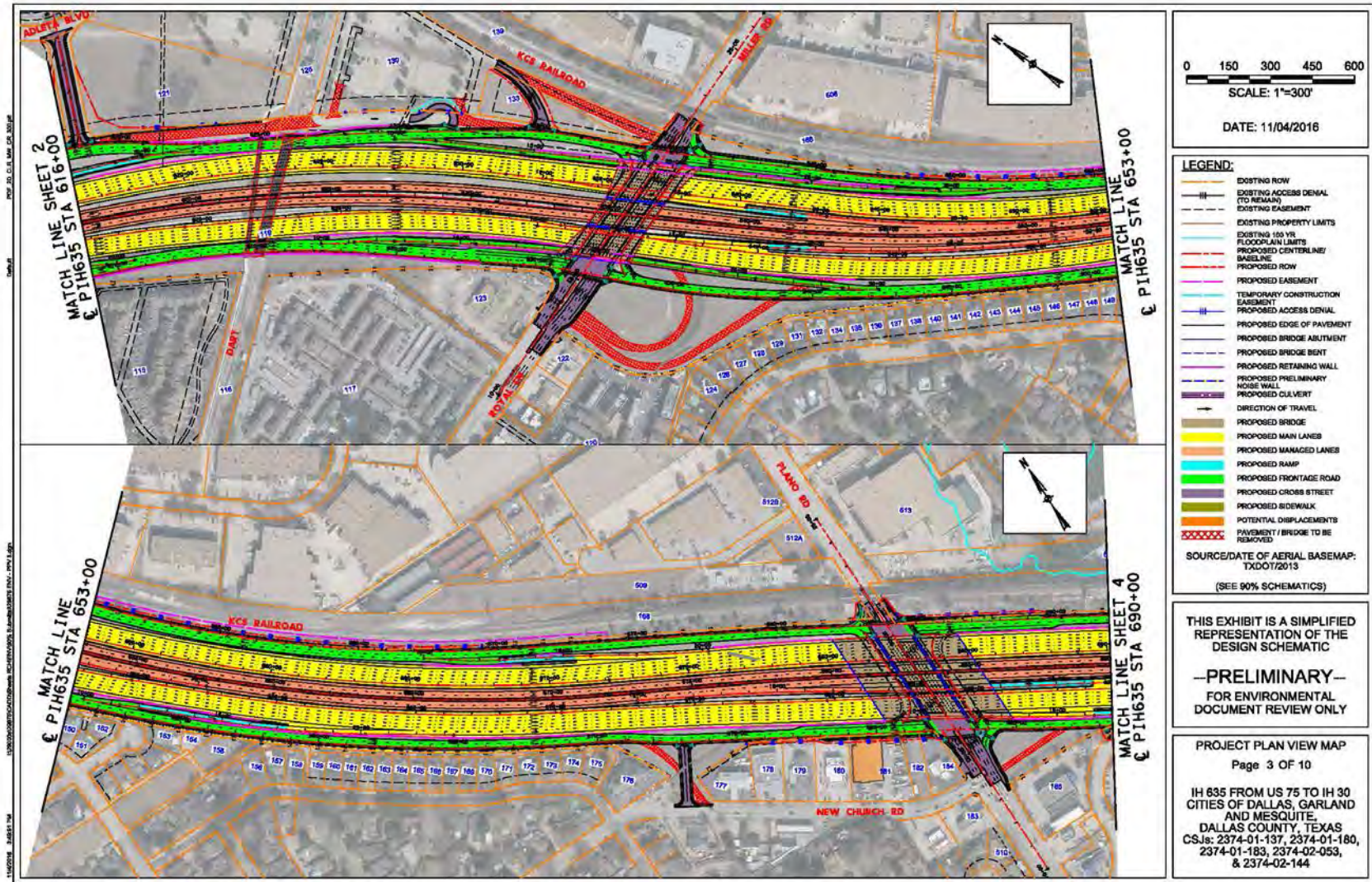
- LEGEND:**
- EXISTING ROW
 - EXISTING ACCESS DENIAL (TO REMAIN)
 - EXISTING EASEMENT
 - EXISTING PROPERTY LIMITS
 - EXISTING 100 YR FLOODPLAIN LIMITS
 - PROPOSED CENTERLINE/BASELINE
 - PROPOSED ROW
 - PROPOSED EASEMENT
 - TEMPORARY CONSTRUCTION EASEMENT
 - PROPOSED ACCESS DENIAL
 - PROPOSED EDGE OF PAVEMENT
 - PROPOSED BRIDGE ABUTMENT
 - PROPOSED BRIDGE BENT
 - PROPOSED RETAINING WALL
 - PROPOSED PRELIMINARY NOISE WALL
 - PROPOSED CULVERT
 - DIRECTION OF TRAVEL
 - PROPOSED BRIDGE
 - PROPOSED MAIN LANES
 - PROPOSED MANAGED LANES
 - PROPOSED RAMP
 - PROPOSED FRONTAGE ROAD
 - PROPOSED CROSS STREET
 - PROPOSED SIDEWALK
 - POTENTIAL DISPLACEMENTS
 - PAVEMENT / BRIDGE TO BE REMOVED
- SOURCE/DATE OF AERIAL BASEMAP:
TXDOT/2013
- (SEE 90% SCHEMATICS)

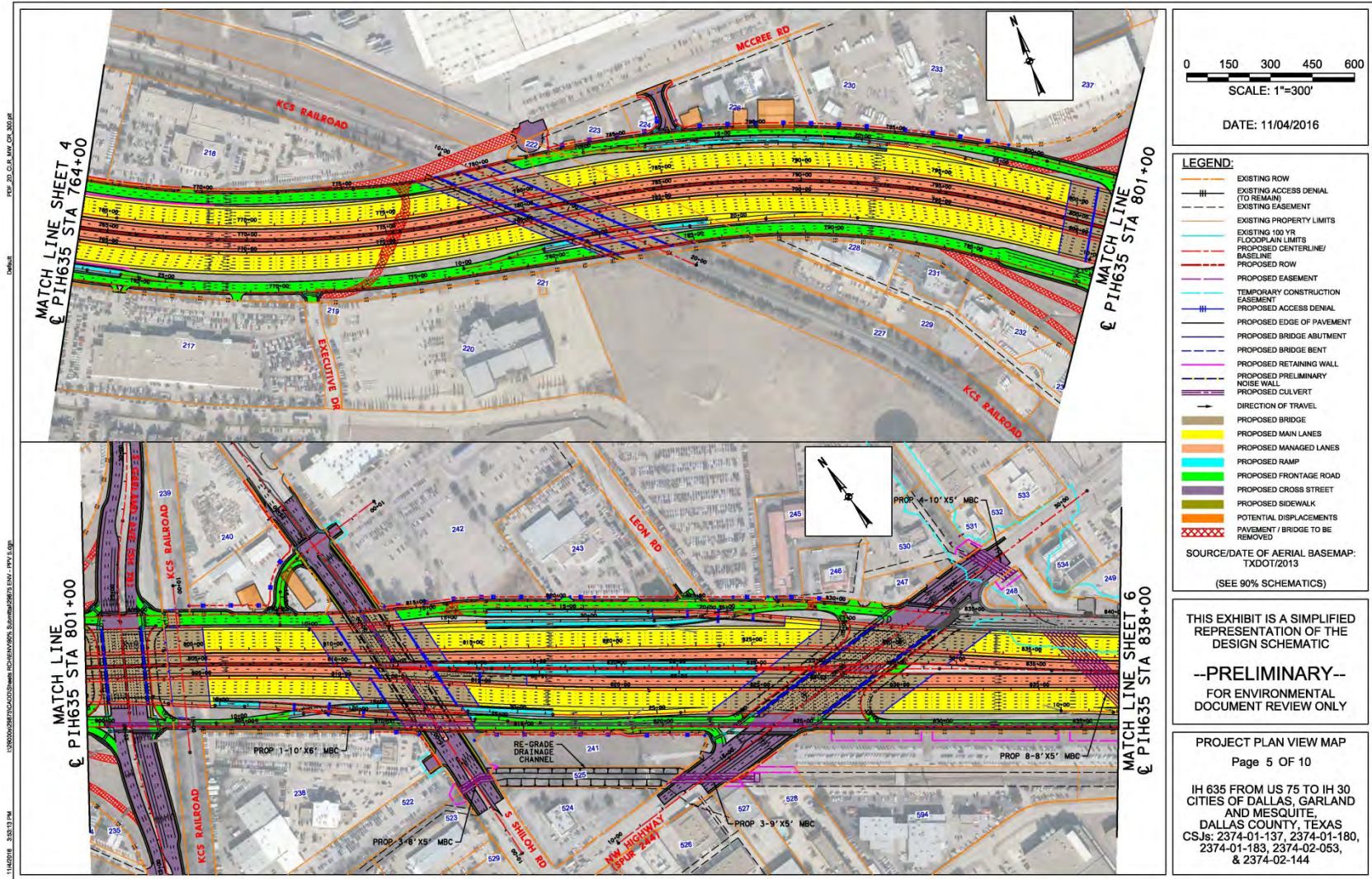
THIS EXHIBIT IS A SIMPLIFIED
REPRESENTATION OF THE
DESIGN SCHEMATIC

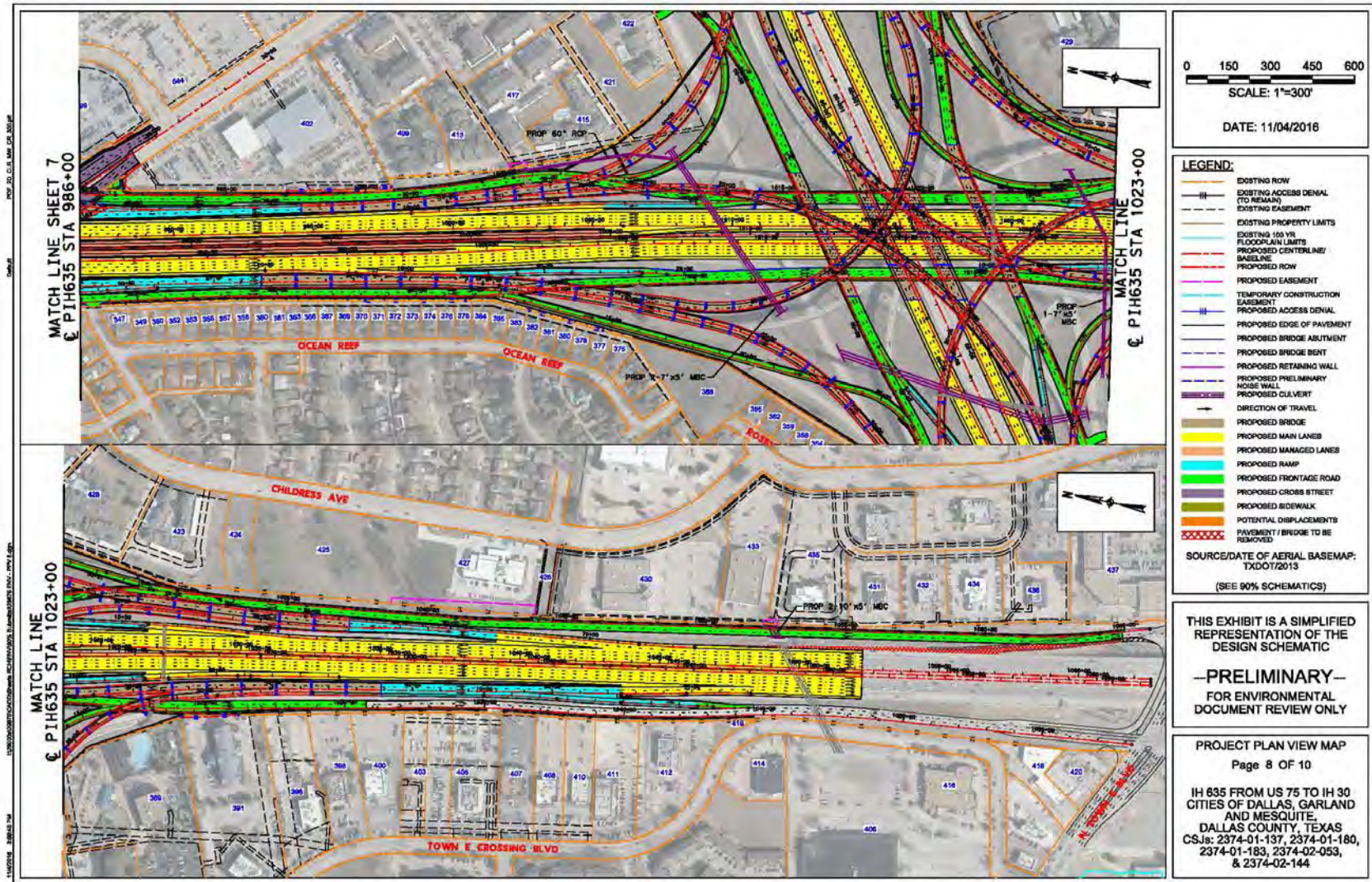
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FOR ENVIRONMENTAL
DOCUMENT REVIEW ONLY

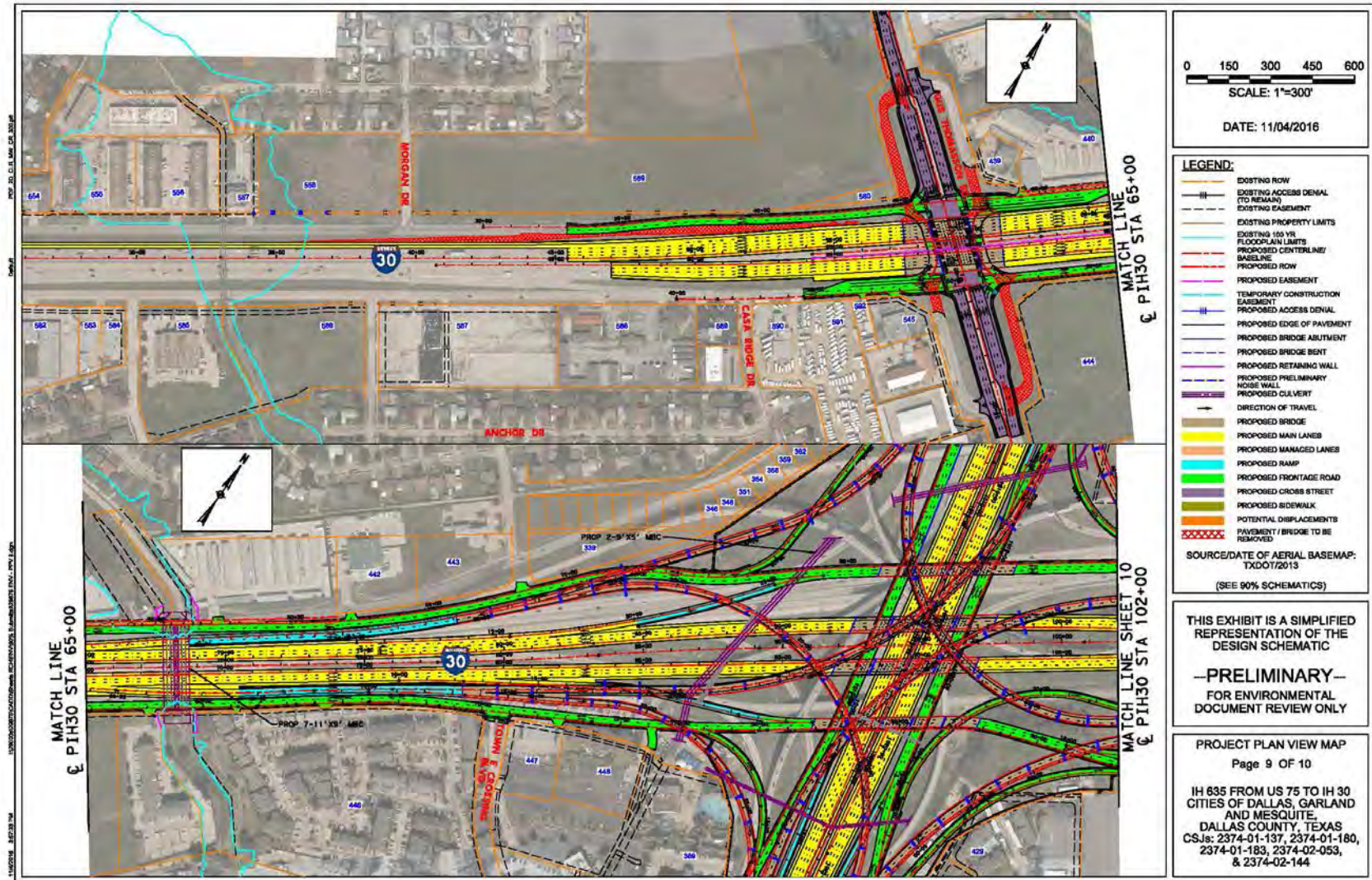
PROJECT PLAN VIEW MAP
Page 2 OF 10

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144







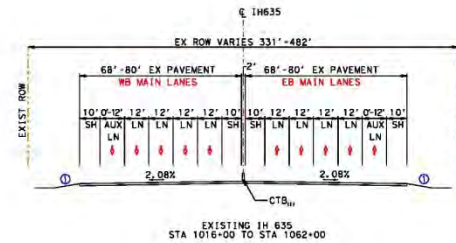
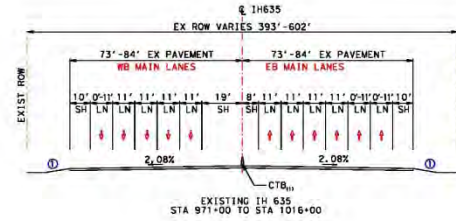
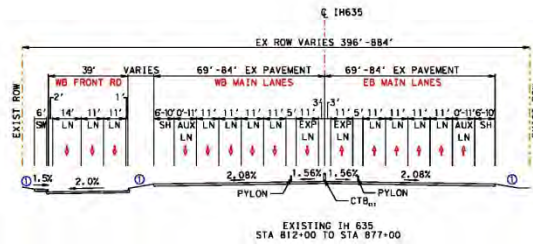
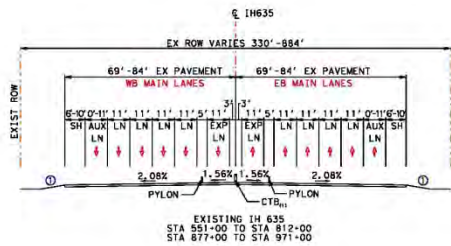
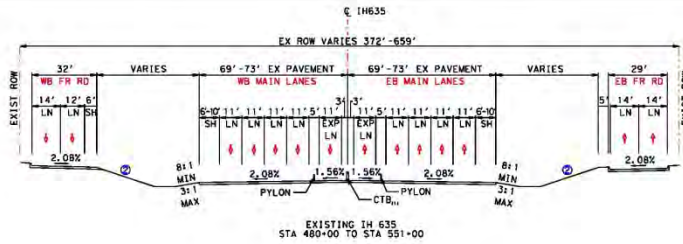


TOP OF CURB MAX 10.0%

10.0%

10.0%

10.0%



- NOTES:
- ① 6:1 DESIRABLE
 - ② 4:1 MAX
 - ③ 3:1 MAX
 - ④ 4" DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

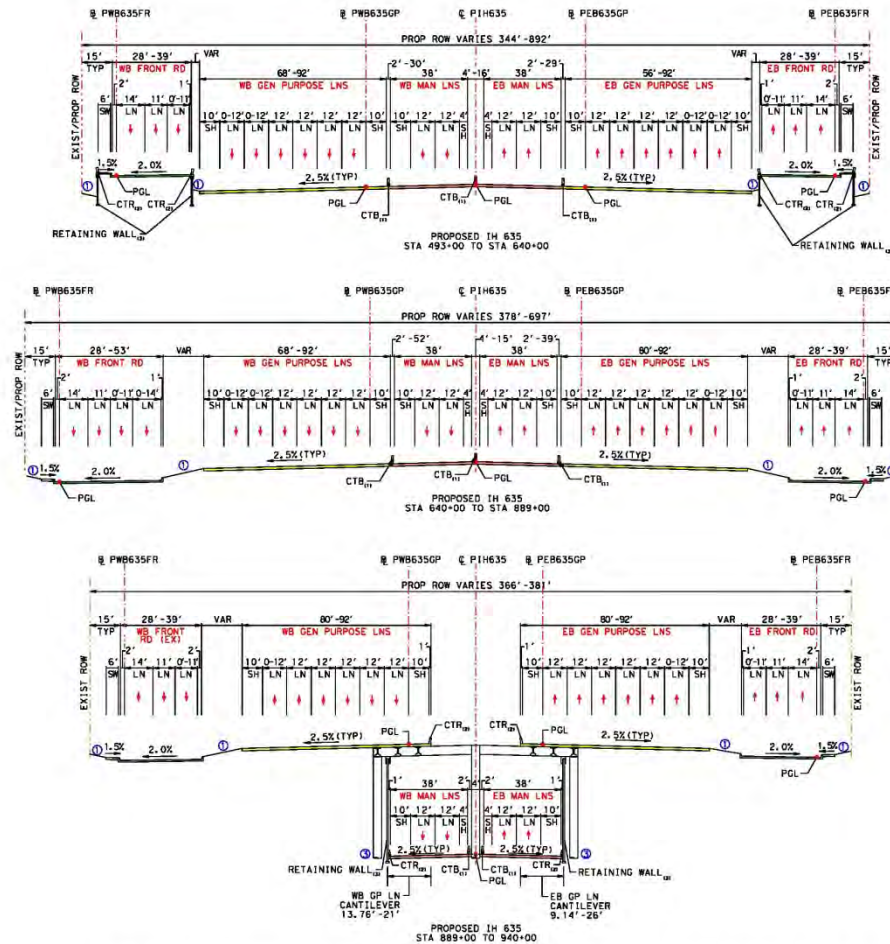
THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

--PRELIMINARY--
FOR ENVIRONMENTAL DOCUMENT REVIEW ONLY

PROJECT TYPICAL SECTIONS
PAGE 1 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144

EXISTING IH 635 TYPICAL SECTIONS



- NOTES:
- ① 6:1 DESIRABLE
4:1 MAX
 - ② 4:1 DESIRABLE
3:1 MAX
 - ③ 4' DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL
LIMITS SHOWN IN TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED
REPRESENTATION OF THE
DESIGN SCHEMATIC

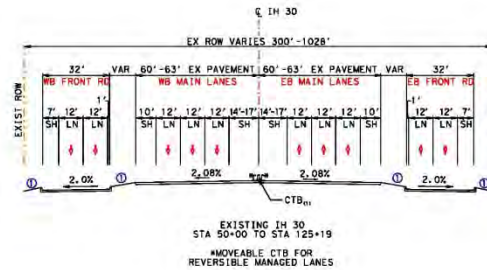
--PRELIMINARY--
FOR ENVIRONMENTAL
DOCUMENT REVIEW ONLY

PROJECT TYPICAL SECTIONS

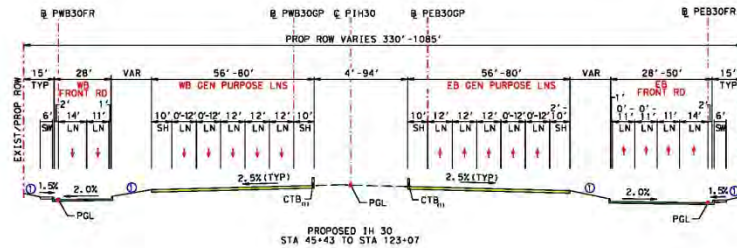
PAGE 2 OF 8

IH 635 FROM US 75 TO IH 30
 CITIES OF DALLAS, GARLAND
 AND MESQUITE,
 DALLAS COUNTY, TEXAS
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053,
 & 2374-02-144

PROPOSED IH 635 TYPICAL SECTIONS



EXISTING IH 30 TYPICAL SECTION



PROPOSED IH 30 TYPICAL SECTION

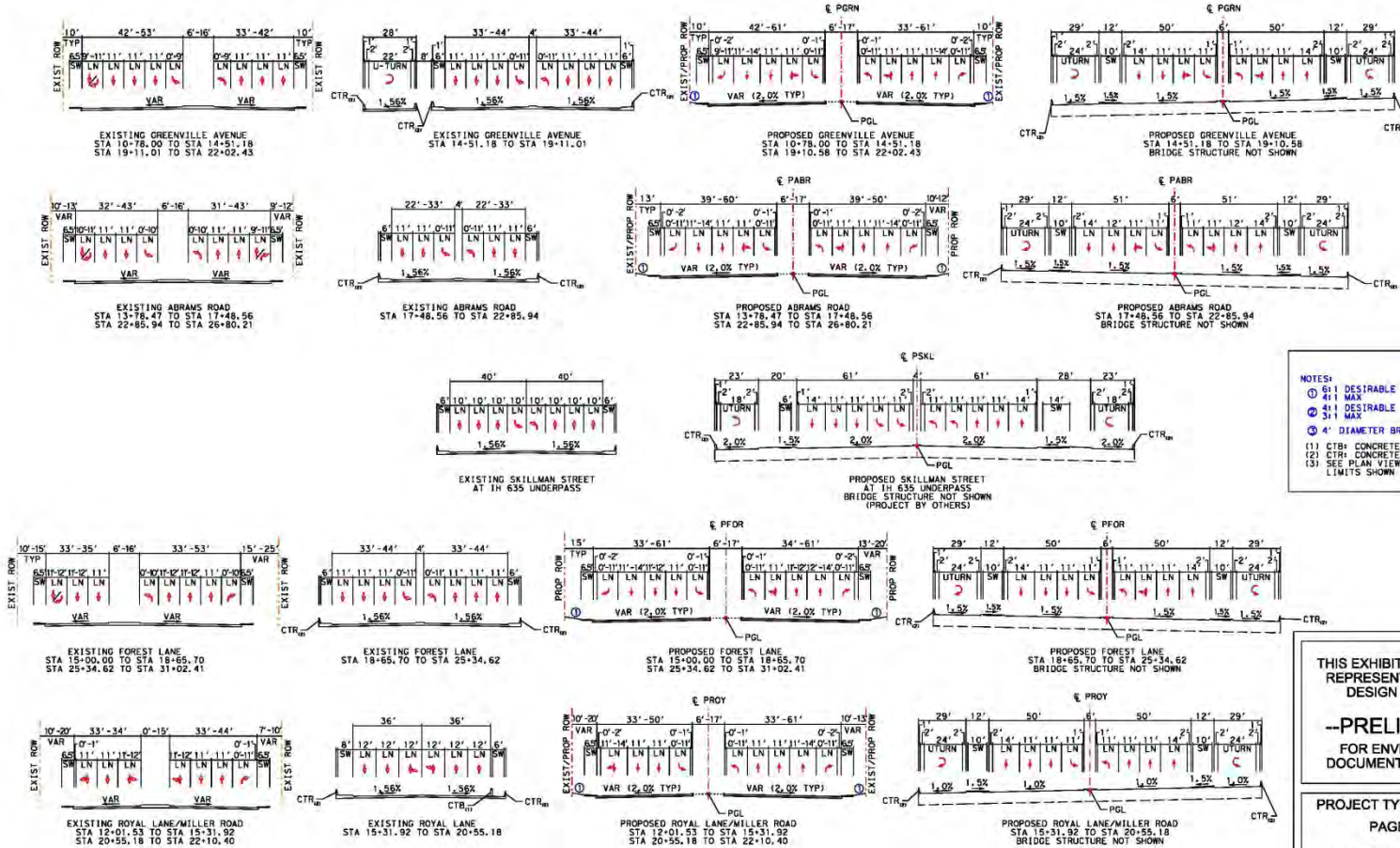
- NOTES:
- ① 6:1 DESIRABLE
4:1 MAX
 - ② 4:1 DESIRABLE
3:1 MAX
 - ③ 4' DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL
LIMITS SHOWN IN TYPICAL SECTIONS

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FOR ENVIRONMENTAL
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PROJECT TYPICAL SECTIONS
PAGE 4 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



- NOTES:
- ① 6" DESIRABLE
 - ② 4" MAX
 - ③ 4" DESIRABLE
 - ④ 3" MAX
 - ⑤ 4" DIAMETER BRIDGE COLUMN
 - (1) CTR: CONCRETE TRAFFIC BARRIER
 - (2) CTR: CONCRETE TRAFFIC RAIL
 - (3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

--PRELIMINARY--
FOR ENVIRONMENTAL DOCUMENT REVIEW ONLY

PROJECT TYPICAL SECTIONS
PAGE 5 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144

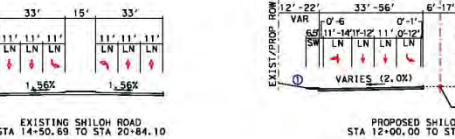
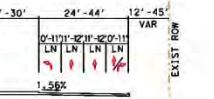
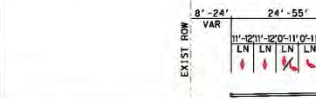
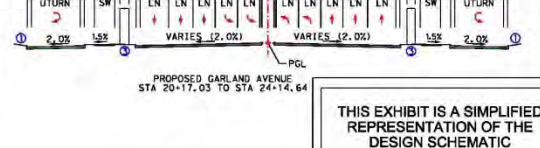
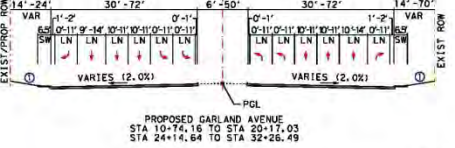
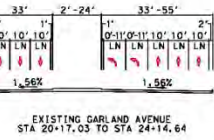
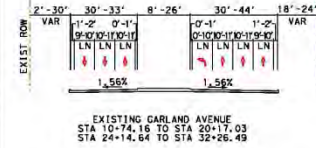
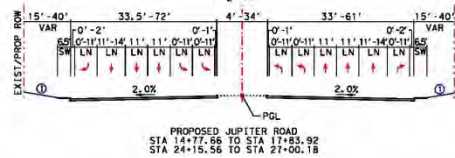
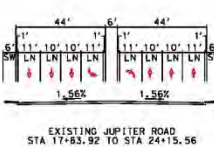
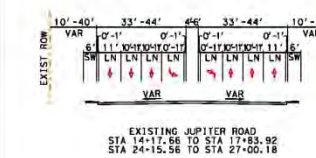
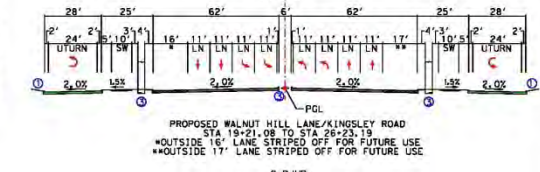
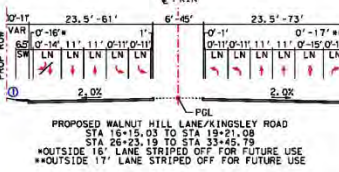
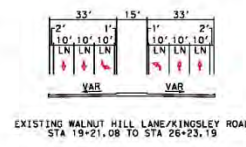
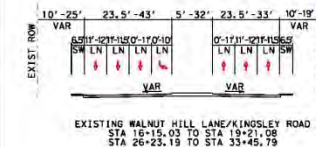
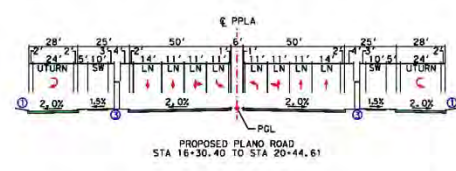
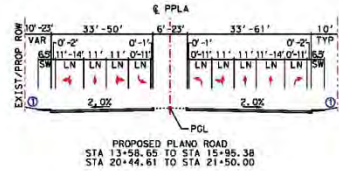
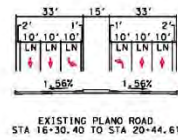
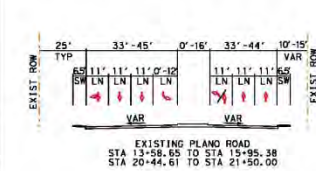
CROSS STREET TYPICAL SECTIONS

TOP OF CURB MAX. 10' 0"

10' 0"

10' 0"

10' 0"



- NOTES:
- ① DESIRABLE
 - ② MAX
 - ③ DESIRABLE
 - ④ MAX
 - ⑤ 4" DIAMETER BRIDGE COLUMN
 - (1) CTB: CONCRETE TRAFFIC BARRIER
 - (2) CTR: CONCRETE TRAFFIC RAIL
 - (3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

CROSS STREET TYPICAL SECTIONS

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--PRELIMINARY--
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PROJECT TYPICAL SECTIONS
PAGE 6 OF 8

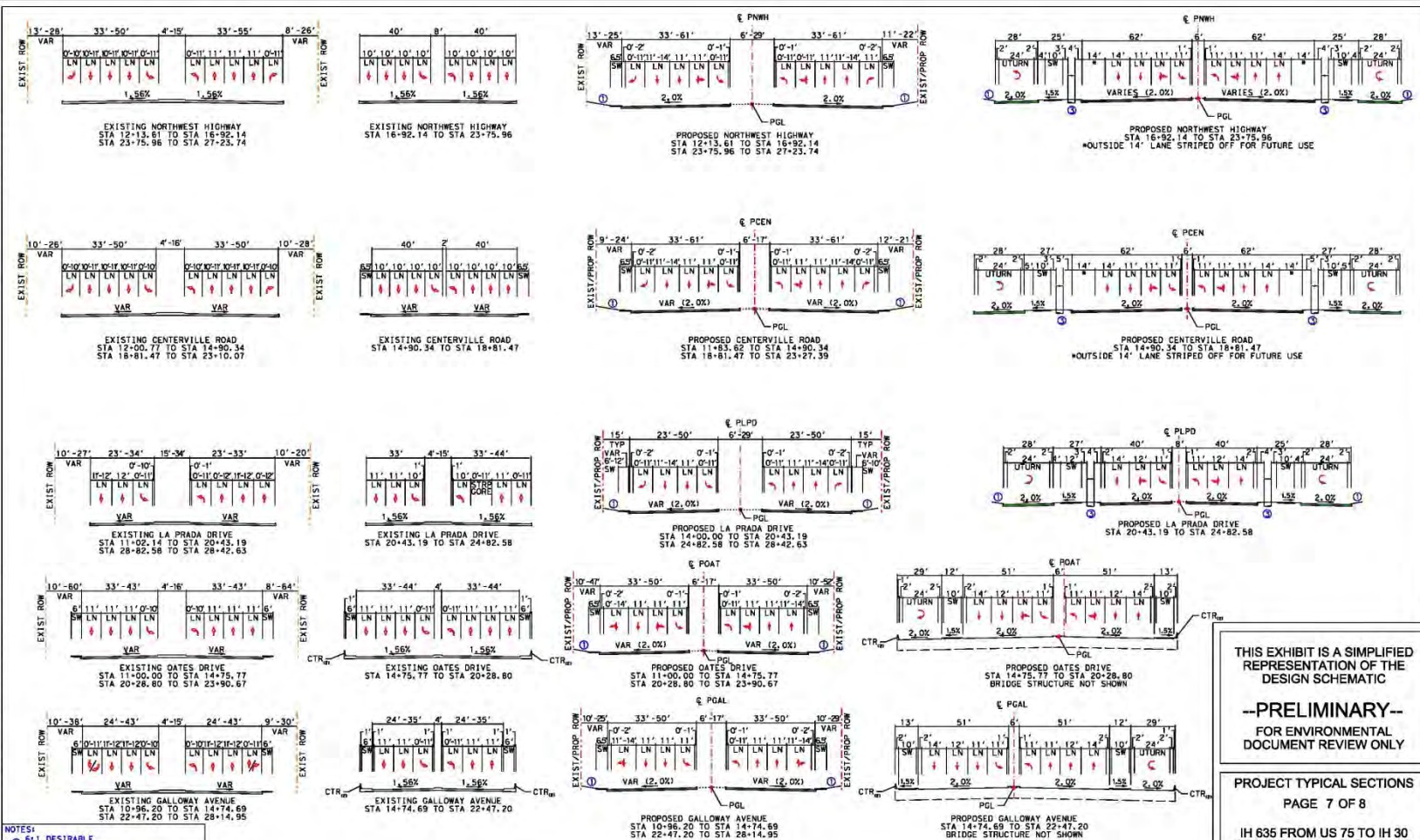
IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144

TOP TO BOTTOM: 100% PLAN

100% PLAN

100% PLAN

100% PLAN



- NOTES:
- ① 6:1 DESIRABLE
 - ② 4:1 MAX
 - ③ 3:1 DESIRABLE
 - ④ 3:1 MAX
 - ⑤ 4' DIAMETER BRIDGE COLUMN
- (1) CTB CONCRETE TRAFFIC BARRIER
(2) CTR CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

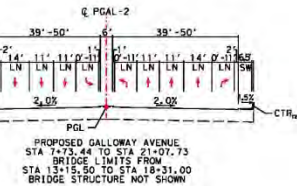
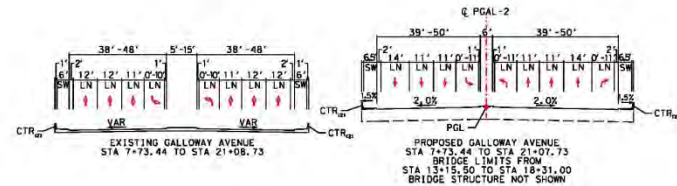
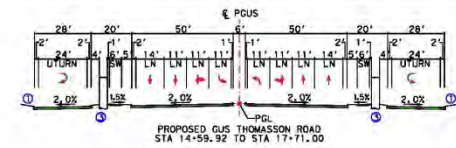
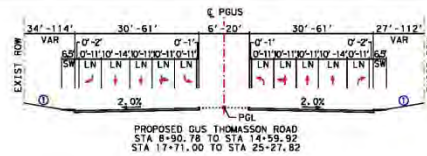
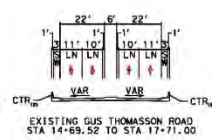
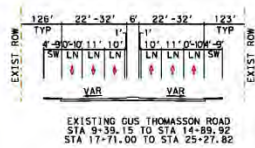
CROSS STREET TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

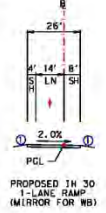
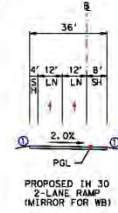
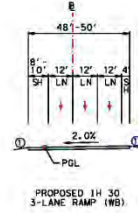
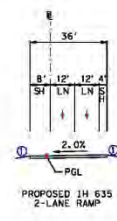
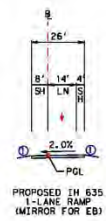
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PROJECT TYPICAL SECTIONS
PAGE 7 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
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DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



CROSS STREET TYPICAL SECTIONS



PROPOSED RAMP TYPICAL SECTIONS

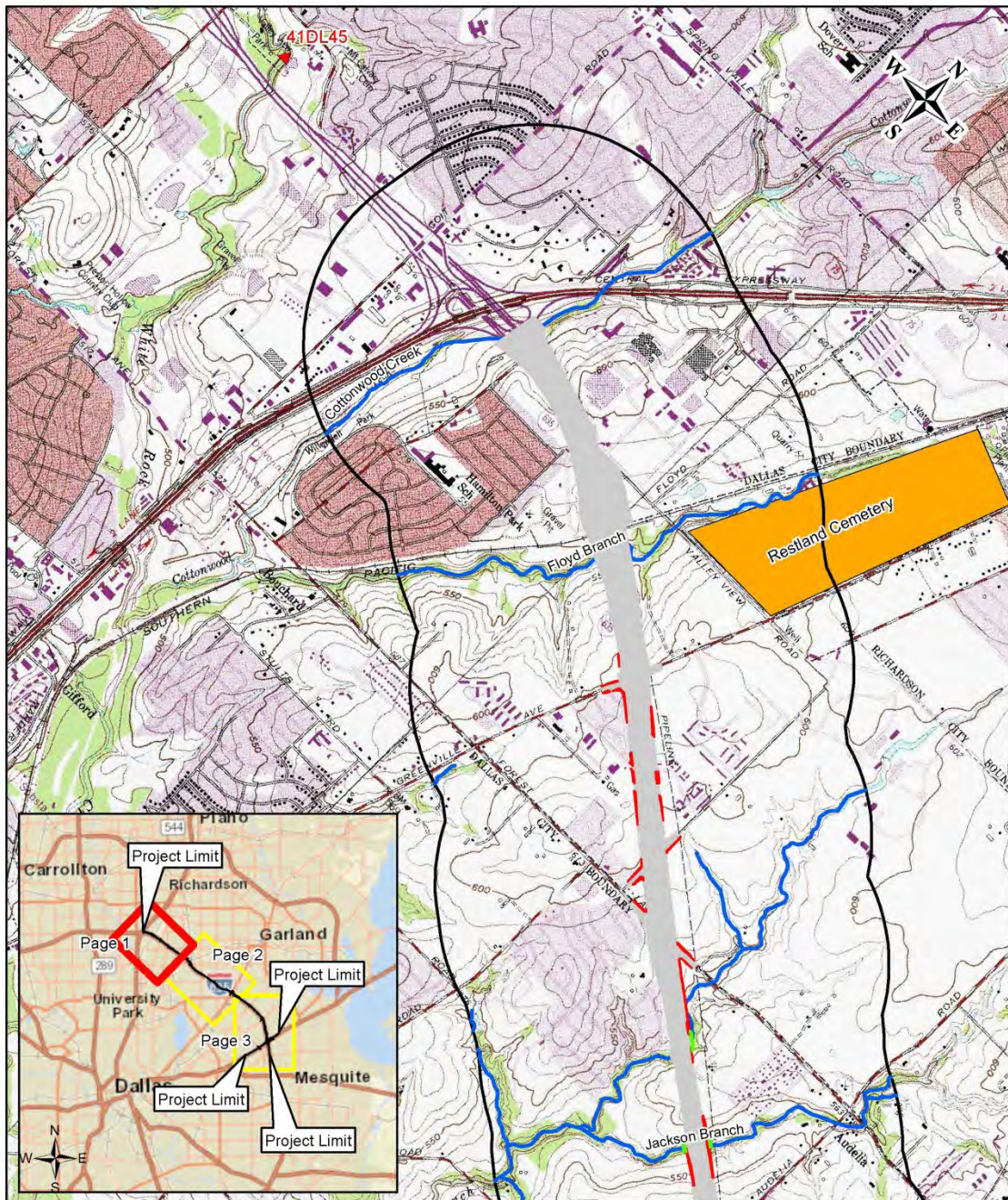
- NOTES:
- ① 5'1" DESIRABLE 4'1" MAX
 - ② 4'1" DESIRABLE 3'1" MAX
 - ③ 4" DIAMETER BRIDGE COLLAR
 - (1) CTR: CONCRETE TRAFFIC BARRIER
 - (2) CTR: CONCRETE TRAFFIC RAIL
 - (3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

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--PRELIMINARY--
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PROJECT TYPICAL SECTIONS
PAGE 8 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
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& 2374-02-144

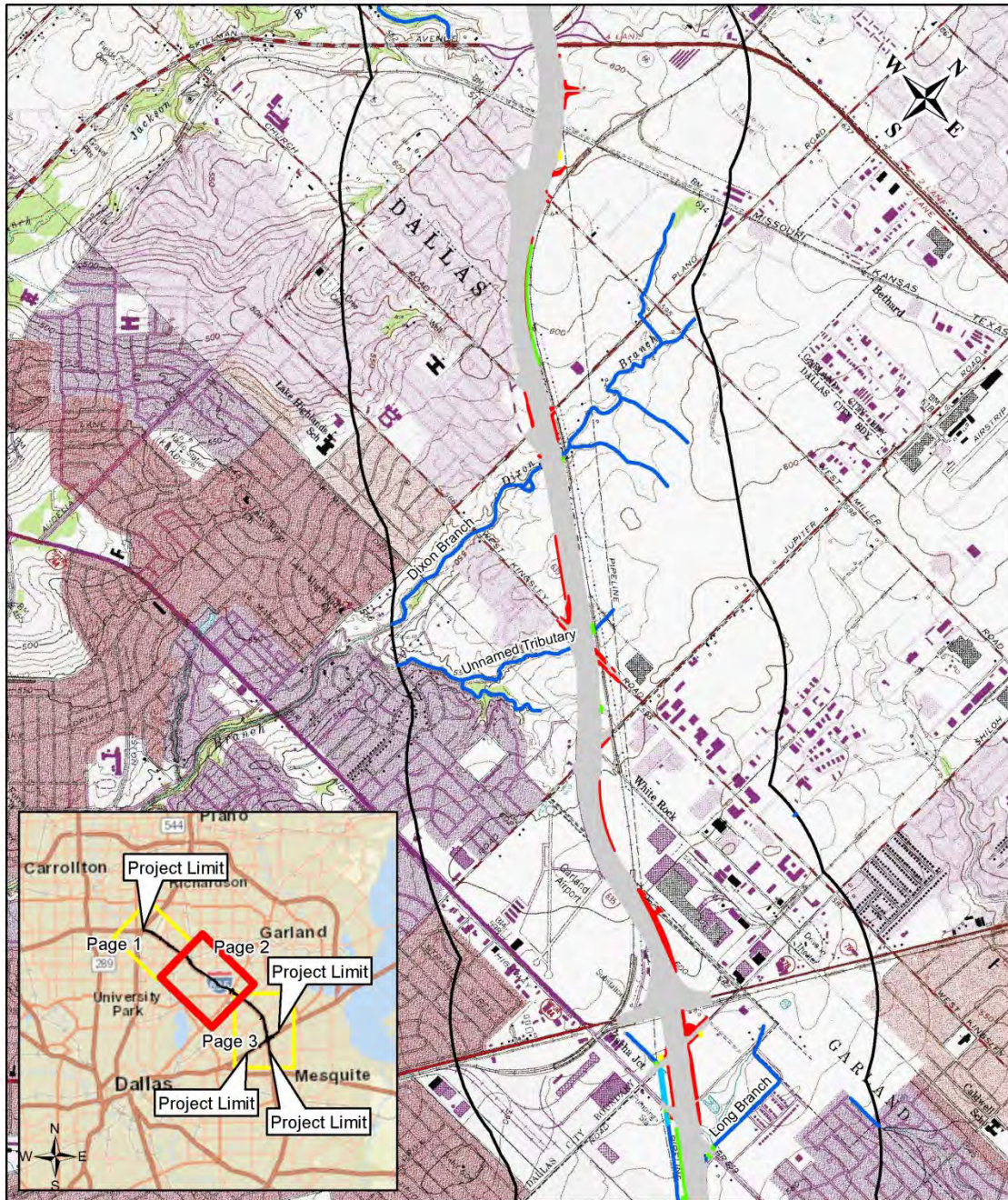


Legend

- | | |
|--|---|
| ■ Proposed ROW | ■ Cemetery |
| ■ Proposed Drainage Easement | ▲ Archeological Site |
| ■ Existing ROW | |
| 1-km Study Area | 0 750 1,500 3,000 Feet |
| — Stream Crossing | |

Archeological Resources and Study Area (Page 1 of 3)

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053 & 2374-02-144



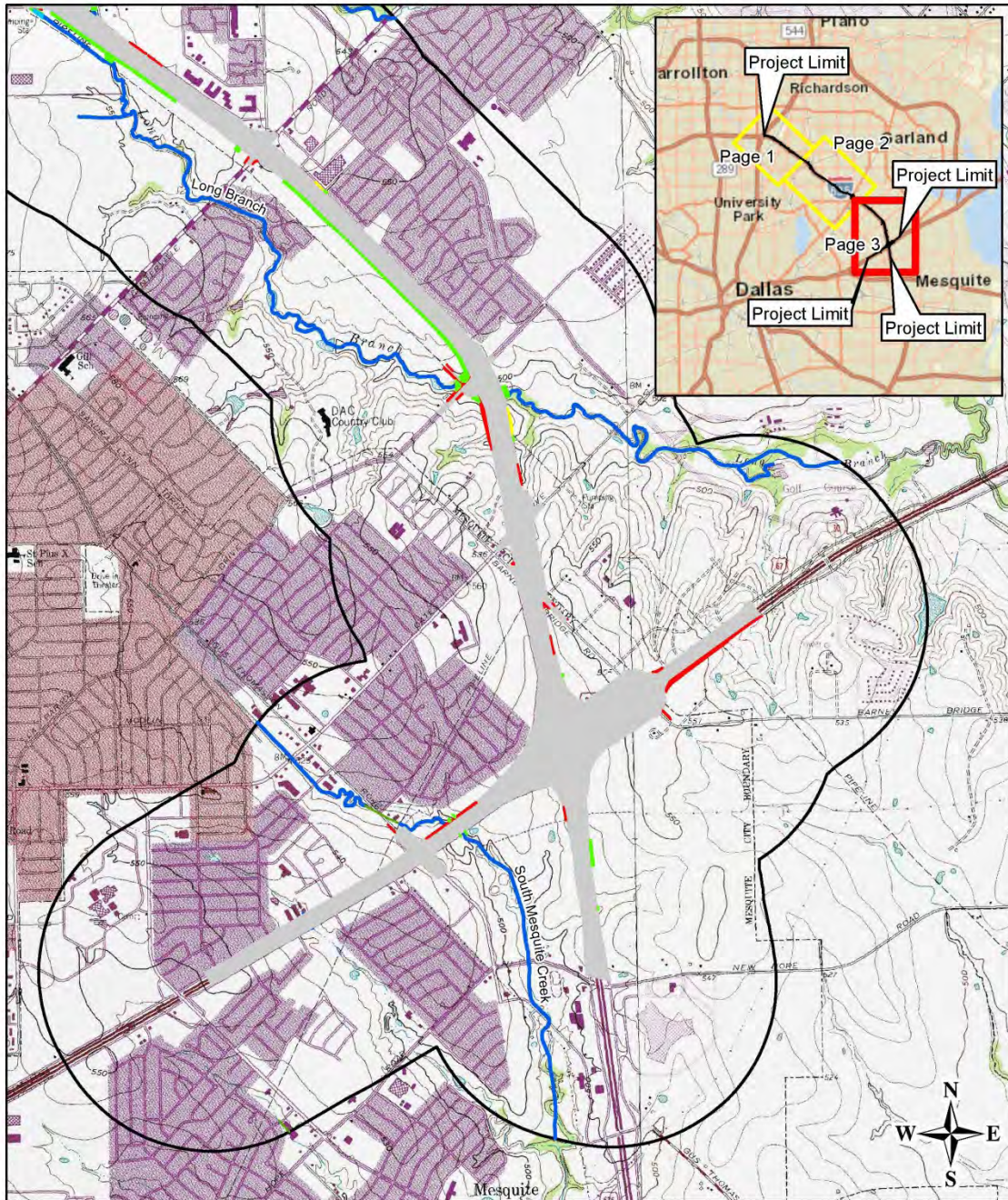
Legend

| | |
|--|--|
| █ Proposed ROW | 1-km Study Area |
| █ Proposed Drainage Easement | — Stream Crossing |
| █ Proposed Construction Easement | |
| █ Existing Drainage Easement | |
| █ Existing ROW | |

0 750 1,500 3,000 Feet

Archeological Resources and Study Area (Page 2 of 3)

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053 & 2374-02-144



Legend

| | |
|--|--|
| █ Proposed ROW | 1-km Study Area |
| █ Proposed Drainage Easement | — Stream Crossing |
| █ Proposed Construction Easement | |
| █ Existing Drainage Easement | |
| █ Existing ROW | |
| | 0 750 1,500 3,000 Feet |

Archeological Resources and Study Area (Page 3 of 3)

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053 & 2374-02-144

Project Coordination Request for Historical Studies Project



Project Coordination Request for Historical Studies Project

[Reset Form](#)

Main CSJ: 2374-01-137

Child CSJs: 2374-01-180, 2374-01-183, 2374-02-053, 2374-02-144

District(s): Dallas

County(ies): Dallas

Roadway Name: IH 635 LBJ East

Limits From: US 75

Limits To: IH 30

Project Description: Re-Evaluation consultation is being conducted for the proposed reconstruction and improvement to an 11.2-mile section of IH 635 in Dallas County, Texas. The proposed project extends along IH 635 through portions of the cities of Dallas, Garland, and Mesquite (see the Project Location Map in Appendix C). The original IH 635 Environmental Assessment (EA) received environmental clearance through a Federal Highway Administration (FHWA) Finding of No Significant Impact (FONSI) issued on January 30, 2003. The purpose of this project coordination request is to update the previous coordination that occurred in 2001 (see the memo dated January 24, 2001 in Appendix A).

The proposed project would reconstruct IH 635 to provide five general-purpose lanes (one additional general purpose lane in each direction), two tolled-managed lanes in each direction, and two to three frontage road lanes in each direction, from US 75 to near Royal Lane/Miller Road. From near Royal Lane/Miller Road to La Prada Drive, the proposed project would reconstruct IH 635 to provide five general-purpose lanes (one additional general purpose lane in each direction), two non-tolled express lanes in each direction, and two to three frontage road lanes in each direction. From La Prada Drive to south of IH 30, the project would reconstruct IH 635 to provide lane balance transitions between the general purpose lanes, proposed express lanes, and IH 30 interchange. Additionally, two to three frontage road lanes would be constructed in each direction from La Prada to Town East Boulevard.

Improvements to IH 30 would include reconstructing the main lanes to provide at least three lanes in each direction through the IH 635 interchange with allowances provided for potential future managed/express lanes. Auxiliary lanes would also be constructed for the IH 635 interchange. The IH 30 improvements would include two to three lane frontage roads in each direction (see the Project Plan View Map and Project Typical Sections in Appendix C).

The proposed project includes the construction of multiple noise walls located along the project corridor, where reasonable and feasible. Approximately 16.3 acres of additional right of way (ROW) and approximately 9.6 acres of easements would be required. Proposed ROW acquisition would be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

Proposed Area of Potential Effects

The proposed Area of Potential Effects (APE) varies along the project area. The APE is 150 feet from all proposed ROW/easements and is the existing ROW where project activities are confined to the existing ROW, per the Programmatic Agreement among the FHWA, TxDOT, the SHPO, and the ACHP. The APE and study area are shown on the Historic Properties and APE on Aerial Photograph Map in Appendix C.



[Reset Form](#)

Previously Recorded Historic Properties

The Texas Historical Commission (THC) Historic Properties Atlas online was reviewed for all previously recorded historic properties within ¼ mile of the project area. The atlas revealed two historical markers and one cemetery within the ¼-mile radius, listed below and identified on the Historic Properties and APE on Aerial Photograph map in Appendix C (Source: THC Atlas, <http://atlas.thc.state.tx.us/Map> (accessed October 24, 2016)).

1. Restland Cemetery (Cemetery), 13005 Greenville Avenue, Dallas, TX

Restland Cemetery was founded in 1925 with the intent to serve as a landscaped park with the use of in-ground memorial markers. It is the burial ground for many prominent figures in the Dallas area including politicians and professional athletes.

2. Highland Oaks Church of Christ (Official Texas Historical Marker, 2005), 10805 Kingsley Rd., Dallas, TX

The church originally formed in 1885 when Dallas was a small town, and met in private homes, the county courthouse and other places until they built their first building in 1867 at Ross and Market streets. Civil War veteran Gen. Richard M. Gano served the congregation as minister and church elder for many years after returning from the war. The church split in 1877 over the use of musical instruments. The congregation moved several times throughout its history, finally establishing at Kingsley Rd.

3. Florence Ranch Home (Recorded Texas Historic Landmark, 1978), 1424 Barnes Bridge Rd., Mesquite, TX

David and Julia (Beaty) Florence built the first portion of this ranch house in 1871-72 after moving to Dallas from Van Zandt County. It is a simple frame structure with elaborate wood trim. By 1890, the house and grounds were enlarged, occupying 730 acres. After Florence retired in 1908, his son Emet and his wife Perle (Curtis) continued to run the ranch, called Meadow View Farm.

The TxDOT Google Earth Database of Historic Properties, Districts and Bridges and the National Register of Historic Places (NRHP) Google Earth database of historic properties were also reviewed. No additional historic properties were identified (see TxDOT Google Earth Database of Eligible Historic Bridges - Search Results in Appendix C). TxDOT's Bridge Inspection Database disclosed over 35 bridges in the project area that are of historic age and are coded as "3" under "Item 37 - Historical Significance". TxDOT's Historic Bridge Manual published in March 2014 defines code "3" for historical significance as a "Bridge [that] is Not Eligible for the NRHP." A list of bridges and their eligibility is attached in Appendix E.



[Reset Form](#)

District personnel should complete this form with all appropriate documentation attached. ENV-HIST staff review is contingent on provision of an active CSJ (or equivalent if the project is not a construction project) against which environmental work can be charged. District-provided responses should reflect known data about the project and identify any limitations that hindered provision of the requested information. ENV-HIST staff will review the PCR form and attached information per established Documentation Standards. This review will result in:

- ENV-HIST environmental clearance of the project; OR
- ENV-HIST identification of additional technical studies required for clearance; OR
- ENV-HIST rejection of the PCR for failure to meet specific Documentation Standards and instructions on how to redress the rejection.

This form specifies minimally required information needed to properly facilitate ENV-HIST's review process. Please submit all relevant documentation with this PCR at one time.

NOTE: * If this project information changes over the course of design OR if the funding source changes, then HIST requires re-coordination and a revised PCR in ECOS.

Information Required to Process Historic Resources Coordination and Consultation

1. Targeted ENV clearance date: March 10, 2017
2. *Anticipated letting date: 2018
3. "Historic-age" date (let date minus 45 years): 1973
4. Yes *The proposed action is subject to federal permitting (i.e. Corps of Engineers, Coast Guard, IBWC, etc.).

Describe:

US Army Corps of Engineers permit required (permit type still under analysis).

5. Yes *The proposed action requires additional ROW (purchased or donated) or easements?

| Parcel ID | Required New ROW (acres) | Required New Easements | |
|-----------------|-----------------------------|------------------------|------------|
| | | Temporary | Permanent |
| See Appendix B. | 16.3 | 0.5 | 9.1 |
| Total: | 16.3 | 0.5 | 9.1 |



Project Coordination Request for Historical Studies Project

[Reset Form](#)

6. The following maps, tables or equivalents been uploaded to ECOS?

| Yes/No/NA | Map Type | ECOS File Name: |
|------------|--|--|
| <u>Yes</u> | Existing and proposed ROW boundaries. | 2374-01-137 LBJ East HIST PCR Appendices.pdf |
| <u>Yes</u> | Area of Potential Effects (APE) appropriate for project type. | 2374-01-137 LBJ East HIST PCR Appendices.pdf |
| <u>Yes</u> | Parcel boundaries for properties within the APE. | 2374-01-137 LBJ East HIST PCR Appendices.pdf |
| <u>Yes</u> | Results of the Texas Historic Sites Atlas search, identifying NHL, NRHP, SAL, and RTHL resources located within one-quarter mile of the project area listed in a table format and identified on color aerial map(s) or equivalent. | 2374-01-137 LBJ East HIST PCR Appendices.pdf |
| | Comments: See Project Description above and the Historic Properties and APE on Aerial Photograph map located in Appendix C. | |
| <u>Yes</u> | Results of Google Earth search with HIST-provided eligibility and historic bridge layers. | 2374-01-137 LBJ East HIST PCR Appendices.pdf |
| | Comments: No historic properties identified. | |

7. Yes Representative and dated photographs of the project area are uploaded to ECOS.

Note: Photographs should include the following elements:

1. Buildings/structures in the APE and those adjacent.
2. Road Features (culverts, bridges, landscaping, etc.
3. Areas of proposed construction.

File Name in ECOS: 2374-01-137 LBJ East HIST PCR Appendices.pdf

8. Yes Preliminary plans are uploaded to ECOS.

File Name in ECOS: 2374-01-137 LBJ East HIST PCR Appendices.pdf

9. Yes Historic-age bridges are within the project area.

| Location | NBI # | Year Built | Eligibility |
|-----------------|-------|------------|--------------|
| See Appendix E. | - | - | Not Eligible |

9.1 Yes Aerial map(s) or equivalent with bridge location(s) identified are uploaded to ECOS.

File Name in ECOS: 2374-01-137 LBJ East HIST PCR Appendices.pdf



Project Coordination Request for Historical Studies Project

[Reset Form](#)

9.2 No CHC consultation required (contact HIST if needed).

10. No Rock masonry features (culverts, ditches, walls, etc.) are within the project area.

11. No Historic-age rest area(s) are located within the project area.

12. No The proposed action involves the relocation of historical markers.

13. No Additional consulting parties (other than the THC) may be involved in this project.

Additional Project Comments:

District Personnel Certification

 Yes I reviewed all submitted documents for quality assessment and control.

District Personnel Name

Date:



Project Coordination Request for Historical Studies Project

[Reset Form](#)

The following table shows the revision history for this document.

| Revision History | |
|-------------------------------|--|
| Effective Date Month, Year | Reason for and Description of Change |
| December 2013 | Version 1 released. |
| June 2015 | Version 2 released. The form was converted to a PDF format. Form level validations were installed to ensure that all certified forms contained the minimum required information. Various questions were modified to accommodate the improved functionality of the PDF format. |
| August 2015 | Version 3 released. Revised the form to make it compatible with Adobe Acrobat Reader DC. No changes were made to the question sequence or form logic. |

Project Coordination Request Appendices

Interstate Highway (IH) 635 LBJ East Ultimate Project

From US 75 to IH 30

Cities of Dallas, Garland, and Mesquite, Dallas County, Texas

CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144

Appendix A – Previous Coordination

Appendix B – Proposed Right of Way (ROW) and Easement Information

Appendix C – Project Maps

Appendix D – Project Photos

Appendix E – Historic-Age Bridge Locations

Appendix A –Previous Coordination



cc cfm

Texas Department of Transportation

DEWITT C. GREER STATE HIGHWAY BLDG. • 125 E. 11TH STREET • AUSTIN, TEXAS 78701-2483 • (512) 463-8585

January 24, 2001

SECTION 106: IDENTIFICATION OF HISTORIC PROPERTIES

Dallas County

CSJ# 2374-01-127; 2374-02-053

IH 635 Corridor, East Section, from West of Greenville Ave. to North of Town East Blvd., Dallas, Garland and Mesquite, Texas

RECEIVED

PROJECT MANAGEMENT

Mr. Greg Smith
History Division
Texas Historical Commission
P.O. Box 12276
Austin, Texas 78711

RECEIVED
JAN 29 2001

TEXAS HISTORICAL COMMISSION

Dear Mr. Smith:

The proposed project will be undertaken with federal assistance. In accordance with the Programmatic Agreement (PA) among TxDOT, FHWA, the Advisory Council on Historic Preservation, and the THC, this letter initiates Section 106 consultation for the proposed undertaking. We hereby begin coordination on the National Register eligibility of properties documented during a historic structure survey of the project area.

The referenced undertaking will improve a 10.8-mile long section of IH 635 in an urbanized area in the cities of Dallas, Garland and Mesquite, Texas. The improved roadway will be constructed with the addition of 27.8 acres of new right-of-way (ROW). Proposed improvements to the current eight-lane highway include:

- Adding one 11.8 foot main lane in both directions on IH 635
- Adding 2 HOV lanes in both directions from US 75 to Skillman St.
- Adding 2 reversible HOV lanes in both directions from US 75 to Skillman St.
- Constructing 2 and 3-lane frontage roads to link existing non-continuous frontage roads in each direction
- Changing current ramping configuration along the length of the project, including that of the IH 635/IH 30 interchange.

In accordance with the provisions of 36 CFR 800, Texas Department of Transportation personnel conducted a cultural resources survey to identify properties potentially eligible for listing in the National Register of Historic Places. The project area is defined as an urban corridor with commercial and residential properties dating to the 1960s and the 1970s, which was the time of construction for the existing highway facility. *No properties 50 years of age or older are present within the area of potential effects*, which for this project was determined to be 500 feet from either side of proposed ROW.

MAR 28 2001

Dallas, Texas
RECEIVED

Please sign in the space provided below acknowledging your receipt of this correspondence and return a signed copy of this letter for our files within fifteen days. If you need further information, feel free to call me at 416-2770.

Sincerely,

Mario L. Sanchez
Mario L. Sánchez, Ph.D., R.A.
Historical Architect
Environmental Affairs Division
Attachment

RECEIPT ACKNOWLEDGED
NO HISTORIC PROPERTIES AFFECTED

NAME: _____ DATE: _____
State Historic Preservation Officer

NOT ELIGIBLE
for listing in the
National Register of Historic Places
PROJECT MAY PROCEED
by *[Signature]*
for F. Lawrence Oaks
State Historic Preservation Officer
Date *1-31-01*

Appendix B – Proposed Right of Way and Easement Information

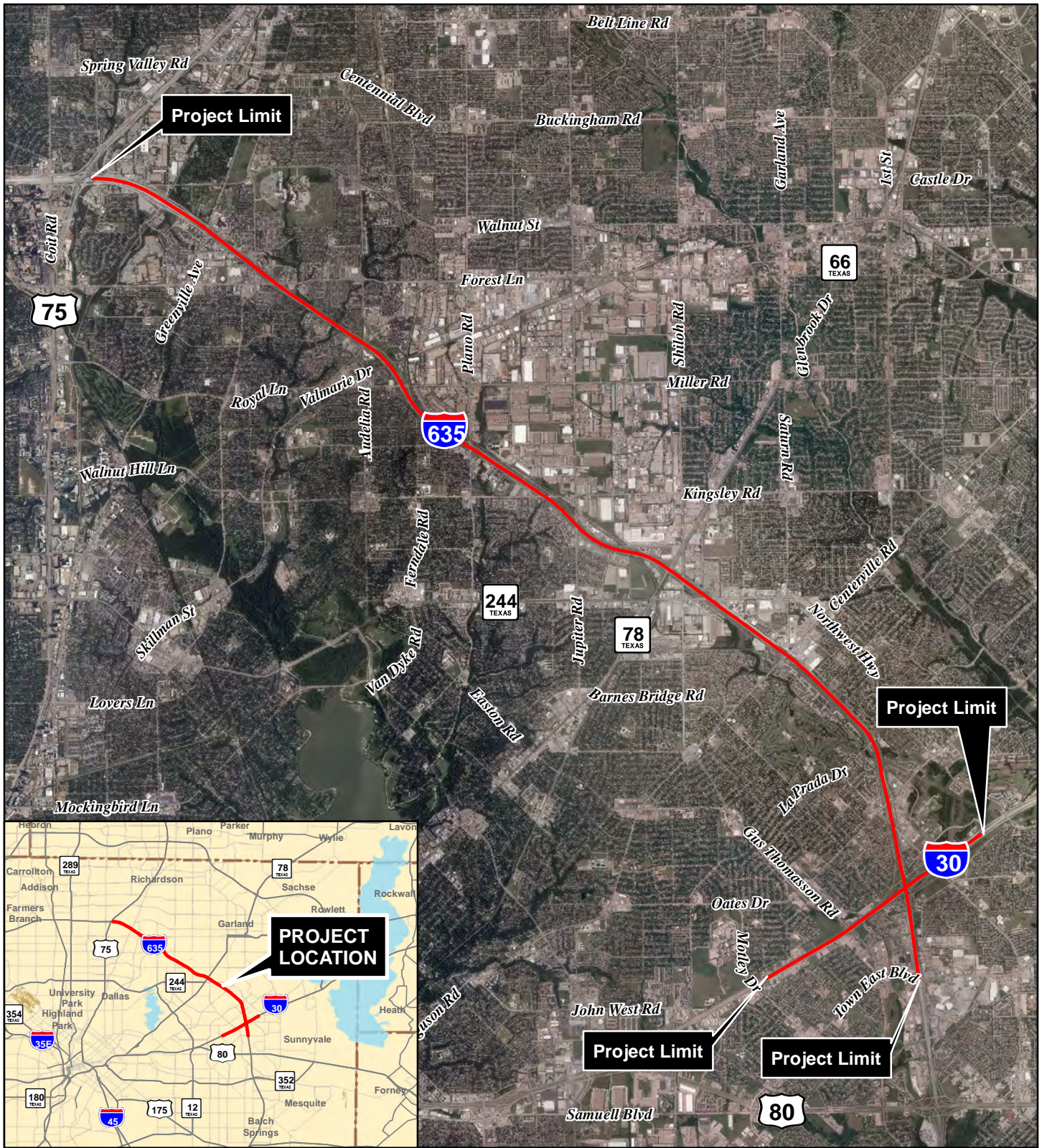
**IH 635 from US 75 to IH 30
Additional ROW and/or Easements**

| Parcel Number (see Plan View Map) | Property Address | Required New ROW | | Required New Permanent Easement | | Required New Temporary Easement | |
|-----------------------------------|-------------------------|------------------|-------|---------------------------------|-------|---------------------------------|-------|
| | | SF | Acres | SF | Acres | SF | Acres |
| 36 | 9079 VANTAGE POINT DR | 635 | 0.01 | – | – | – | – |
| 38 | 9170 LBJ FWY | 49 | <0.00 | – | – | – | – |
| 40 | 12355 GREENVILLE AVE | 2,190 | 0.05 | – | – | – | – |
| 43 | 12330 GREENVILLE AVE | 8,556 | 0.20 | – | – | – | – |
| 44 | 9230 LBJ FWY | 1,665 | 0.04 | – | – | – | – |
| 45 | 9250 LBJ FWY | 15 | <0.00 | – | – | – | – |
| 48 | 9221 LBJ FWY | 4,232 | 0.10 | – | – | – | – |
| 49 | 9292 LBJ FWY | 6,147 | 0.14 | – | – | – | – |
| 50 | 9241 LBJ FWY | 6,817 | 0.16 | – | – | – | – |
| 51 | 9310 LBJ FWY | 5,970 | 0.14 | – | – | – | – |
| 53 | 9330 LBJ FWY | 7,311 | 0.17 | – | – | – | – |
| 55 | 9301 LBJ FWY | 8,811 | 0.20 | – | – | – | – |
| 57 | 9309 LBJ FWY | 736 | 0.02 | – | – | – | – |
| 60 | 9323 LBJ FWY | 5,319 | 0.12 | – | – | – | – |
| 62 | 9331 ABRAMS RD | 778 | 0.02 | – | – | – | – |
| 67 | 11919 ABRAMS RD | 2,491 | 0.06 | – | – | – | – |
| 68 | 11820 ABRAMS RD | 5,702 | 0.13 | – | – | – | – |
| 69 | 9401 LBJ FWY | 4,084 | 0.09 | – | – | – | – |
| 70 | 9441 FOREST LN | 1,287 | 0.03 | – | – | – | – |
| 71 | 9441 LBJ FWY | 218 | 0.01 | – | – | – | – |
| 72 | 9465 LBJ FWY | 3,364 | 0.08 | – | – | – | – |
| 77 | 9501 LBJ FWY | 5,911 | 0.14 | – | – | – | – |
| 78 | 9519 FOREST LN | 2,198 | 0.05 | – | – | – | – |
| 84 | 9500 FOREST LN | 8,766 | 0.20 | – | – | – | – |
| 97 | 9550 FOREST LN | 13,209 | 0.30 | 10,061 | 0.23 | – | – |
| 99 | 9590 FOREST LN | 77 | <0.00 | – | – | – | – |
| 102 | 9676 FERRIS BRANCH BLVD | – | – | 3,425 | 0.08 | – | – |
| 104 | 9941 WHITEHURST DR | 13,782 | 0.32 | 2,530 | 0.06 | – | – |
| 105 | 9821 SUMMERWOOD CIR | 33,981 | 0.78 | 4,644 | 0.11 | – | – |
| 106 | 9700 LBJ FWY | 1,657 | 0.04 | – | – | – | – |
| 118 | 9810 ADLETA BLVD | 8,847 | 0.20 | – | – | – | – |
| 121 | 9804 ADLETA BLVD | 10,877 | 0.25 | – | – | – | – |
| 130 | 10111 MILLER RD | 48 | <0.00 | – | – | 4,855 | 0.11 |
| 133 | 10111 MILLER RD | 12,654 | 0.29 | – | – | – | – |
| 139 | KANSAS CITY SOUTHERN RR | 213 | <0.00 | – | – | – | – |
| 504 | 9375 FOREST LN | 1,531 | 0.04 | – | – | – | – |
| 572 | 12270 GREENVILLE AVE | 2,320 | 0.05 | – | – | – | – |
| 168 | 99999 NO NAME ST | 30,348 | 0.70 | 41,432 | 0.95 | – | – |

| Parcel Number (see Plan View Map) | Property Address | Required New ROW | | Required New Permanent Easement | | Required New Temporary Easement | |
|---|-------------------------|------------------|-------|------------------------------------|-------|------------------------------------|-------|
| | | SF | Acres | SF | Acres | SF | Acres |
| 177 | 10551 NEW CHURCH RD | 15 | <0.00 | – | – | – | – |
| 178 | 10557 NEW CHURCH RD | 645 | 0.01 | – | – | – | – |
| 179 | 10561 NEW CHURCH RD | 951 | 0.02 | – | – | – | – |
| 180 | 10601 NEW CHURCH RD | 2,394 | 0.05 | – | – | – | – |
| 181 | 10611 NEW CHURCH RD | 4,748 | 0.11 | – | – | – | – |
| 182 | 10631 NEW CHURCH RD | 1,741 | 0.04 | – | – | – | – |
| 184 | 10025 PLANO RD | 1,886 | 0.04 | – | – | – | – |
| 185 | 9938 PLANO RD | 1,074 | 0.02 | – | – | – | – |
| 190 | 99999 NO NAME ST | 11,294 | 0.26 | 849 | 0.02 | – | – |
| 192 | 10805 WALNUT HILL LN | 3,526 | 0.08 | – | – | – | – |
| 193 | 10949 WALNUT HILL LN | 2,124 | 0.05 | – | – | – | – |
| 194 | 11035 WALNUT HILL LN | 1,443 | 0.03 | – | – | – | – |
| 196 | 11085 WALNUT HILL LN | 619 | 0.01 | – | – | – | – |
| 197 | 11111 WALNUT HILL LN | 25,943 | 0.60 | – | – | – | – |
| 198 | 11100 WALNUT HILL LN | 3,686 | 0.08 | – | – | – | – |
| 199 | 99999 NO NAME ST | 14,283 | 0.33 | 6,967 | 0.16 | – | – |
| 208 | 3702 W KINGSLEY RD | – | – | 3,463 | 0.08 | – | – |
| 210 | 11405 LBJ FWY | 2,472 | 0.07 | – | – | – | – |
| 215 | KANSAS CITY SOUTHERN RR | 6,961 | 0.16 | – | – | – | – |
| 216 | 11611 LBJ FWY | 4,645 | 0.11 | – | – | – | – |
| 218 | 11675 LBJ FWY | 2,107 | 0.05 | – | – | – | – |
| 515 | 11200 KINGSLEY RD | 129 | <0.00 | – | – | – | – |
| 516 | 3700 W KINGSLEY RD | 730 | 0.02 | – | – | – | – |
| 517 | 3536 W KINGSLEY RD | 4,019 | 0.09 | – | – | – | – |
| 222 | 2900 MCCREE RD | 7,017 | 0.16 | – | – | – | – |
| 223 | 2950 MCCREE RD | 17,109 | 0.39 | – | – | – | – |
| 224 | 2820 MCCREE RD | 14,797 | 0.34 | – | – | – | – |
| 226 | 2800 MCCREE RD | 47,696 | 1.09 | – | – | – | – |
| 230 | 2780 MCCREE RD | 31,943 | 0.73 | – | – | – | – |
| 238 | 12277 SHILOH RD | 1,010 | 0.02 | – | – | 3,190 | 0.07 |
| 240 | 4101 S SHILOH RD | 45,724 | 1.05 | – | – | – | – |
| 241 | 12200 SHILOH RD | 8,349 | 0.19 | 197 | <0.00 | – | – |
| 242 | 12625 LBJ FWY | 14,334 | 0.33 | – | – | 3,704 | 0.09 |
| 243 | 12635 LBJ FWY | 16,065 | 0.37 | – | – | – | – |
| 244 | 12715 LBJ FWY | 5,288 | 0.12 | – | – | – | – |
| 245 | 12721 LBJ FWY | 3,103 | 0.07 | – | – | – | – |
| 246 | 12733 LBJ FWY | 790 | 0.02 | – | – | – | – |
| 248 | 2102 W NORTHWEST HWY | – | – | 3,426 | 0.08 | – | – |
| 253 | 2002 EASTGATE DR | 6 | <0.00 | – | – | – | – |
| 254 | 1910 EASTGATE DR | 1,208 | 0.03 | – | – | – | – |
| 255 | 1902 EASTGATE DR | 1,393 | 0.03 | – | – | – | – |
| 256 | 1810 EASTGATE DR | 1,901 | 0.04 | – | – | – | – |
| 262 | 11601 FERGUSON RD | 350 | 0.01 | 106,839 | 2.45 | – | – |
| 273 | 4111 ATHLETIC CLUB DR | 12,813 | 0.29 | 11,059 | 0.25 | – | – |
| 278-A to D | 1122 W CENTERVILLE RD | – | – | 1,091 | 0.03 | 1,285 | 0.03 |
| 290 | 11200 LBJ FWY | 3,884 | 0.09 | 143,927 | 3.30 | – | – |
| 291 | 10800 LBJ FWY | 2,088 | 0.05 | 5,470 | 0.13 | – | – |

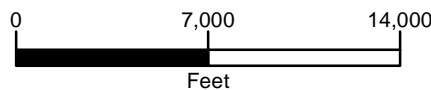
| Parcel Number (see Plan View Map) | Property Address | Required New ROW | | Required New Permanent Easement | | Required New Temporary Easement | |
|---|--------------------------|------------------|-------------|------------------------------------|------------|------------------------------------|------------|
| | | SF | Acres | SF | Acres | SF | Acres |
| 321 | 2200 LA PRADA DR | 2,221 | 0.05 | – | – | – | – |
| 330 | 1751 OATES DR | 9,979 | 0.23 | 6,145 | 0.14 | – | – |
| 331 | 1900 PINEHURST LN | 19,395 | 0.45 | – | – | – | – |
| 340 | 2212 OATES DR | 1,027 | 0.02 | – | – | – | – |
| 344 | 15330 LBJ FWY | – | – | 11,832 | 0.27 | – | – |
| 364 | 15900 LBJ FWY | – | – | – | – | 3,821 | 0.09 |
| 387 | 1919 OATES DR | 1,928 | 0.04 | – | – | – | – |
| 390 | 1805 OATES DR | – | – | 637 | <0.00 | 1,474 | 0.03 |
| 391 | 4000 TOWNE CROSSING BLVD | 917 | 0.02 | – | – | – | – |
| 399 | 16230 LBJ FWY | 290 | 0.01 | – | – | – | – |
| 402 | 5031 N GALLOWAY AVE | 4,353 | 0.09 | – | – | – | – |
| 404 | 1725 OATES DR | – | – | 3,841 | 0.09 | 1,434 | 0.03 |
| 413 | 4933 N GALLOWAY AVE | – | – | 164 | <0.00 | – | – |
| 417 | 4815 N GALLOWAY AVE | – | – | 695 | 0.02 | – | – |
| 427 | 3865 CHILDRESS AVE | – | – | 14,900 | 0.34 | – | – |
| 433 | 3795 EMPORIUM CIR | – | – | 185 | <0.00 | – | – |
| 435 | 3811 PAVILLION CT | – | – | 528 | 0.01 | – | – |
| 440 | 3950 GUS THOMASSON RD | 6,157 | 0.14 | 1,321 | 0.03 | – | – |
| 441 | 2401 IH 30 | 7,423 | 0.17 | 1,518 | 0.03 | – | – |
| 445 | 2544 IH 30 | – | – | 420 | 0.01 | – | – |
| 446 | 4035 TOWNE CROSSING BLVD | – | – | 882 | 0.02 | – | – |
| 522 | 12200 SHILOH RD | – | – | 1,072 | 0.02 | 1,048 | 0.02 |
| 523 | 12243 SHILOH RD | – | – | 1,101 | 0.03 | – | – |
| 527 | 12699 E NORTHWEST HWY | 152 | <0.00 | 1,096 | 0.03 | – | – |
| 528 | 12670 E NORTHWEST HWY | 719 | 0.02 | 3,057 | 0.07 | – | – |
| 530 | 2155 W NORTHWEST HWY | – | – | 2,494 | 0.06 | – | – |
| 531 | 2129 W NORTHWEST HWY | – | – | 930 | 0.02 | – | – |
| 535 | 11611 FERGUSON RD | – | 0.01 | – | – | – | – |
| 541 | 10600 WOODMEADOW PKWY | 7 | <0.00 | 200 | <0.00 | – | – |
| 542 | 10700 WOODMEADOW PKWY | 1,064 | 0.02 | – | – | – | – |
| 559 | 2749 IH 30 | 1,354 | 0.03 | – | – | – | – |
| 561B | 1650 REPUBLIC PKWY | 2,914 | 0.07 | – | – | – | – |
| 562 | 1520 IH 30 | 89,169 | 2.05 | – | – | – | – |
| 565 | 4500 N GALLOWAY AVE | 3,787 | 0.09 | – | – | – | – |
| TOTAL | | 709,953 | 16.3 | 398,399 | 9.1 | 20,811 | 0.5 |

Appendix C - Project Maps



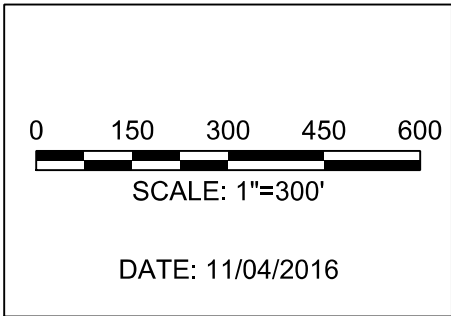
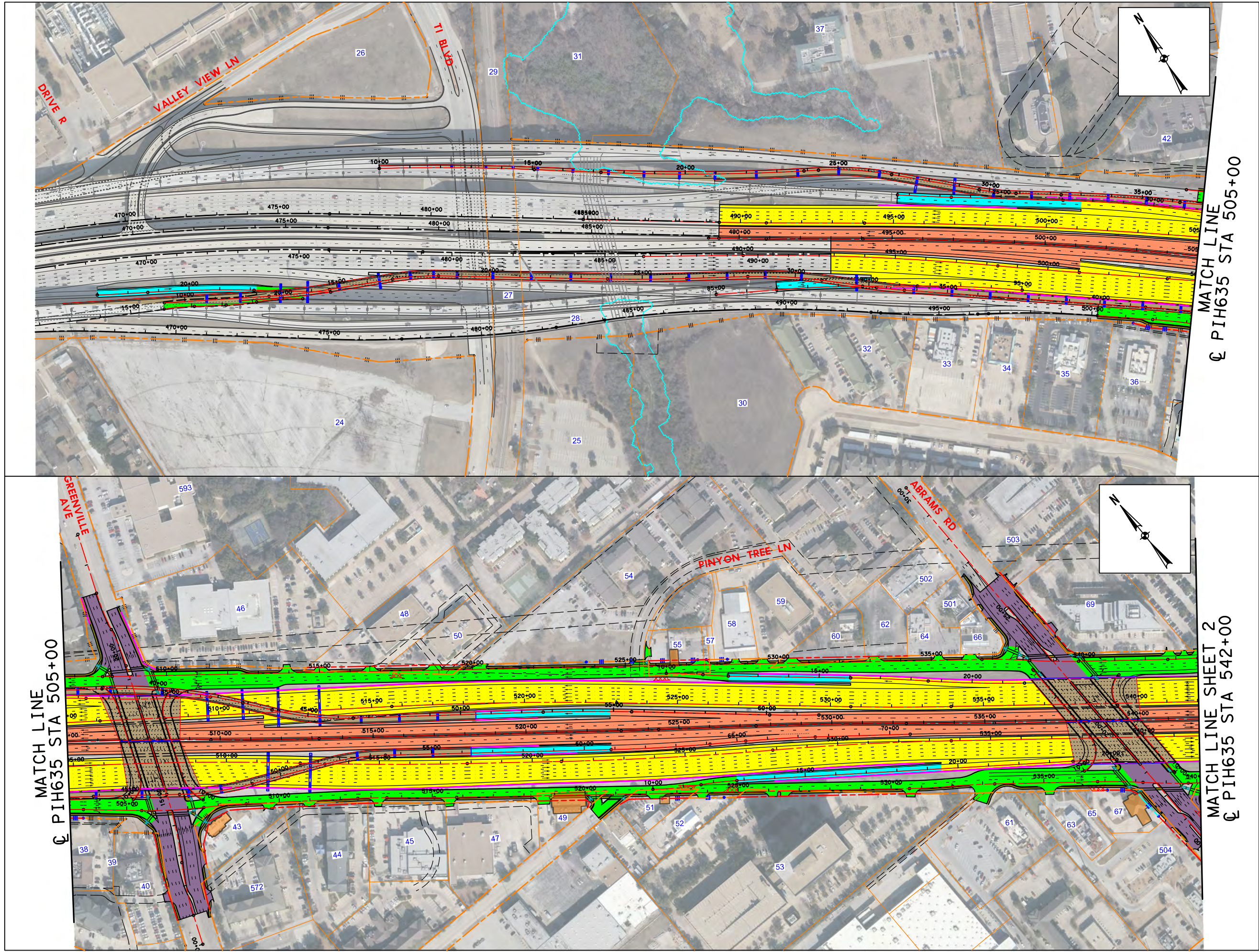
Legend

— Project Limits



Project Location Map on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053, & 2374-02-144



LEGEND:

- EXISTING ROW
- EXISTING ACCESS DENIAL (TO REMAIN)
- EXISTING EASEMENT
- EXISTING PROPERTY LIMITS
- EXISTING 100 YR FLOODPLAIN LIMITS
- PROPOSED CENTERLINE/BASELINE
- PROPOSED ROW
- PROPOSED EASEMENT
- TEMPORARY CONSTRUCTION EASEMENT
- PROPOSED ACCESS DENIAL
- PROPOSED EDGE OF PAVEMENT
- PROPOSED BRIDGE ABUTMENT
- PROPOSED BRIDGE BENT
- PROPOSED RETAINING WALL
- PROPOSED PRELIMINARY NOISE WALL
- PROPOSED CULVERT
- DIRECTION OF TRAVEL
- PROPOSED BRIDGE
- PROPOSED MAIN LANES
- PROPOSED MANAGED LANES
- PROPOSED RAMP
- PROPOSED FRONTAGE ROAD
- PROPOSED CROSS STREET
- PROPOSED SIDEWALK
- POTENTIAL DISPLACEMENTS
- PAVEMENT / BRIDGE TO BE REMOVED

SOURCE/DATE OF AERIAL BASEMAP: TXDOT/2013
(SEE 90% SCHEMATICS)

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

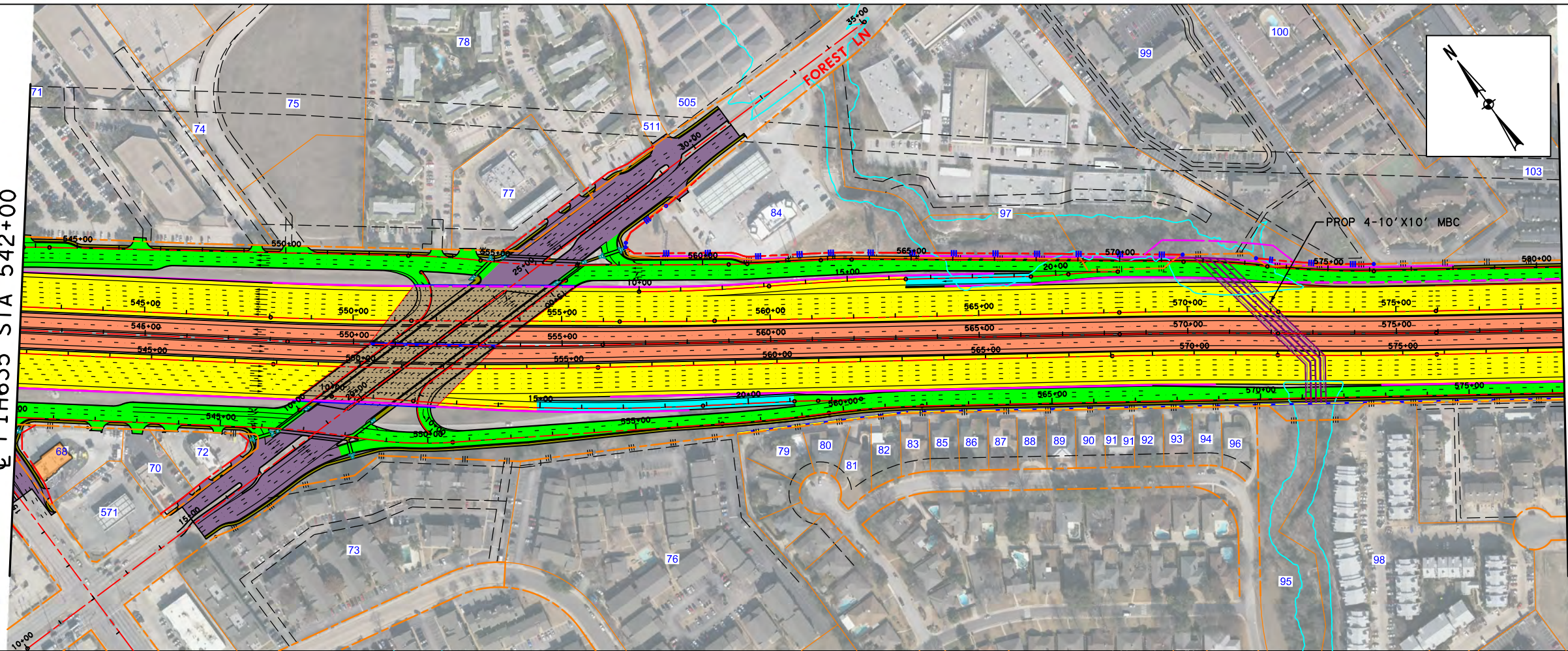
--PRELIMINARY--

FOR ENVIRONMENTAL DOCUMENT REVIEW ONLY

PROJECT PLAN VIEW MAP
Page 1 OF 10

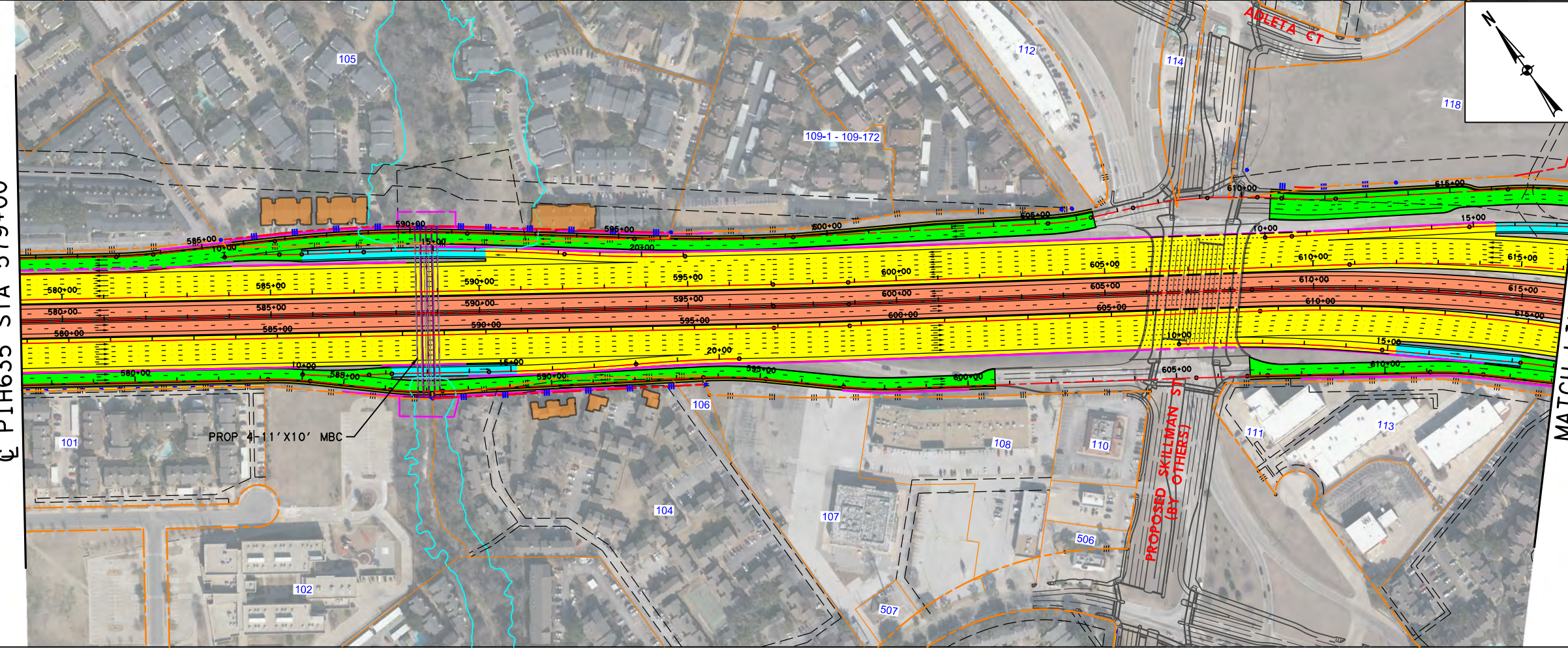
IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144

MATCH LINE SHEET 1
☐ PIH635 STA 542+00

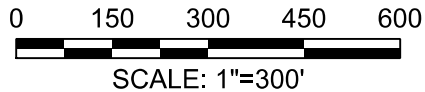


MATCH LINE
☐ PIH635 STA 579+00

MATCH LINE
☐ PIH635 STA 579+00



MATCH LINE SHEET 3
☐ PIH635 STA 616+00



DATE: 11/04/2016

LEGEND:

- EXISTING ROW
- EXISTING ACCESS DENIAL (TO REMAIN)
- EXISTING EASEMENT
- EXISTING PROPERTY LIMITS
- EXISTING 100 YR FLOODPLAIN LIMITS
- PROPOSED CENTERLINE/BASELINE
- PROPOSED ROW
- PROPOSED EASEMENT
- TEMPORARY CONSTRUCTION EASEMENT
- PROPOSED ACCESS DENIAL
- PROPOSED EDGE OF PAVEMENT
- PROPOSED BRIDGE ABUTMENT
- PROPOSED BRIDGE BENT
- PROPOSED RETAINING WALL
- PROPOSED PRELIMINARY NOISE WALL
- PROPOSED CULVERT
- DIRECTION OF TRAVEL
- PROPOSED BRIDGE
- PROPOSED MAIN LANES
- PROPOSED MANAGED LANES
- PROPOSED RAMP
- PROPOSED FRONTAGE ROAD
- PROPOSED CROSS STREET
- PROPOSED SIDEWALK
- POTENTIAL DISPLACEMENTS
- PAVEMENT / BRIDGE TO BE REMOVED

SOURCE/DATE OF AERIAL BASEMAP:
TXDOT/2013

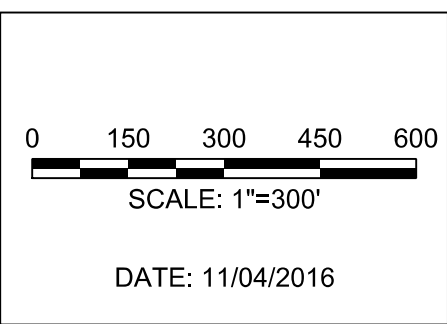
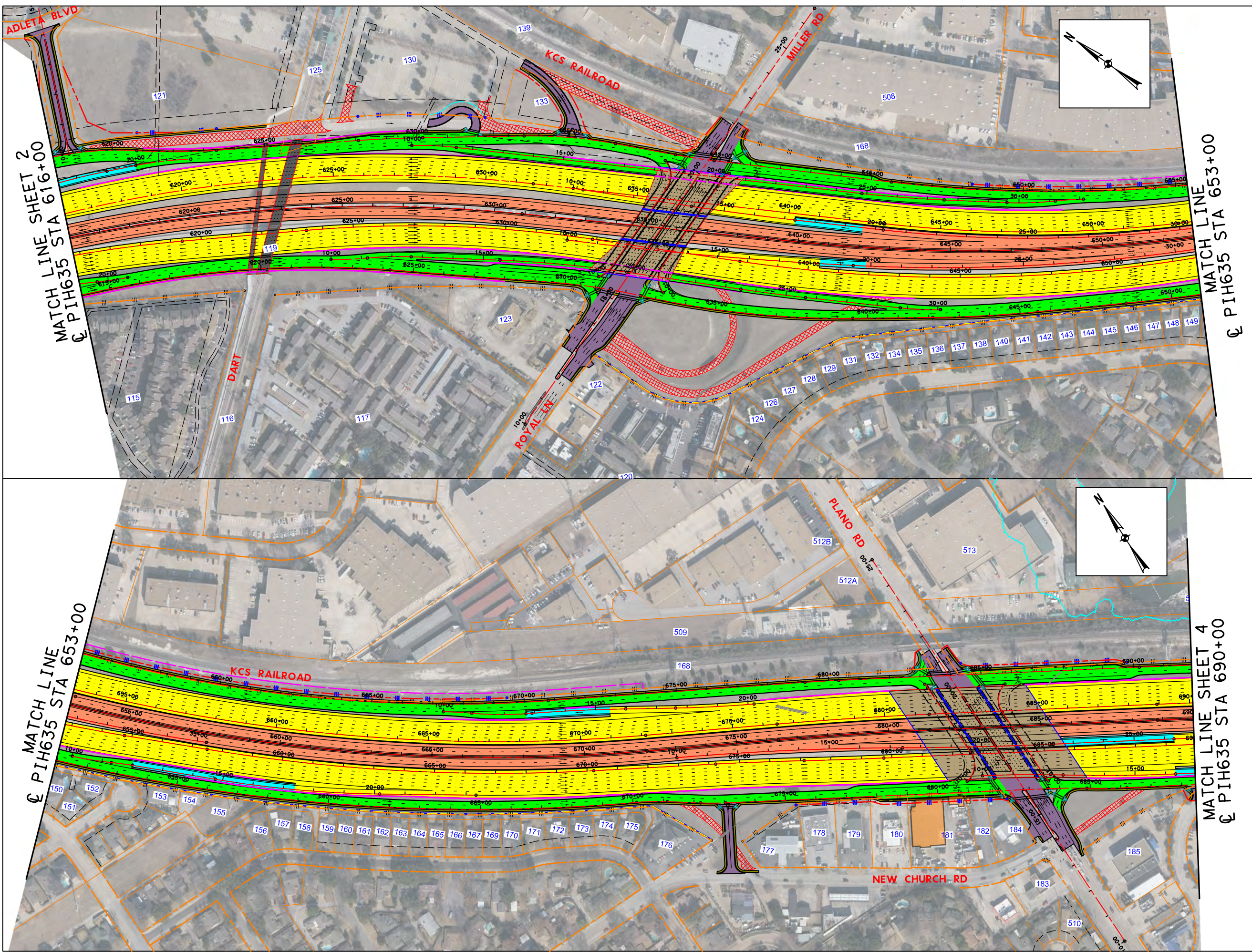
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PROJECT PLAN VIEW MAP
Page 2 OF 10

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



LEGEND:

- EXISTING ROW
- EXISTING ACCESS DENIAL (TO REMAIN)
- EXISTING EASEMENT
- EXISTING PROPERTY LIMITS
- EXISTING 100 YR FLOODPLAIN LIMITS
- PROPOSED CENTERLINE/BASELINE
- PROPOSED ROW
- PROPOSED EASEMENT
- TEMPORARY CONSTRUCTION EASEMENT
- PROPOSED ACCESS DENIAL
- PROPOSED EDGE OF PAVEMENT
- PROPOSED BRIDGE ABUTMENT
- PROPOSED BRIDGE BENT
- PROPOSED RETAINING WALL
- PROPOSED PRELIMINARY NOISE WALL
- PROPOSED CULVERT
- DIRECTION OF TRAVEL
- PROPOSED BRIDGE
- PROPOSED MAIN LANES
- PROPOSED MANAGED LANES
- PROPOSED RAMP
- PROPOSED FRONTAGE ROAD
- PROPOSED CROSS STREET
- PROPOSED SIDEWALK
- POTENTIAL DISPLACEMENTS
- PAVEMENT / BRIDGE TO BE REMOVED

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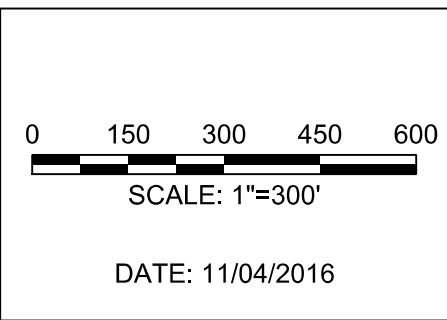
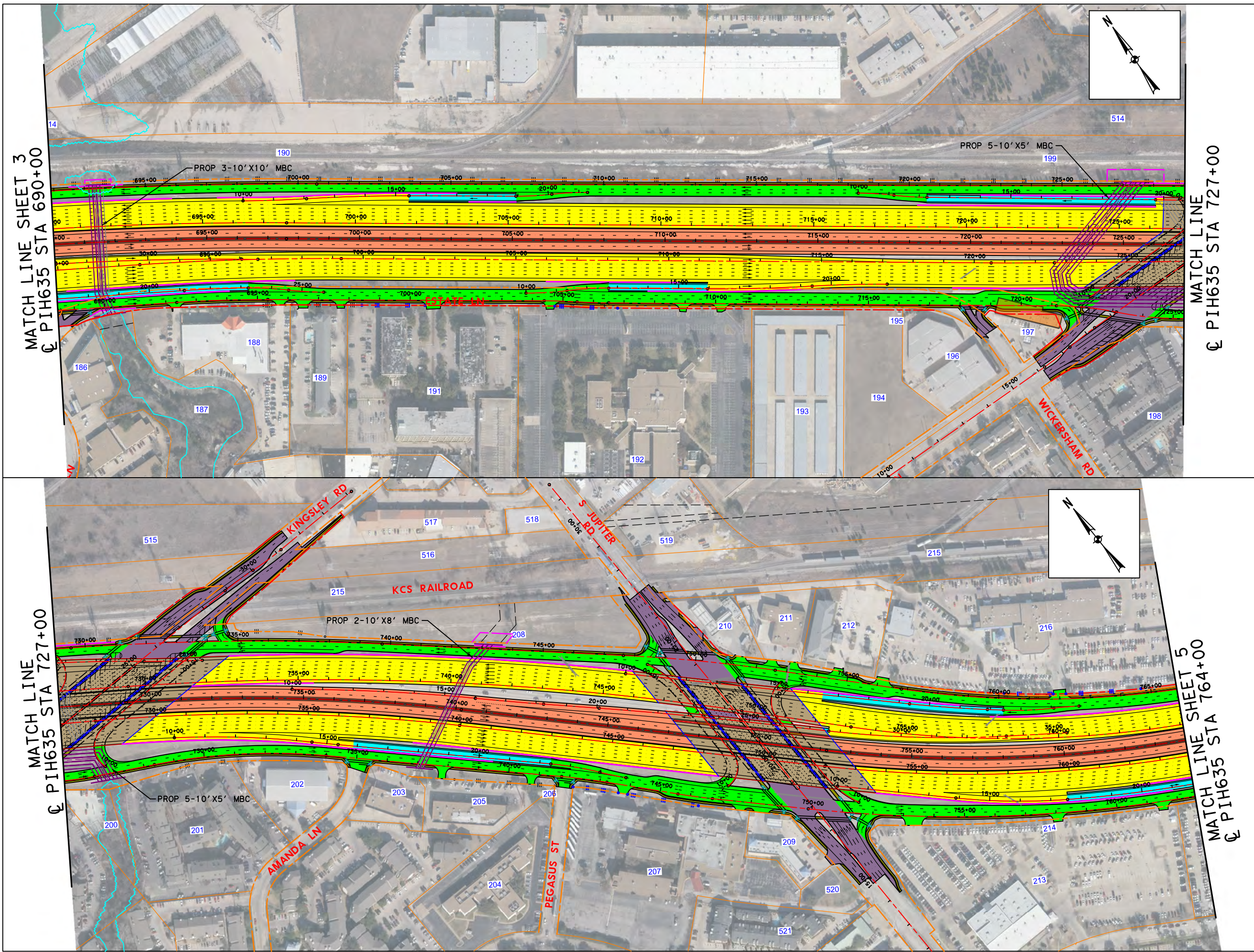
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PROJECT PLAN VIEW MAP

Page 3 OF 10

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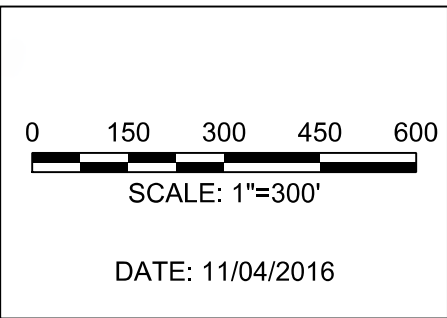
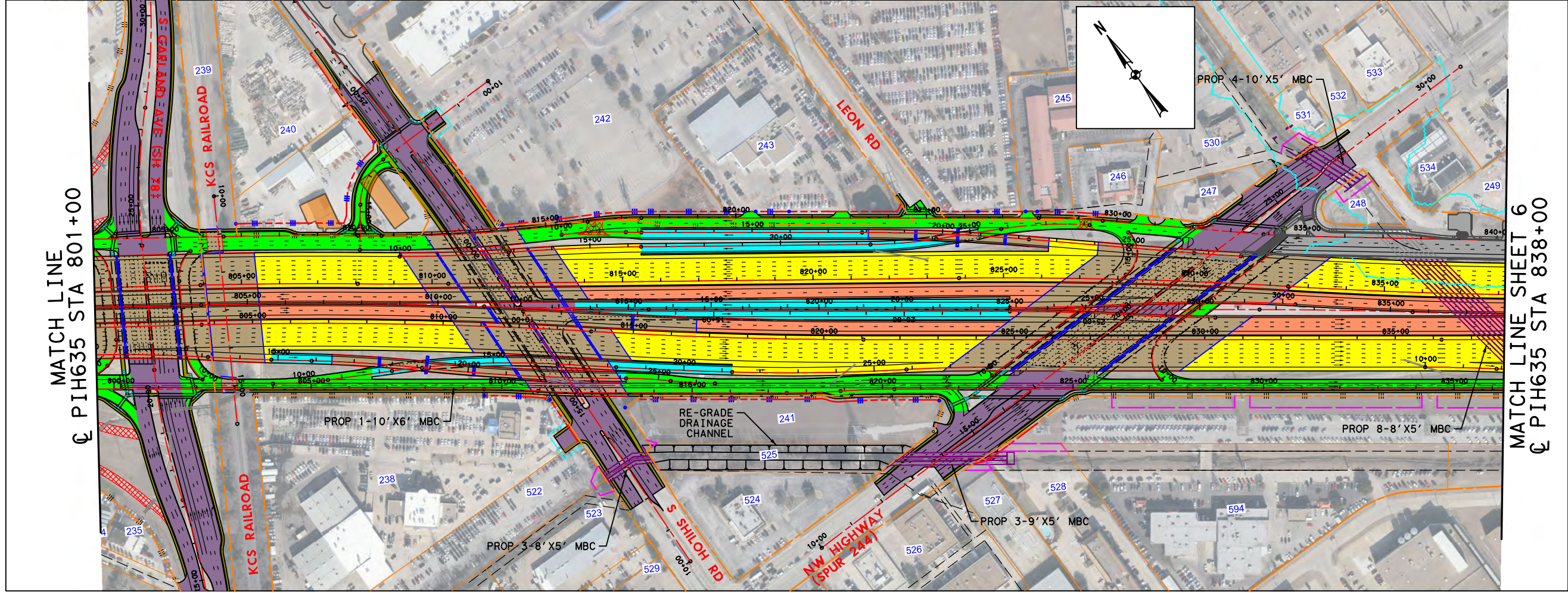
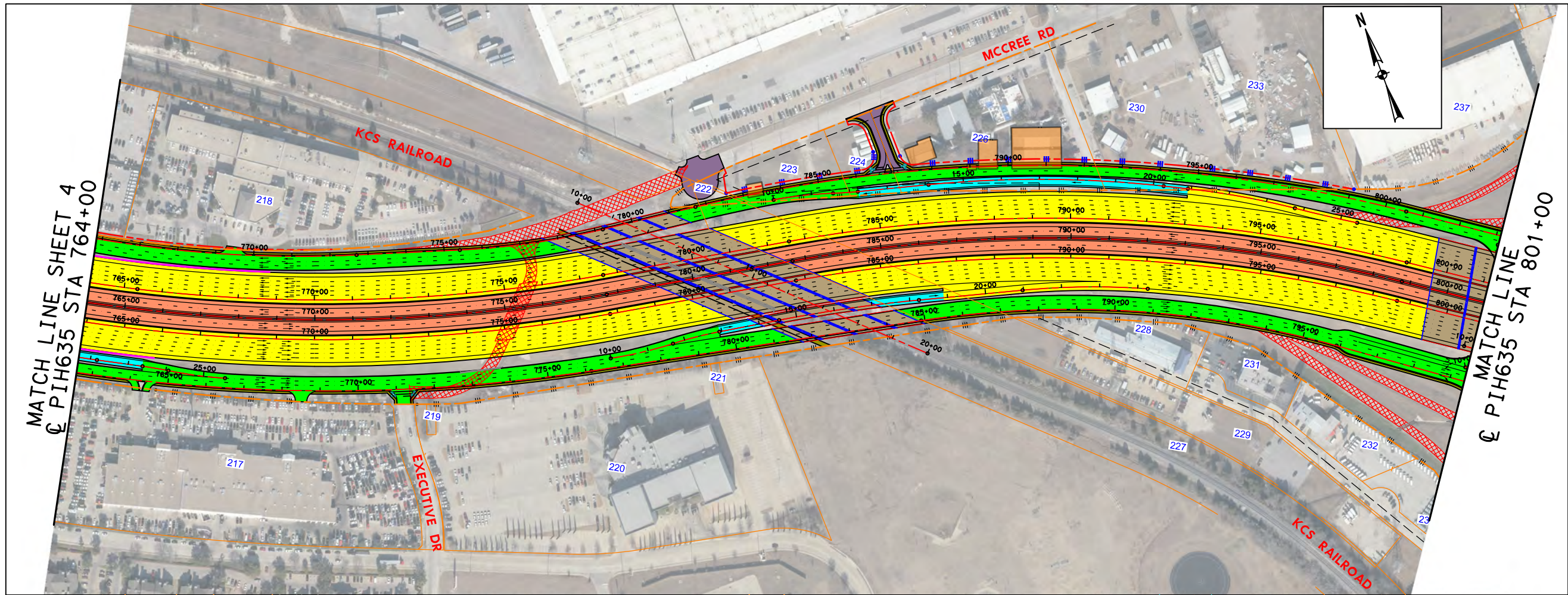
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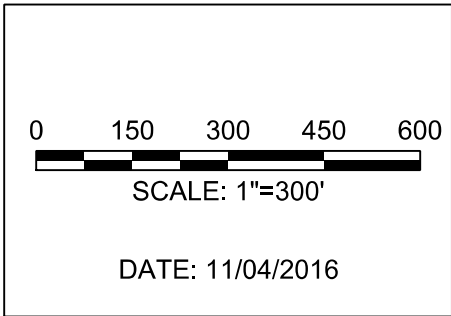
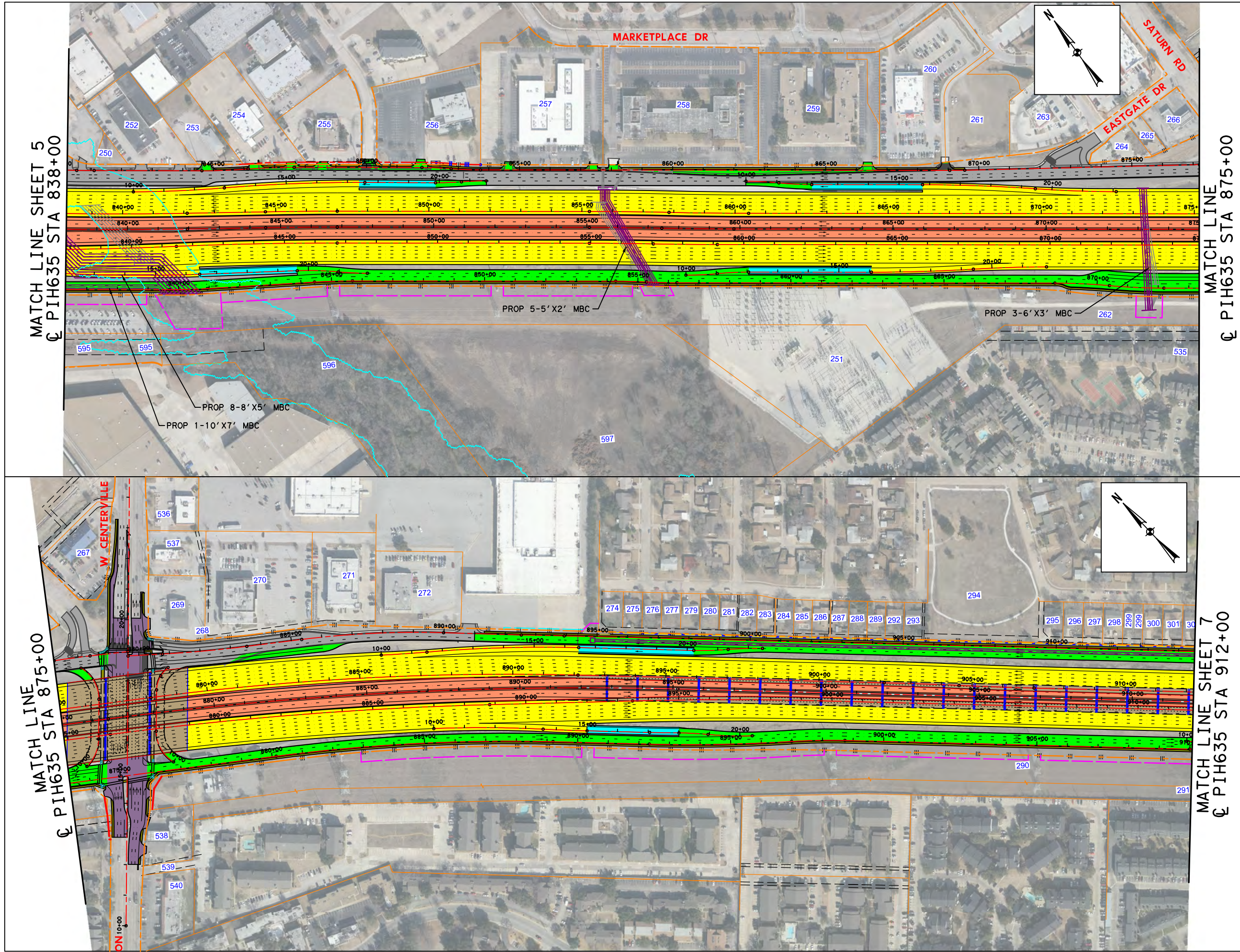
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LEGEND:

- EXISTING ROW
- EXISTING ACCESS DENIAL (TO REMAIN)
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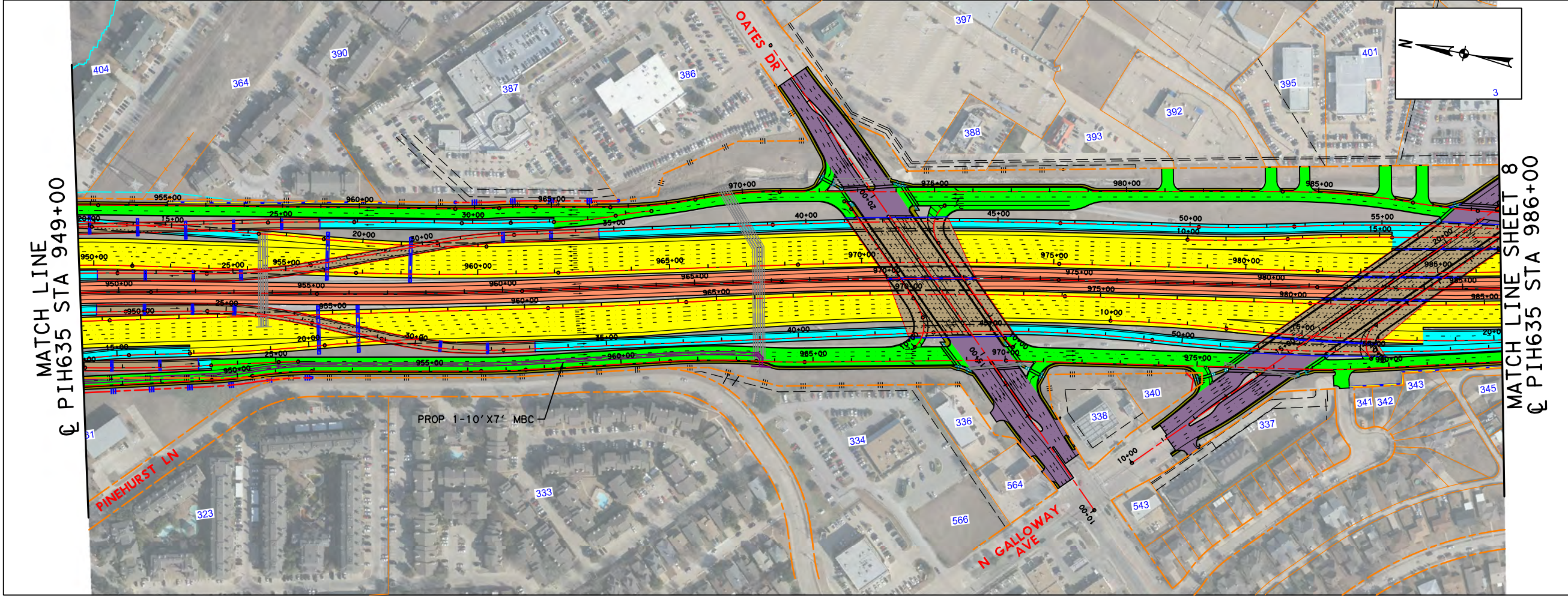
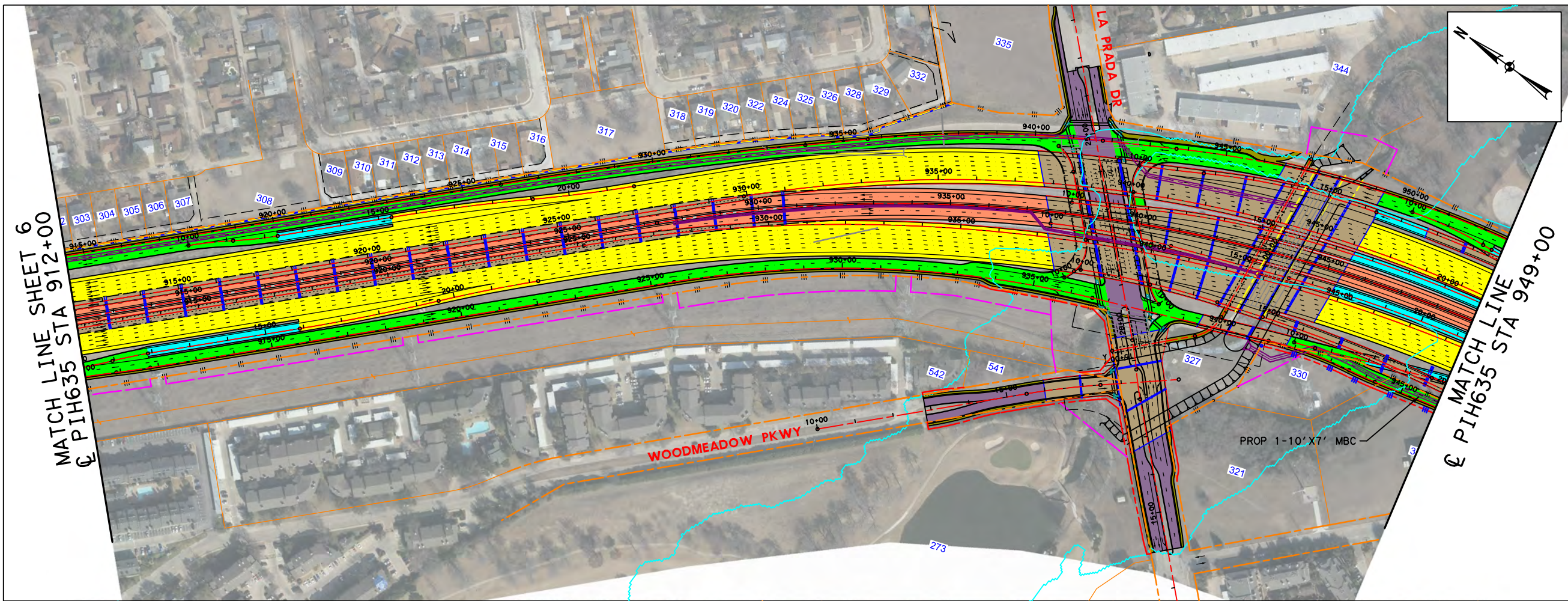
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0 150 300 450 600
SCALE: 1"=300'

DATE: 11/04/2016

LEGEND:

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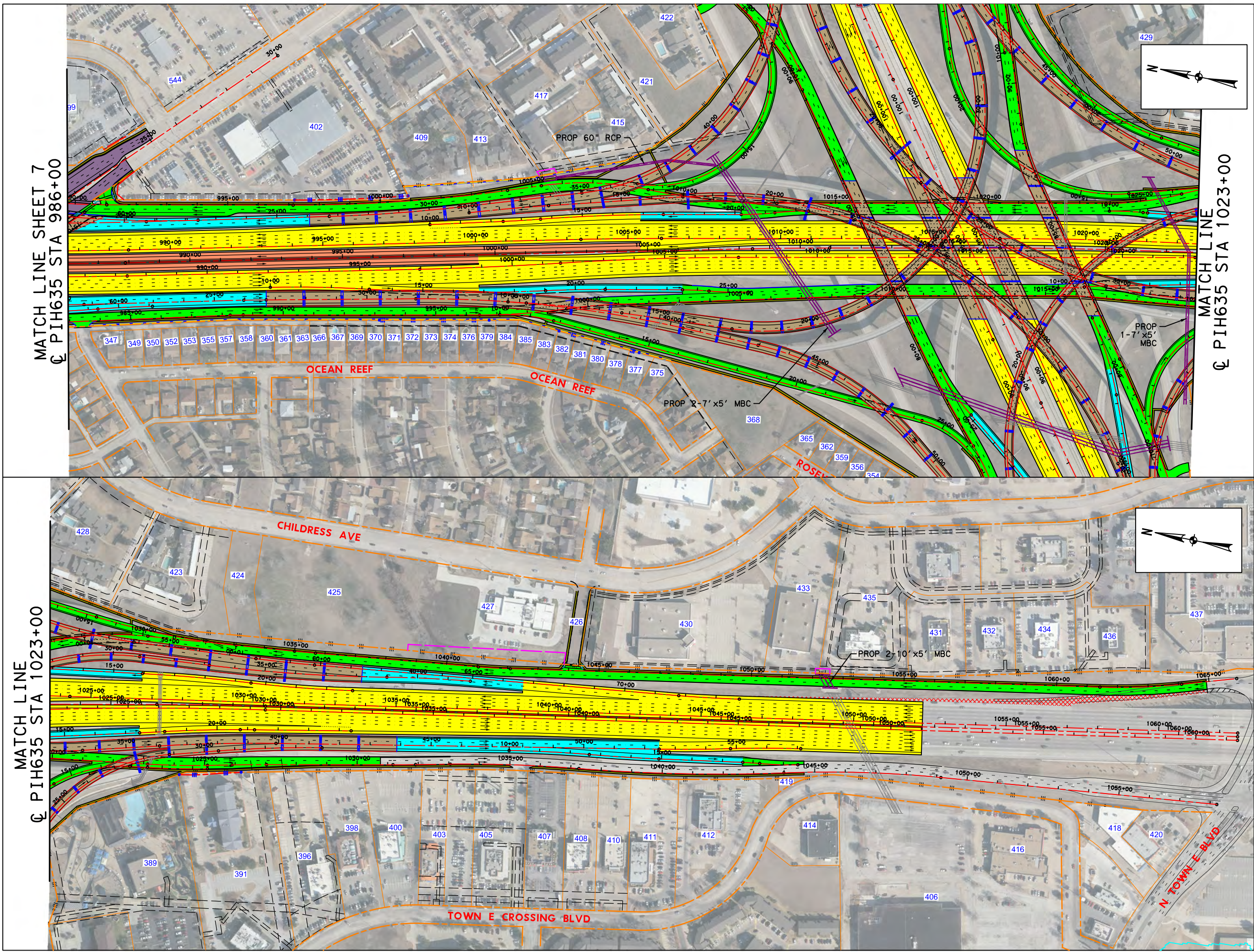
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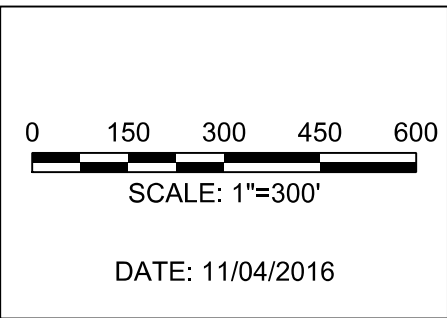
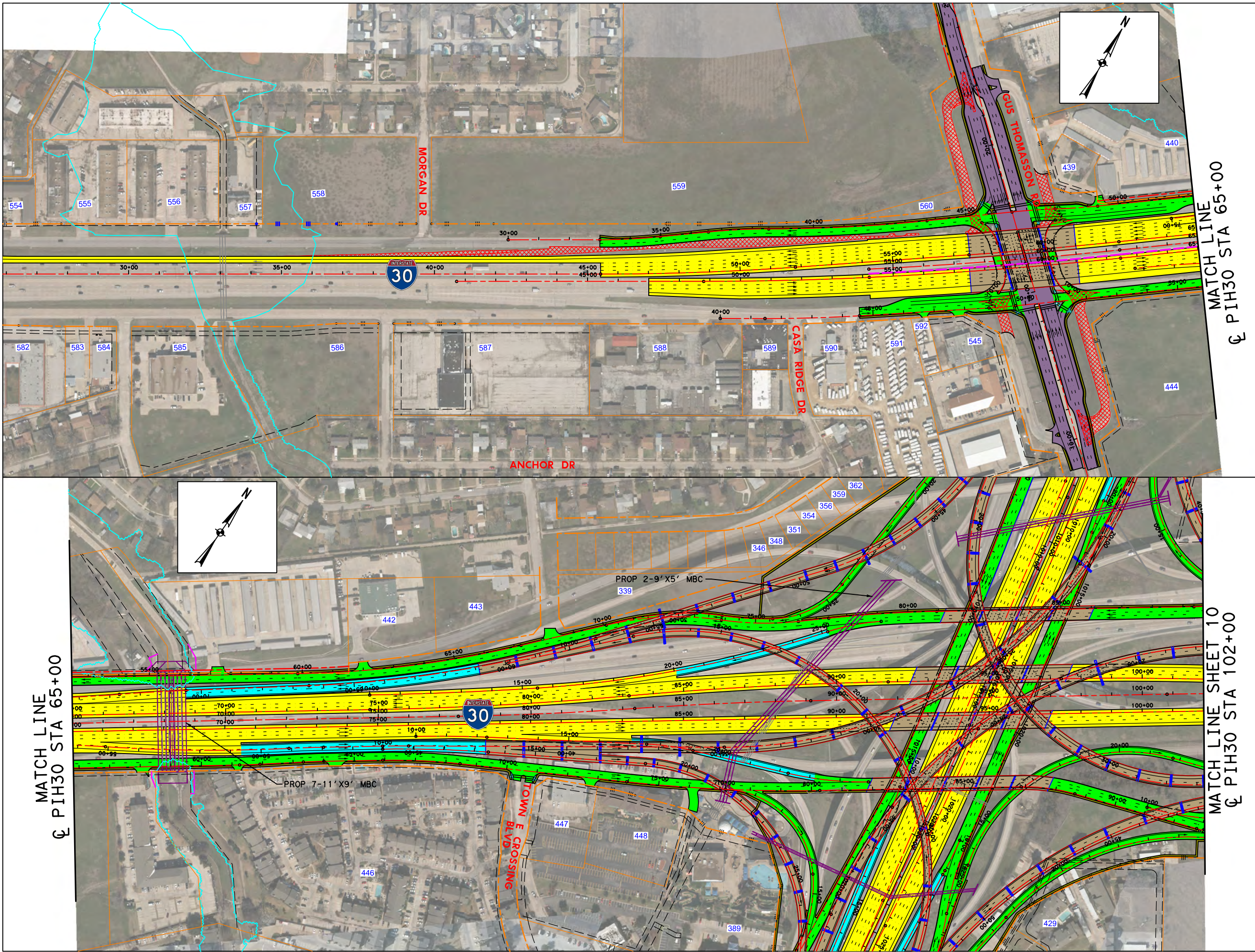
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PROJECT PLAN VIEW MAP

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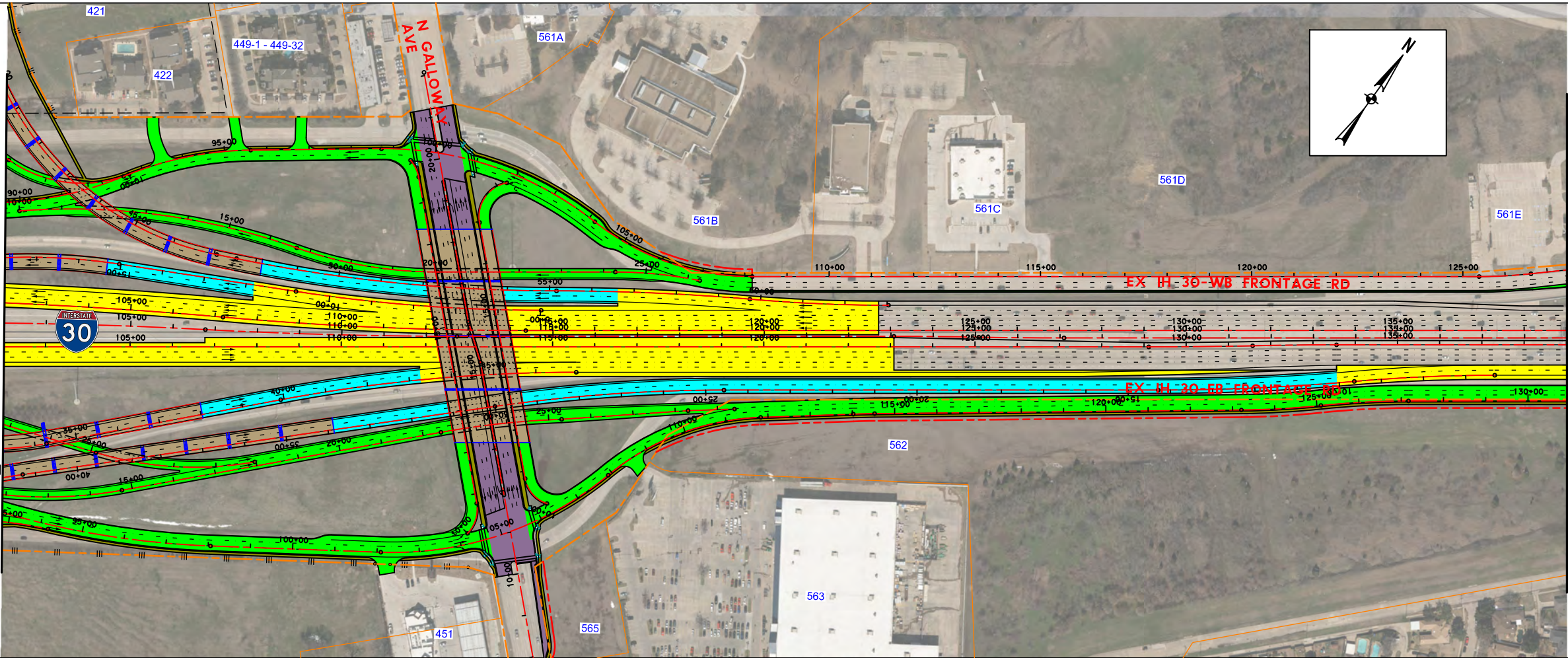
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PROJECT PLAN VIEW MAP

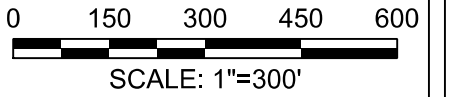
Page 9 OF 10

IH 635 FROM US 75 TO IH 30
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CSJs: 2374-01-137, 2374-01-180,
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MATCH LINE SHEET 9
℄ PIH30 STA 102+00



MATCH LINE
℄ PIH30 STA 139+00



DATE: 11/04/2016

LEGEND:

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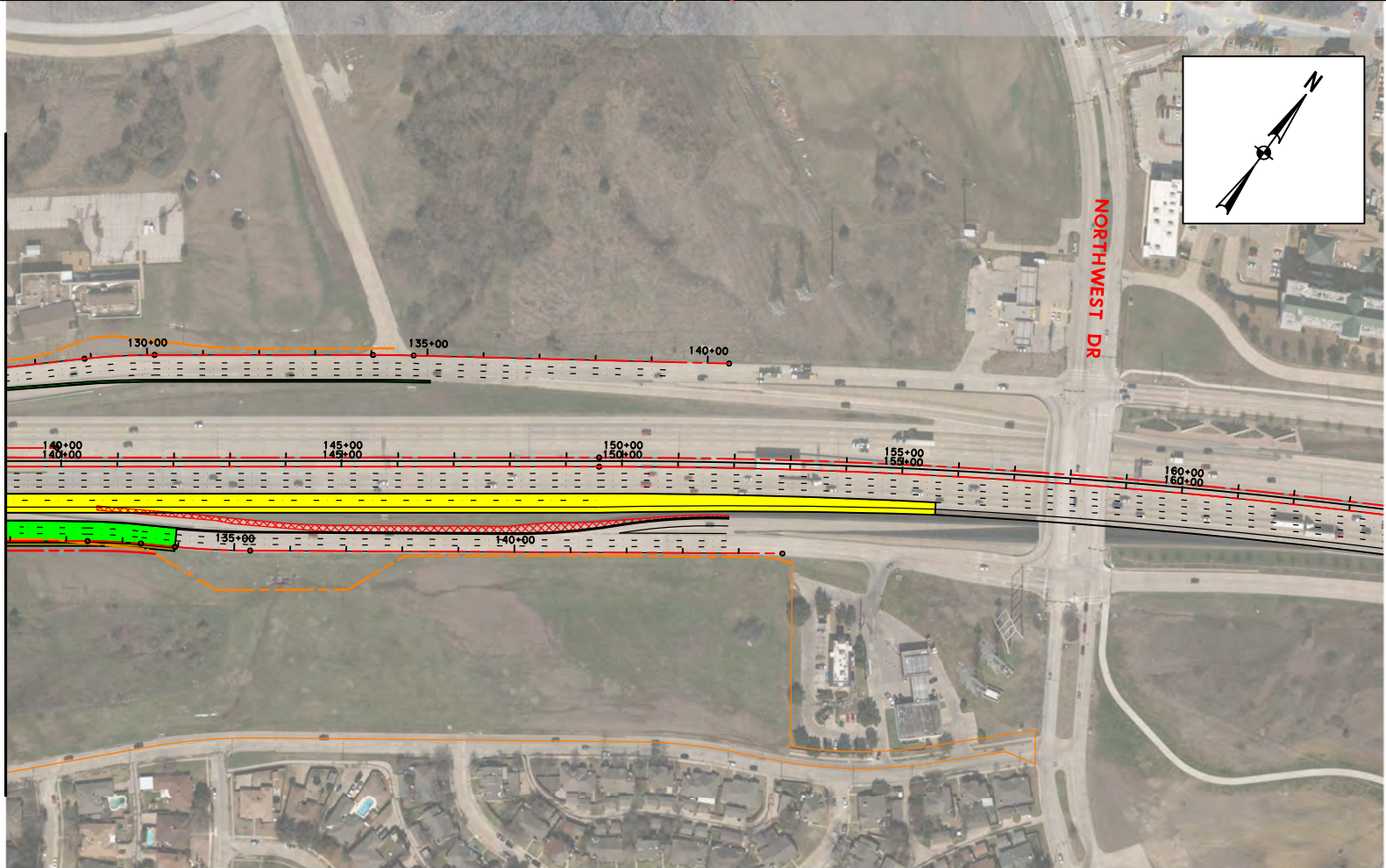
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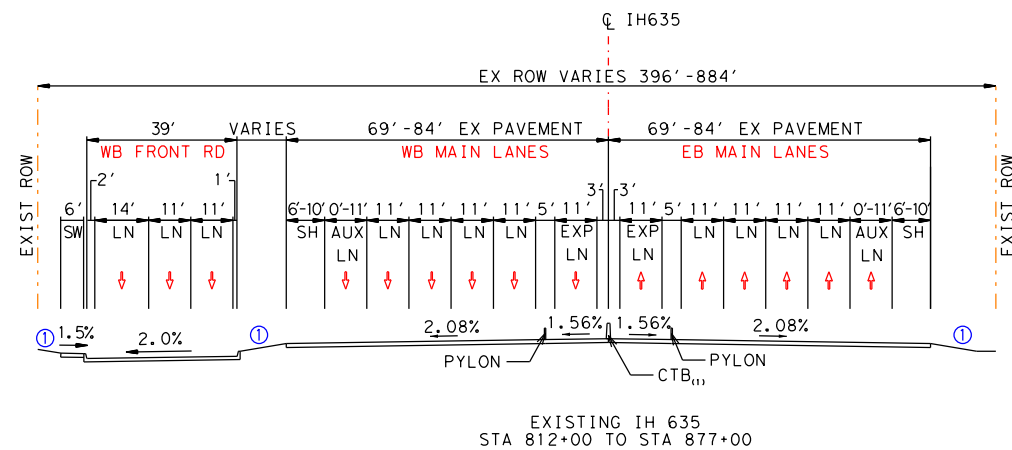
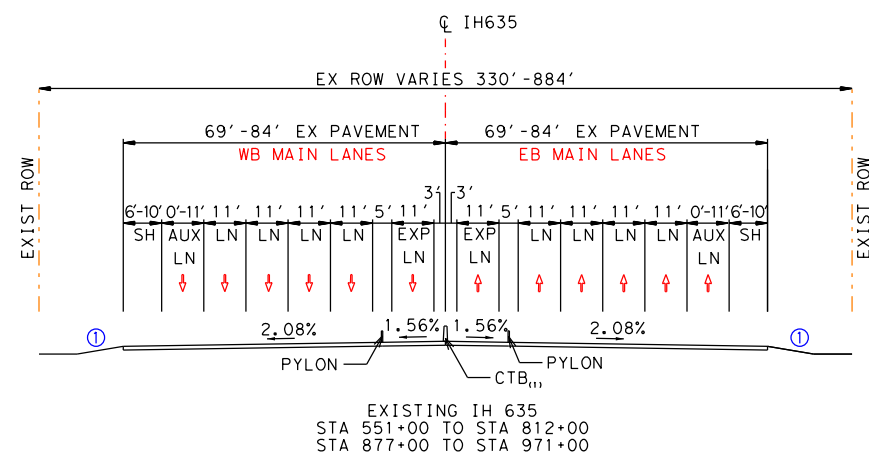
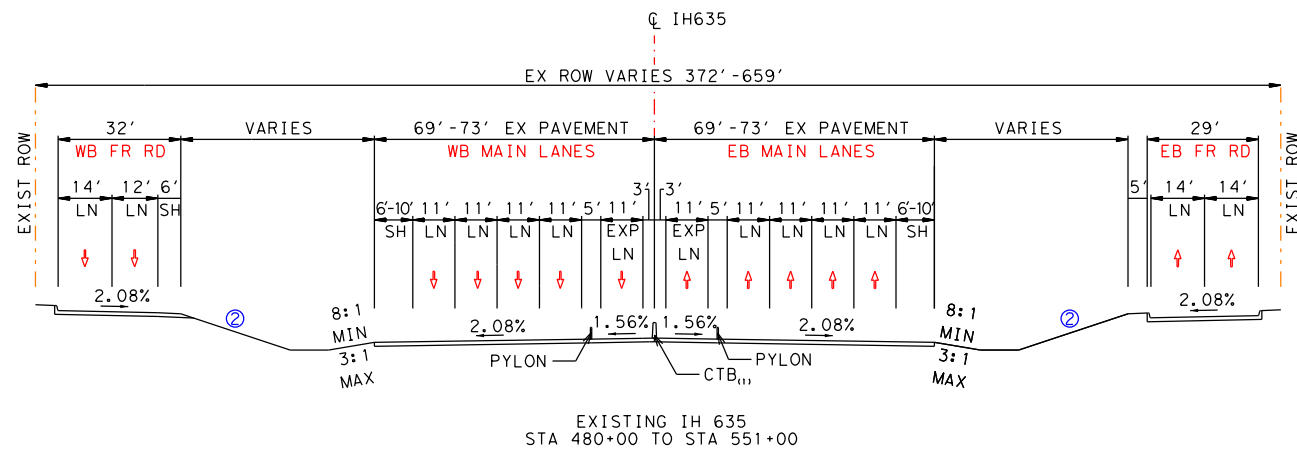
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PROJECT PLAN VIEW MAP
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MATCH LINE
℄ PIH30 STA 139+00

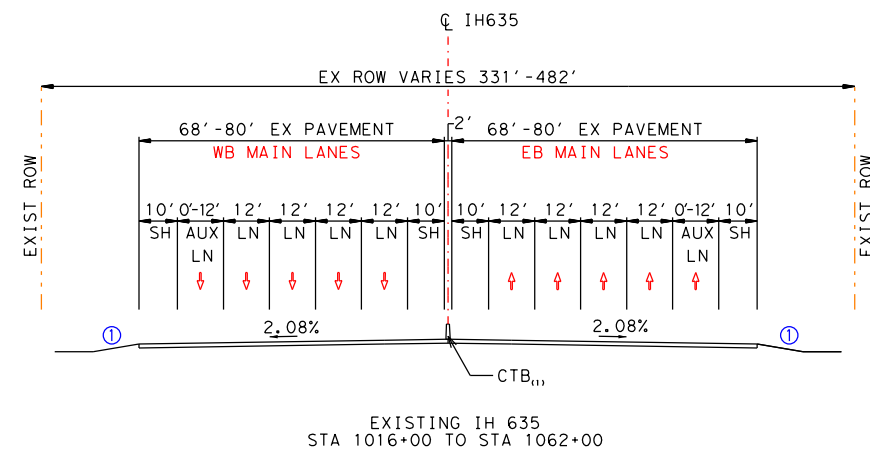
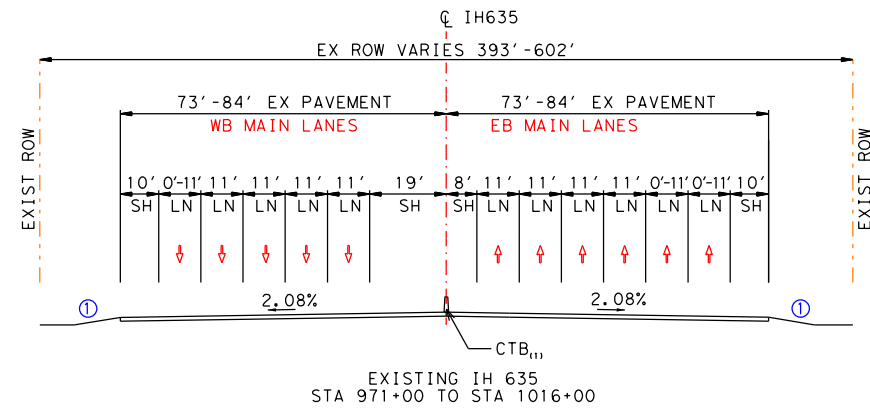




NOTES:

- ① 6:1 DESIRABLE
4:1 MAX
- ② 4:1 DESIRABLE
3:1 MAX
- ③ 4' DIAMETER BRIDGE COLUMN

- (1) CTB: CONCRETE TRAFFIC BARRIER
- (2) CTR: CONCRETE TRAFFIC RAIL
- (3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS



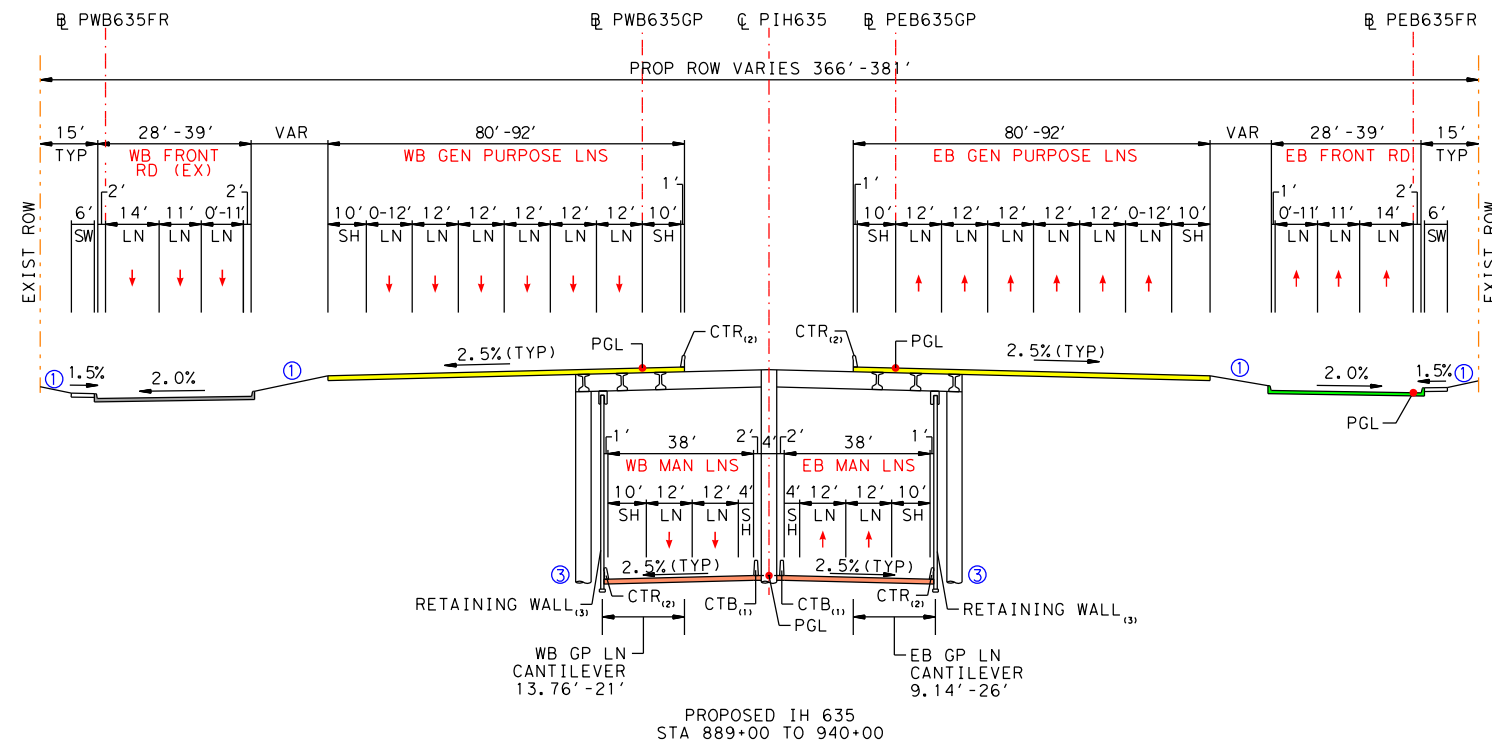
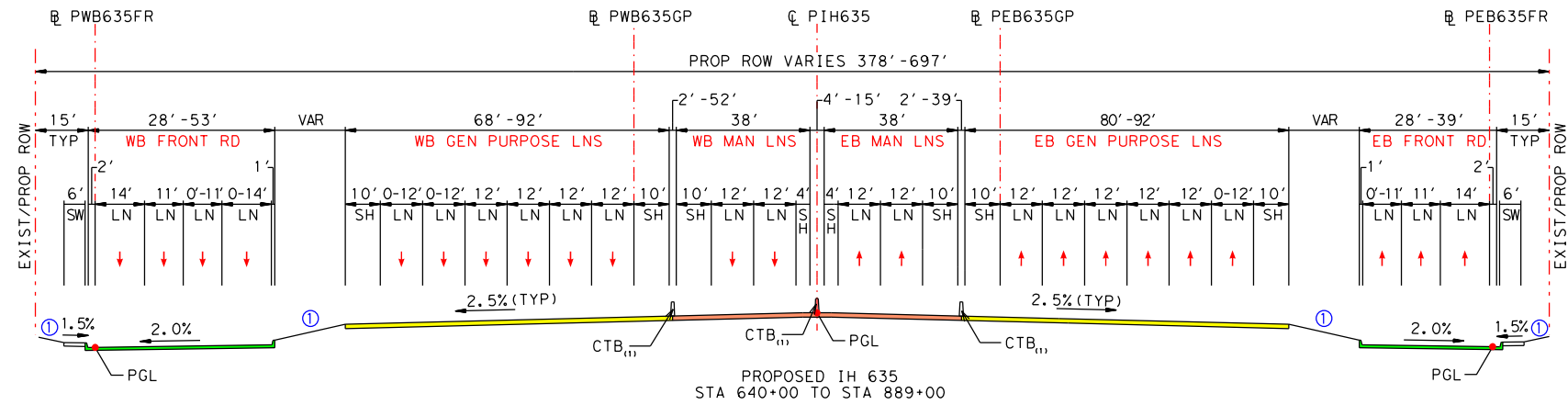
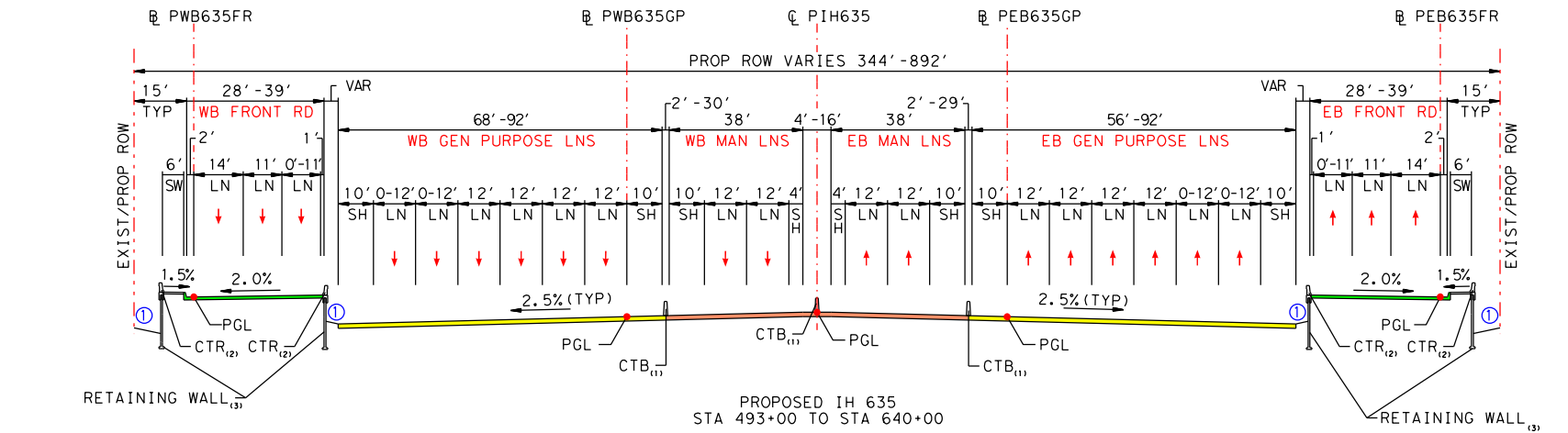
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PROJECT TYPICAL SECTIONS
PAGE 1 OF 8

IH 635 FROM US 75 TO IH 30
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EXISTING IH 635 TYPICAL SECTIONS



- NOTES:
- ① 6:1 DESIRABLE
4:1 MAX
 - ② 4:1 DESIRABLE
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 - ③ 4' DIAMETER BRIDGE COLUMN
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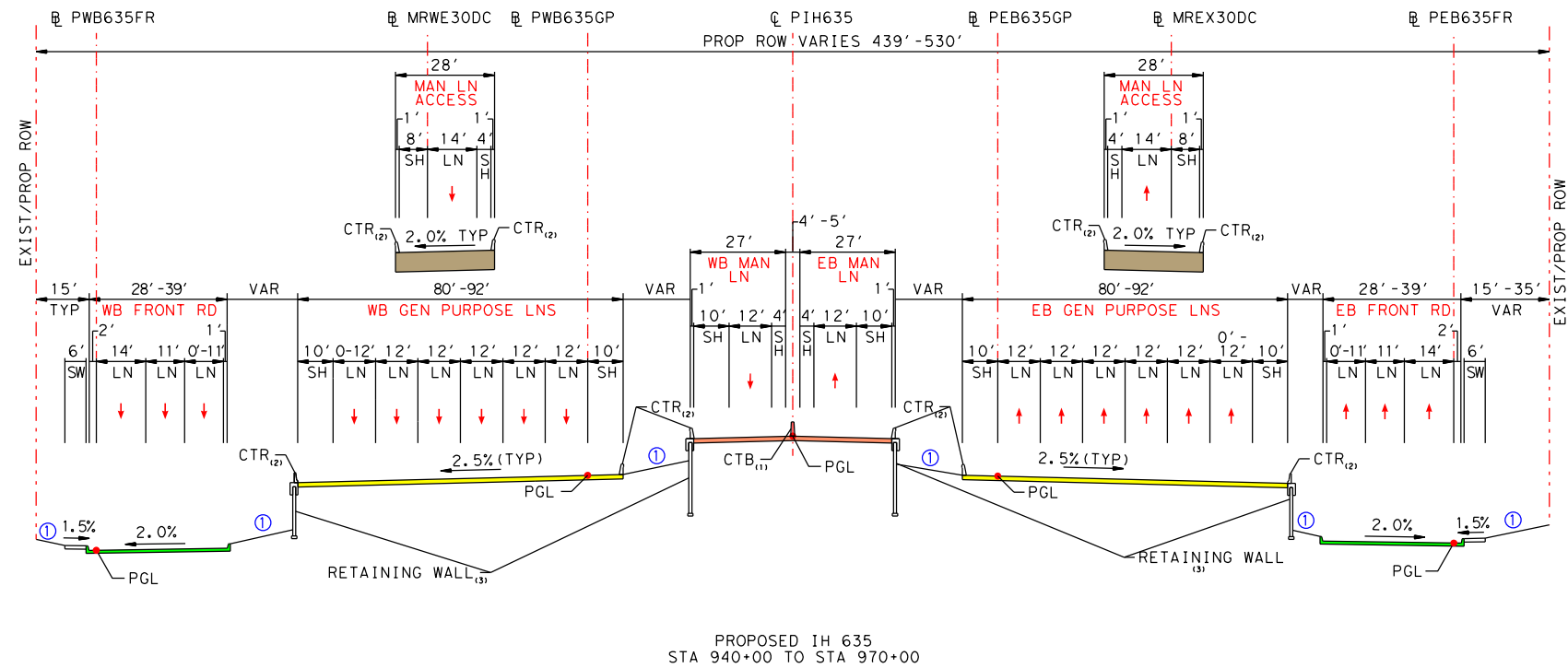
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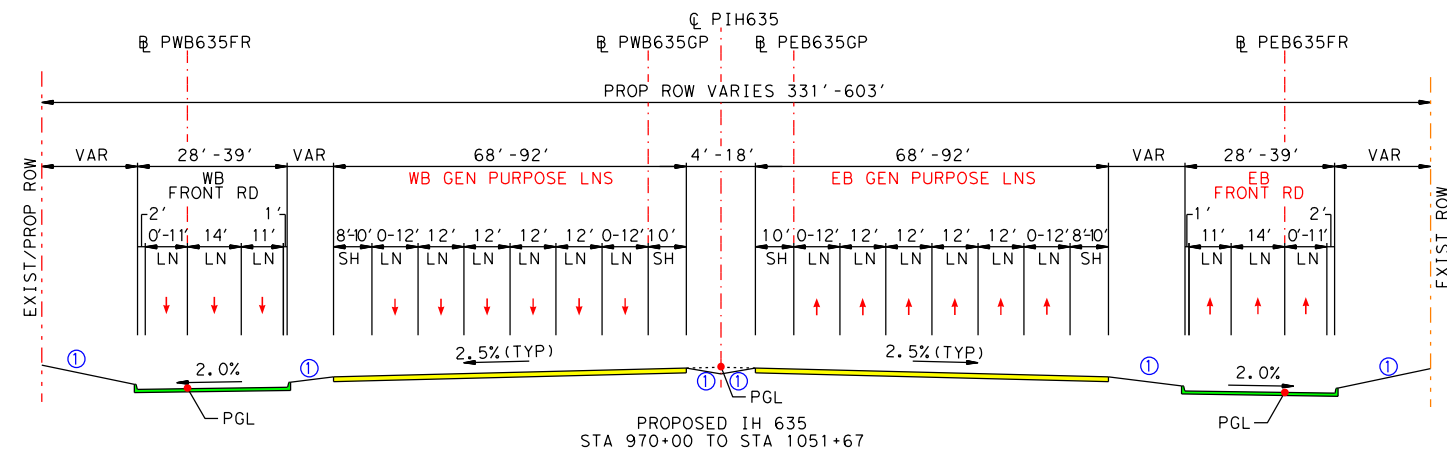
PROPOSED IH 635 TYPICAL SECTIONS



NOTES:

- ① 6:1 DESIRABLE
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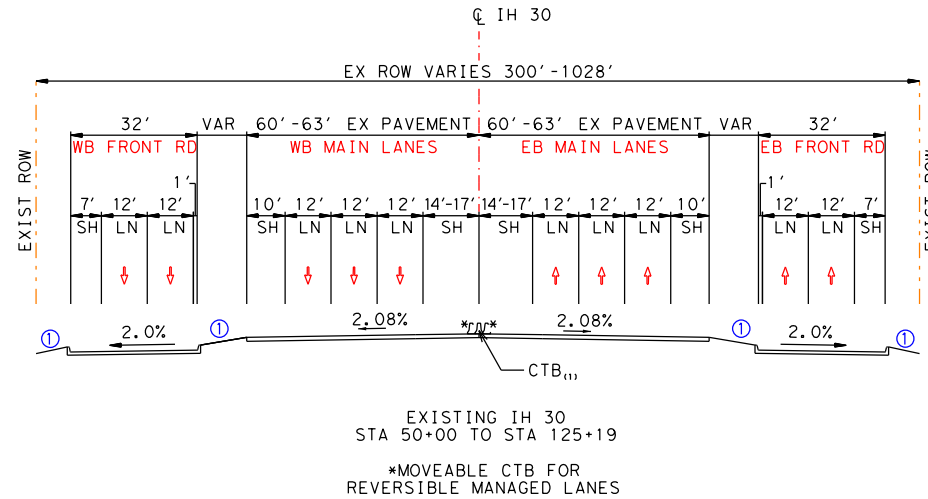
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PROJECT TYPICAL SECTIONS
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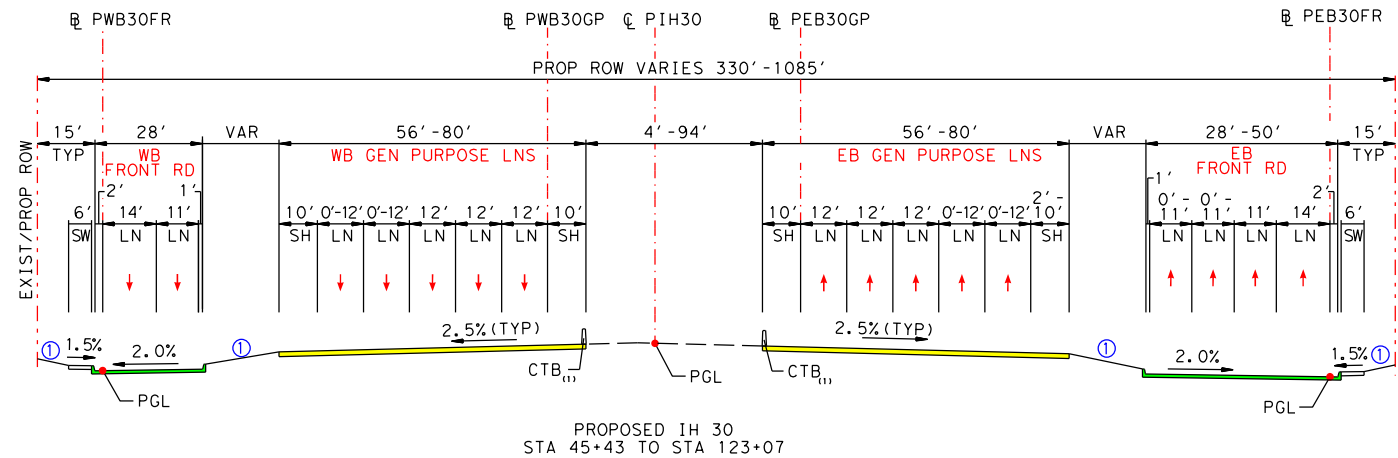
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EXISTING IH 30 TYPICAL SECTION



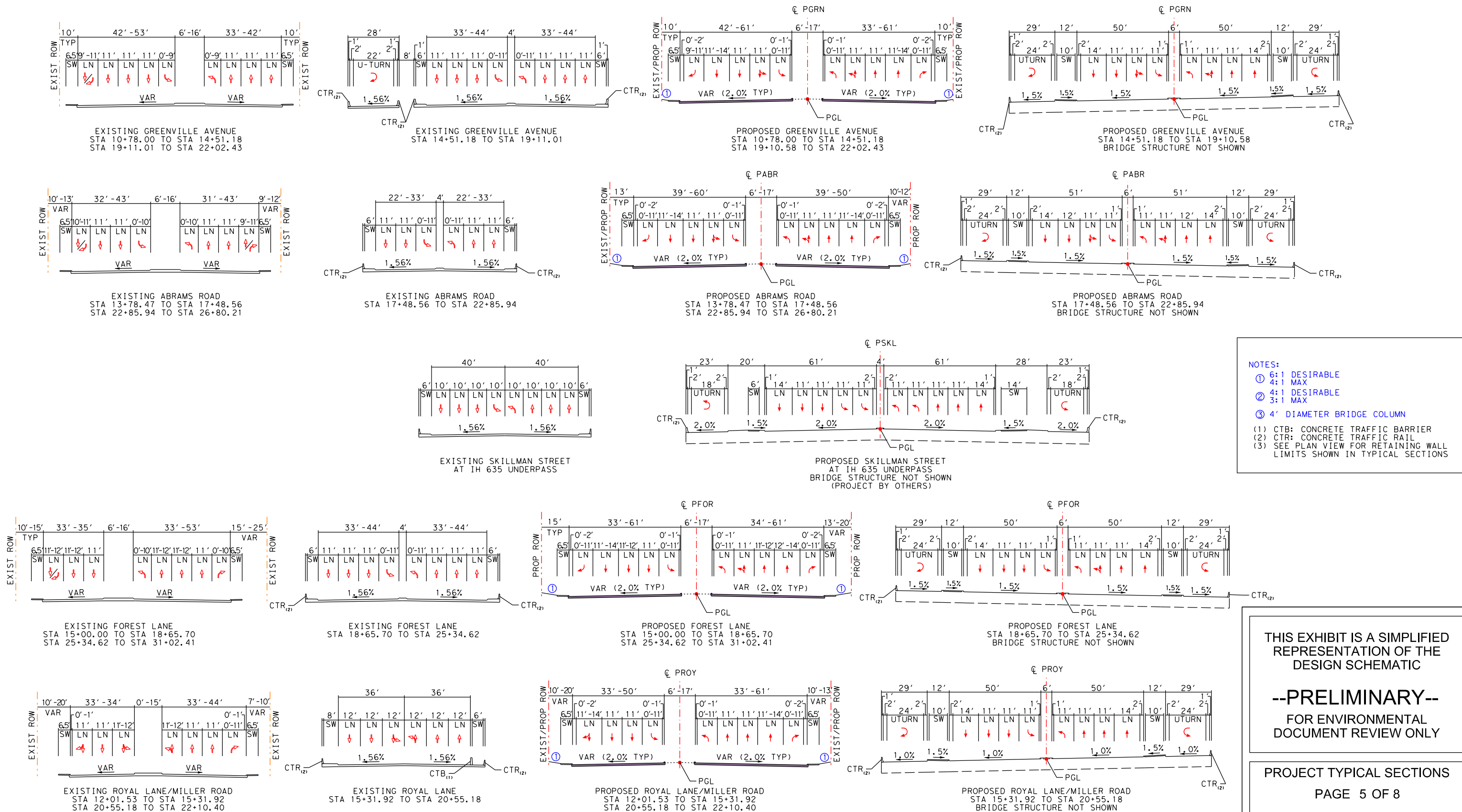
PROPOSED IH 30 TYPICAL SECTION

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NOTES:

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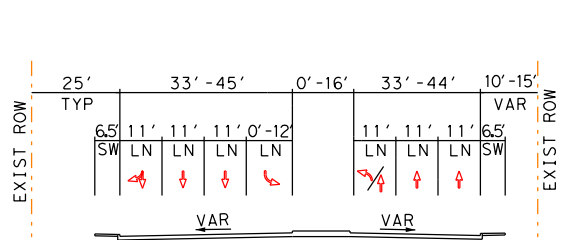
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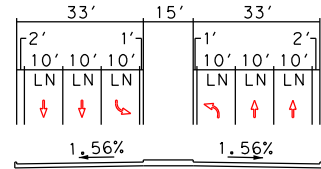
PROJECT TYPICAL SECTIONS
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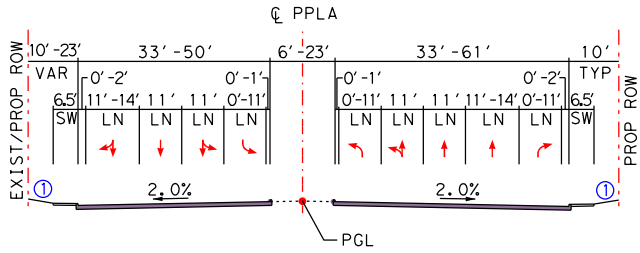
CROSS STREET TYPICAL SECTIONS



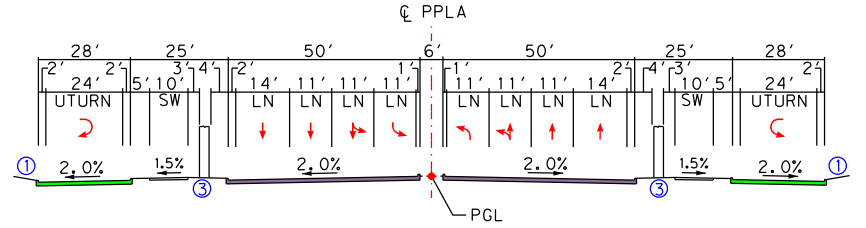
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STA 13+58.65 TO STA 15+95.38
STA 20+44.61 TO STA 21+50.00



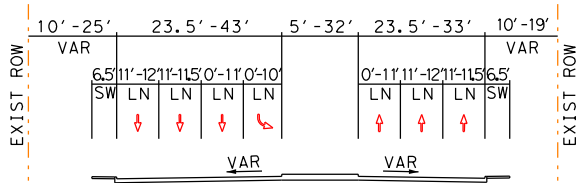
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STA 16+30.40 TO STA 20+44.61



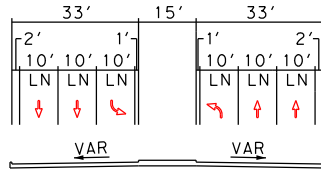
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STA 13+58.65 TO STA 15+95.38
STA 20+44.61 TO STA 21+50.00



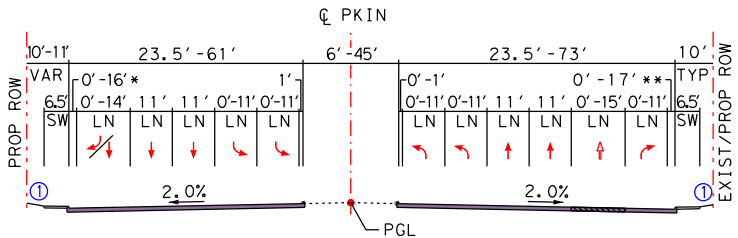
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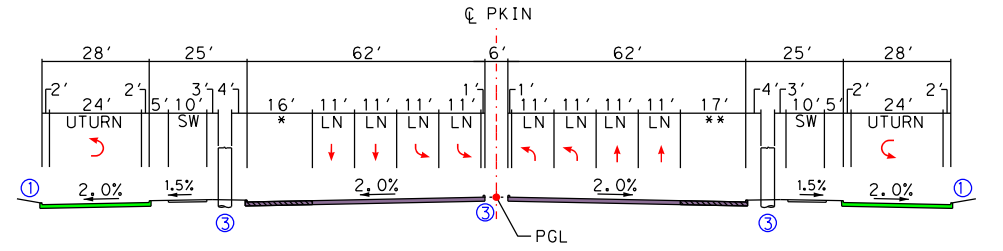
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STA 16+15.03 TO STA 19+21.08
STA 26+23.19 TO STA 33+45.79



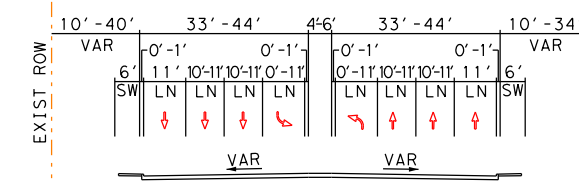
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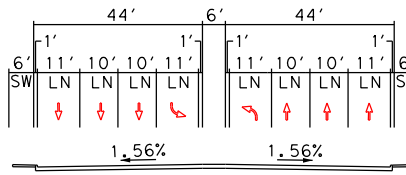
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STA 16+15.03 TO STA 19+21.08
STA 26+23.19 TO STA 33+45.79
*OUTSIDE 16' LANE STRIPED OFF FOR FUTURE USE
**OUTSIDE 17' LANE STRIPED OFF FOR FUTURE USE



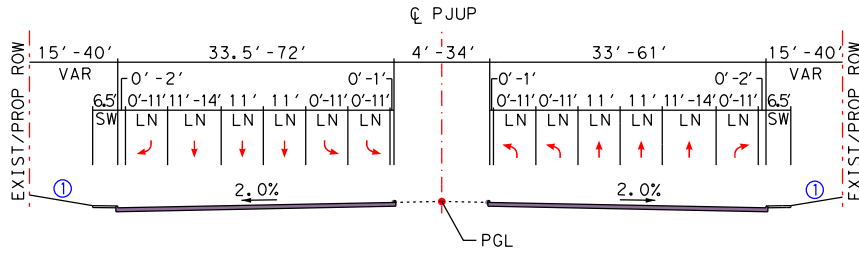
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**OUTSIDE 17' LANE STRIPED OFF FOR FUTURE USE



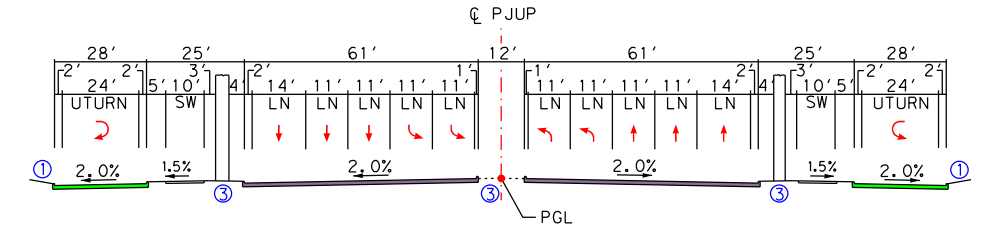
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STA 14+17.66 TO STA 17+83.92
STA 24+15.56 TO STA 27+00.18



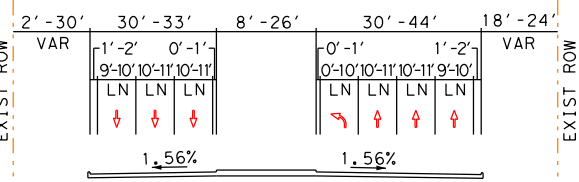
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STA 17+83.92 TO STA 24+15.56



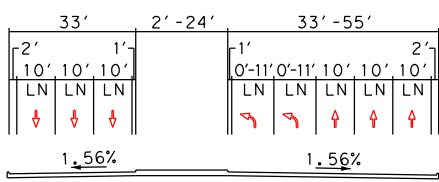
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STA 24+15.56 TO STA 27+00.18



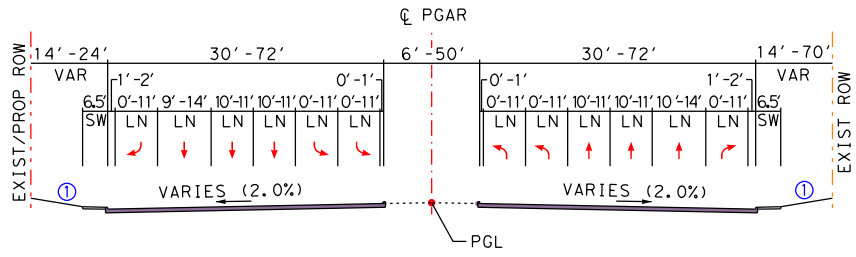
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STA 17+83.92 TO STA 24+15.56



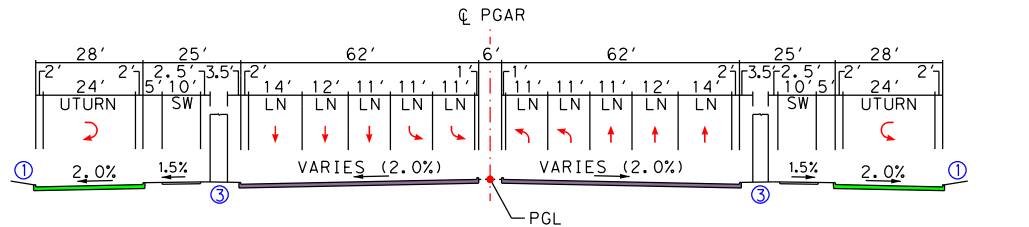
EXISTING GARLAND AVENUE
STA 10+74.16 TO STA 20+17.03
STA 24+14.64 TO STA 32+26.49



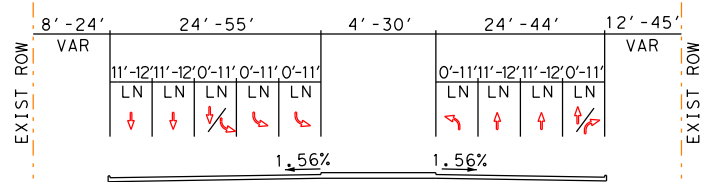
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STA 20+17.03 TO STA 24+14.64



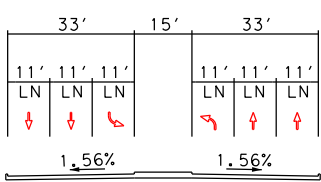
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STA 10+74.16 TO STA 20+17.03
STA 24+14.64 TO STA 32+26.49



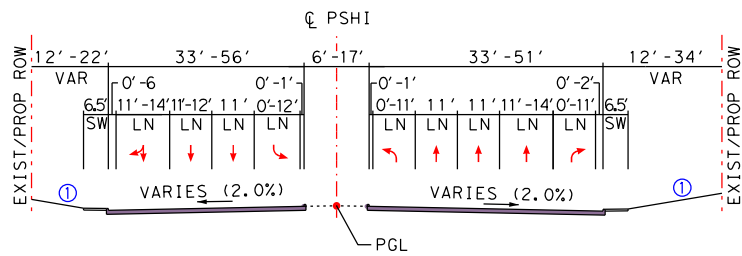
PROPOSED GARLAND AVENUE
STA 20+17.03 TO STA 24+14.64



EXISTING SHILOH ROAD
STA 12+00.00 TO STA 14+50.69
STA 20+84.10 TO STA 26+19.86



EXISTING SHILOH ROAD
STA 14+50.69 TO STA 20+84.10



PROPOSED SHILOH ROAD
STA 12+00.00 TO STA 26+19.86

- NOTES:
- ① 6:1 DESIRABLE
 - ② 4:1 MAX
 - ③ 4:1 DESIRABLE
 - ④ 3:1 MAX
 - ⑤ 4' DIAMETER BRIDGE COLUMN

- (1) CTB: CONCRETE TRAFFIC BARRIER
- (2) CTR: CONCRETE TRAFFIC RAIL
- (3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

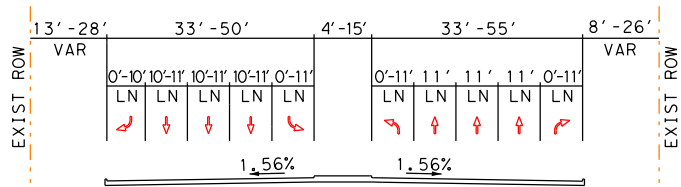
CROSS STREET TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

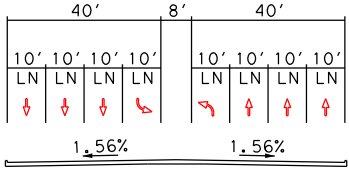
--PRELIMINARY--
FOR ENVIRONMENTAL DOCUMENT REVIEW ONLY

PROJECT TYPICAL SECTIONS
PAGE 6 OF 8

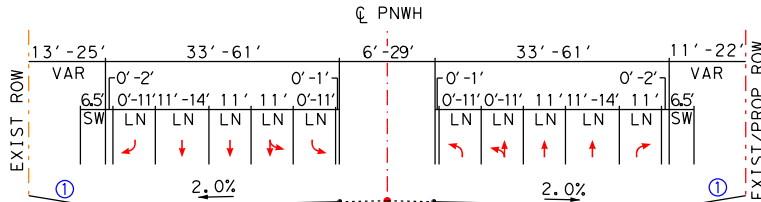
IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



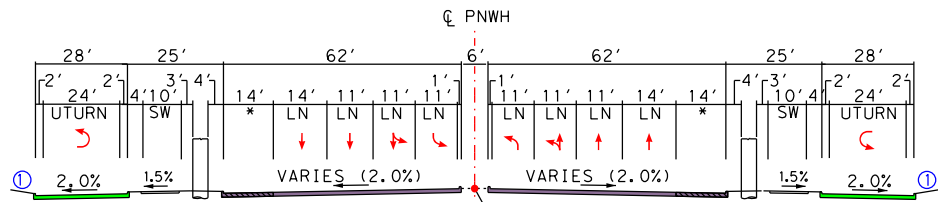
EXISTING NORTHWEST HIGHWAY
STA 12+13.61 TO STA 16+92.14
STA 23+75.96 TO STA 27+23.74



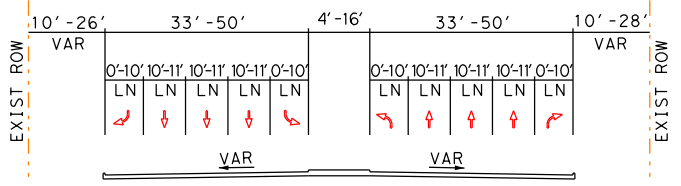
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STA 16+92.14 TO STA 23+75.96



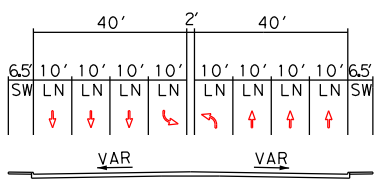
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STA 12+13.61 TO STA 16+92.14
STA 23+75.96 TO STA 27+23.74



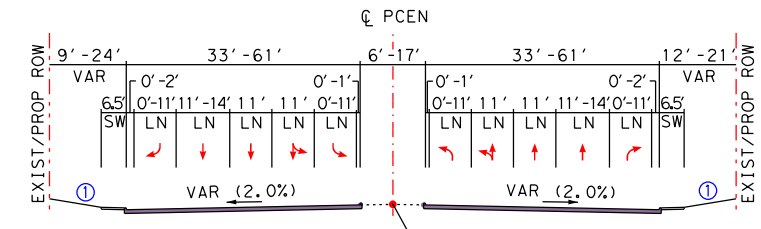
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STA 16+92.14 TO STA 23+75.96
*OUTSIDE 14' LANE STRIPED OFF FOR FUTURE USE



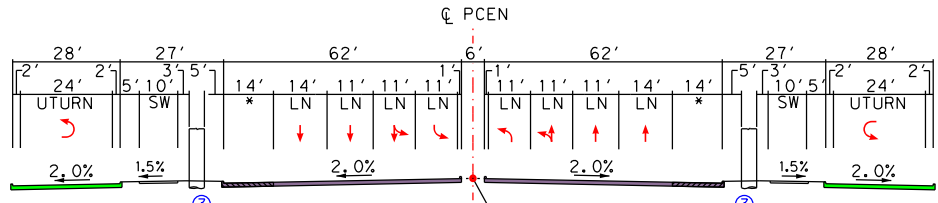
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STA 11+83.62 TO STA 14+90.34
STA 18+81.47 TO STA 23+10.07



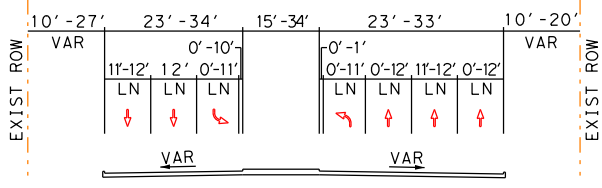
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STA 14+90.34 TO STA 18+81.47



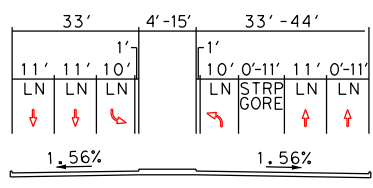
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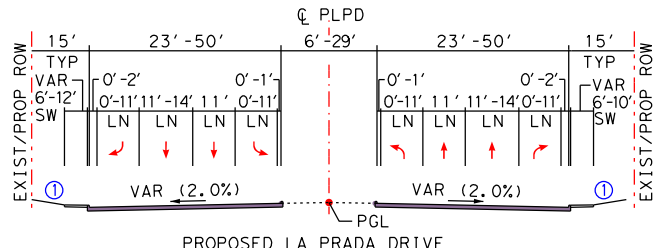
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STA 14+90.34 TO STA 18+81.47
*OUTSIDE 14' LANE STRIPED OFF FOR FUTURE USE



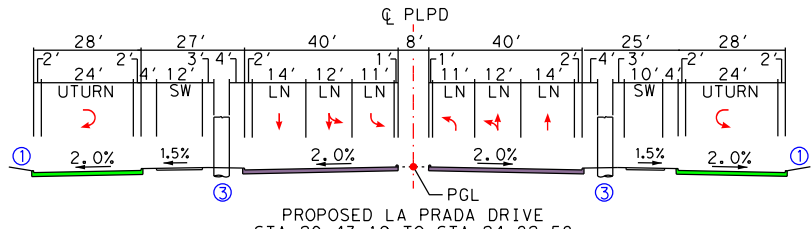
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STA 11+02.14 TO STA 20+43.19
STA 28+82.58 TO STA 28+42.63



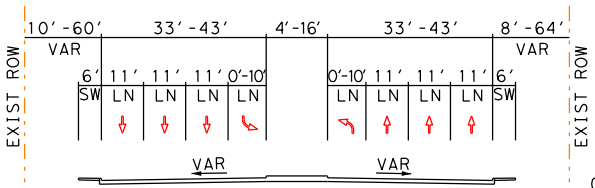
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STA 20+43.19 TO STA 24+82.58



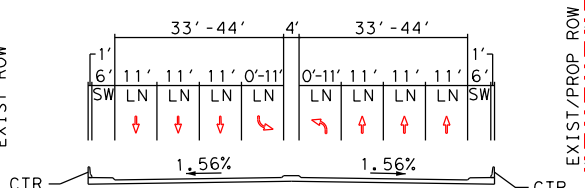
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STA 14+00.00 TO STA 20+43.19
STA 24+82.58 TO STA 28+42.63



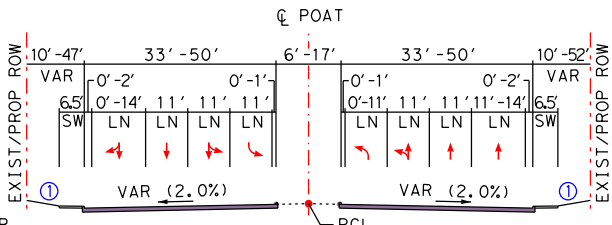
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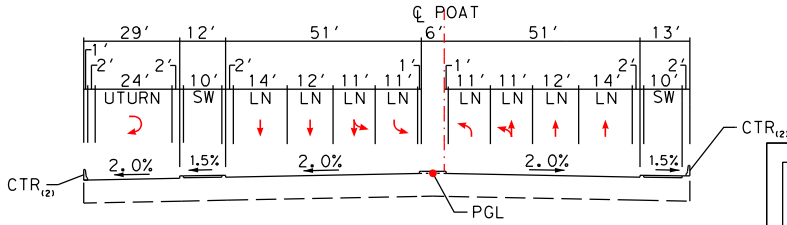
EXISTING OATES DRIVE
STA 11+00.00 TO STA 14+75.77
STA 20+28.80 TO STA 23+90.67



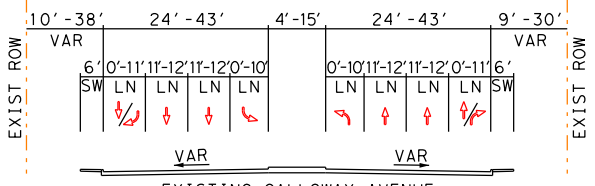
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STA 14+75.77 TO STA 20+28.80



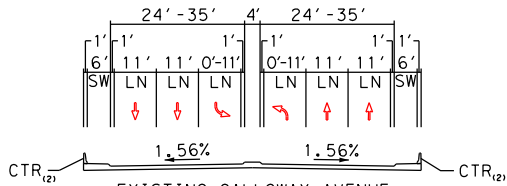
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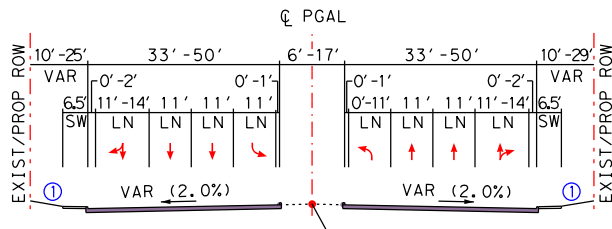
PROPOSED OATES DRIVE
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BRIDGE STRUCTURE NOT SHOWN



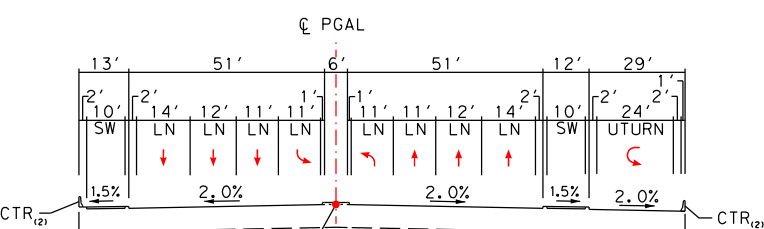
EXISTING GALLOWAY AVENUE
STA 10+96.20 TO STA 14+74.69
STA 22+47.20 TO STA 28+14.95



EXISTING GALLOWAY AVENUE
STA 14+74.69 TO STA 22+47.20



PROPOSED GALLOWAY AVENUE
STA 10+96.20 TO STA 14+74.69
STA 22+47.20 TO STA 28+14.95



PROPOSED GALLOWAY AVENUE
STA 14+74.69 TO STA 22+47.20
BRIDGE STRUCTURE NOT SHOWN

- NOTES:
- ① 6:1 DESIRABLE
 - ② 4:1 MAX
 - ③ 4:1 DESIRABLE
 - ④ 3:1 MAX
 - ⑤ 4' DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

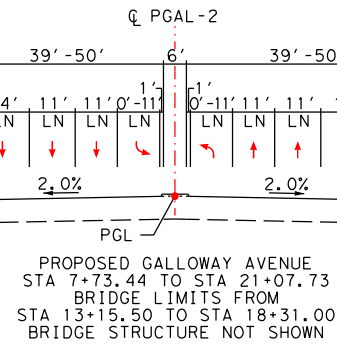
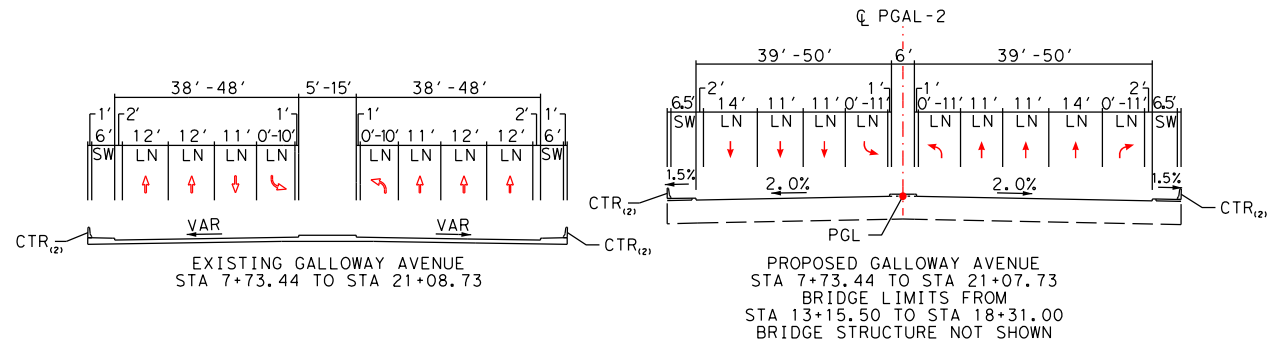
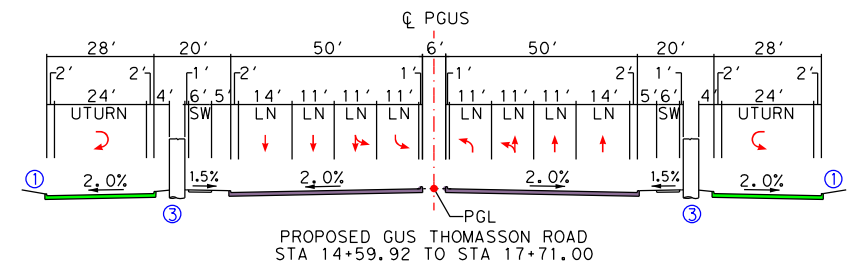
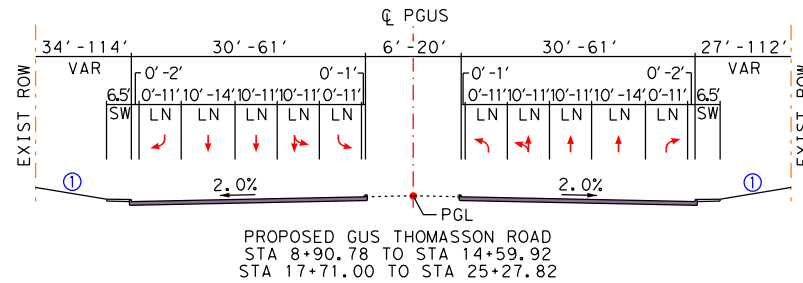
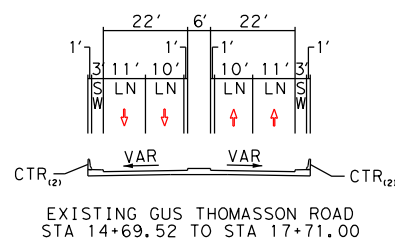
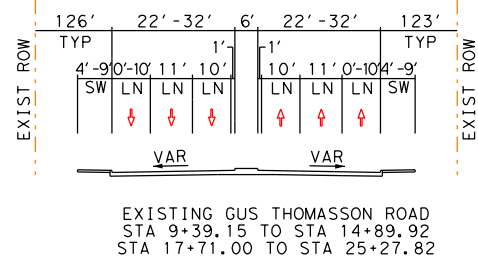
CROSS STREET TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

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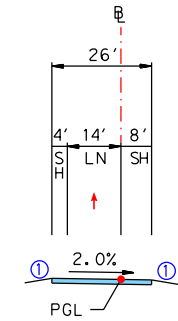
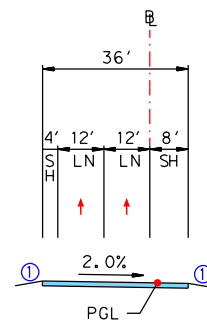
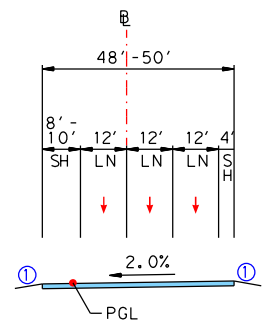
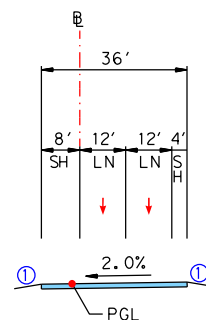
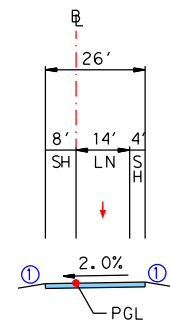
PROJECT TYPICAL SECTIONS
PAGE 7 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



CROSS STREET TYPICAL SECTIONS

- NOTES:
- ① 6:1 DESIRABLE
4:1 MAX
 - ② 4:1 DESIRABLE
3:1 MAX
 - ③ 4' DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL
LIMITS SHOWN IN TYPICAL SECTIONS



PROPOSED RAMP TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED
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DESIGN SCHEMATIC

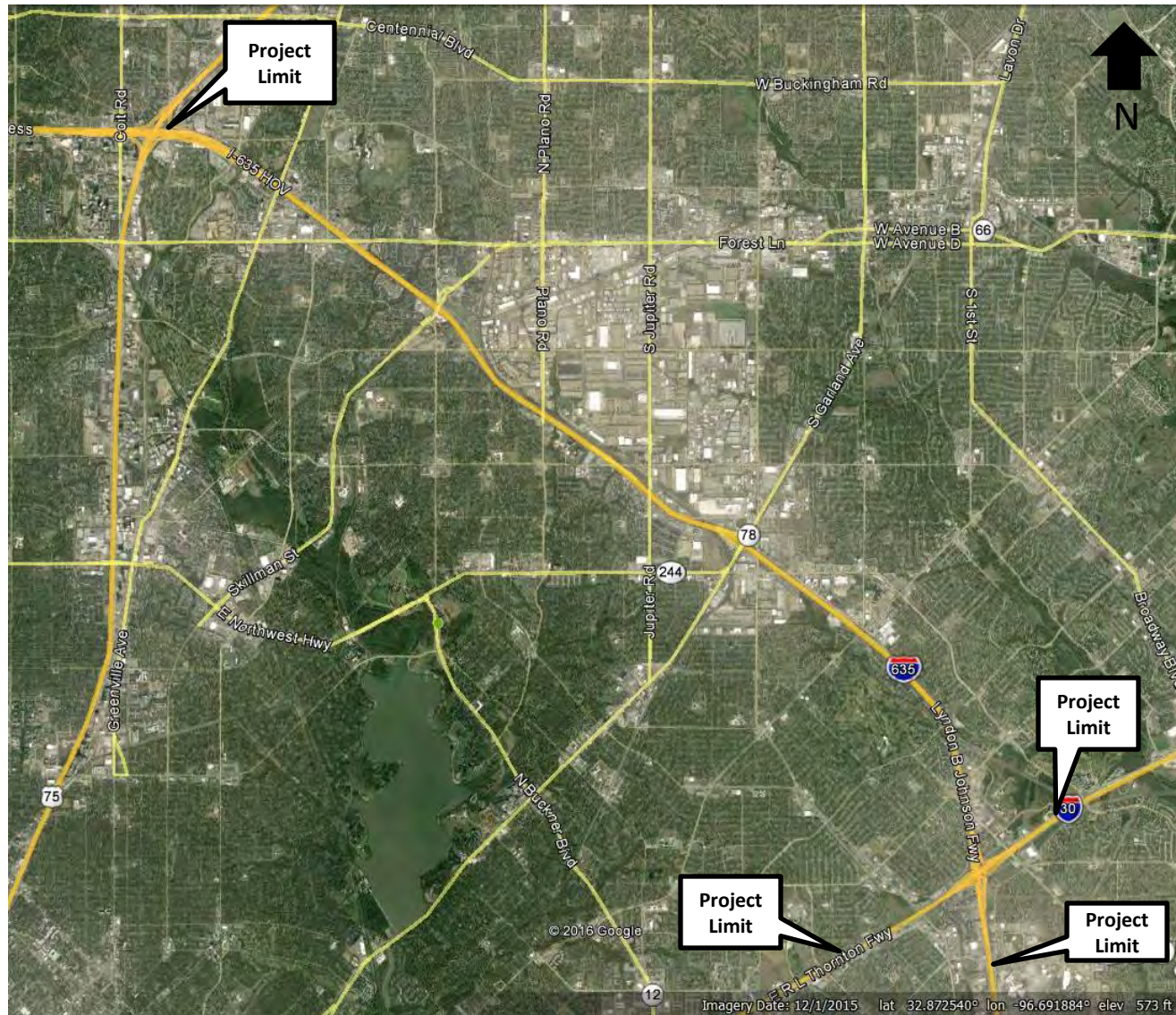
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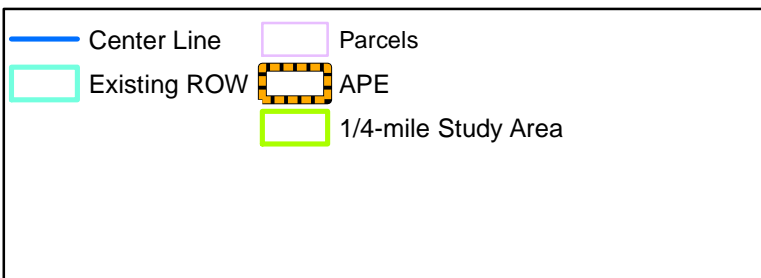
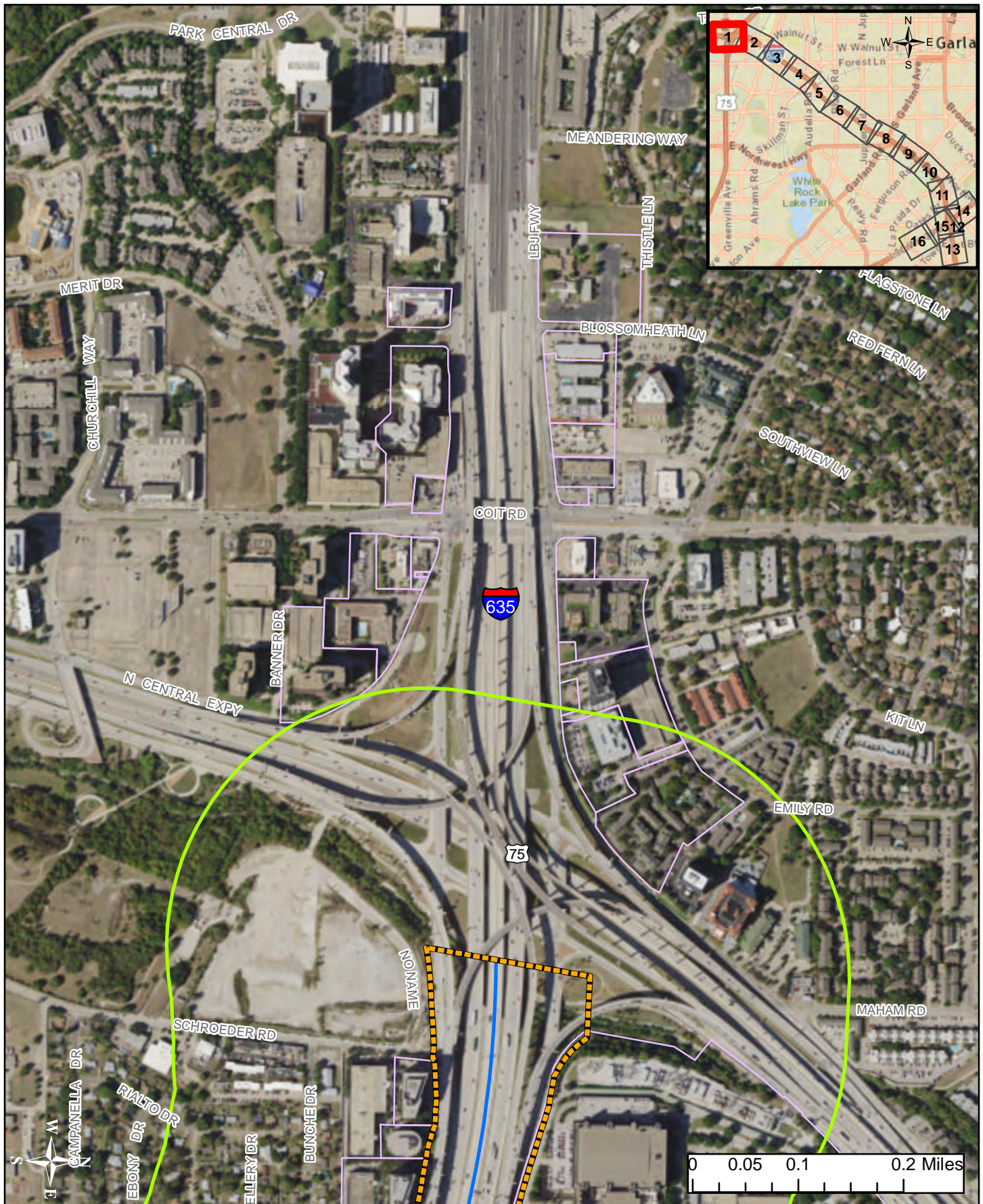
PROJECT TYPICAL SECTIONS
PAGE 8 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144

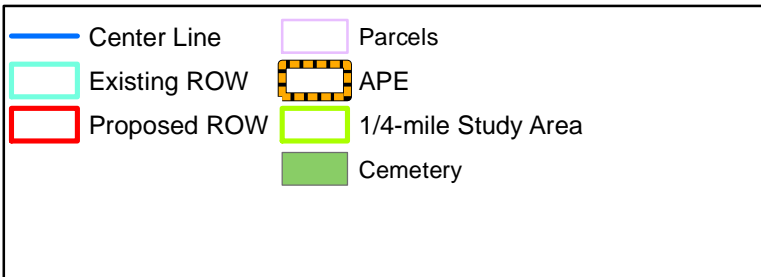
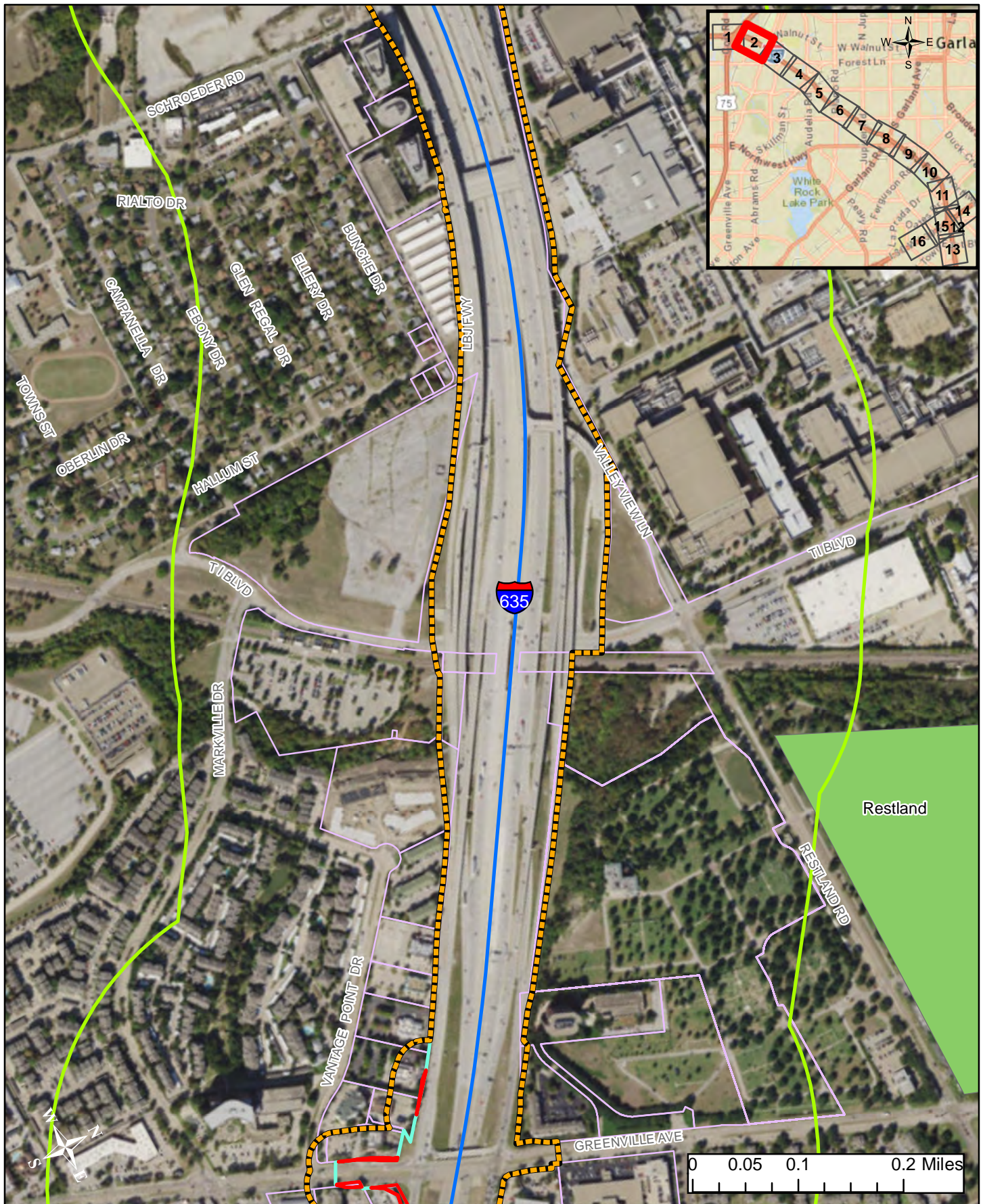
CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 and 2374-02-144

TxDOT Google Earth Database of Eligible Historic Bridges - Search Result



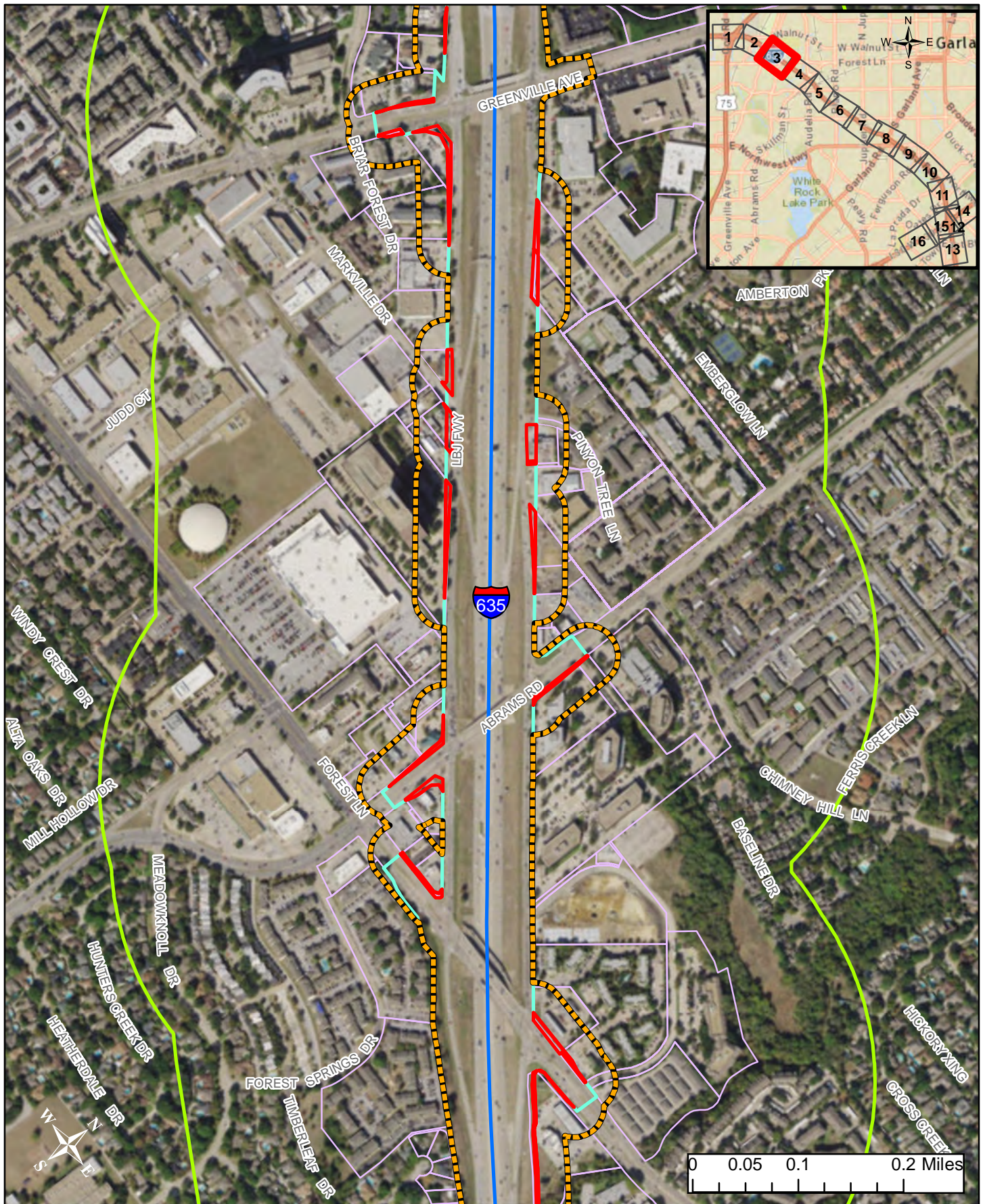



Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



Historic Properties and APE on Aerial Photograph

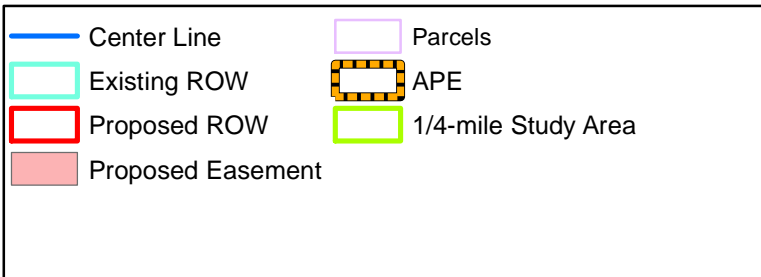
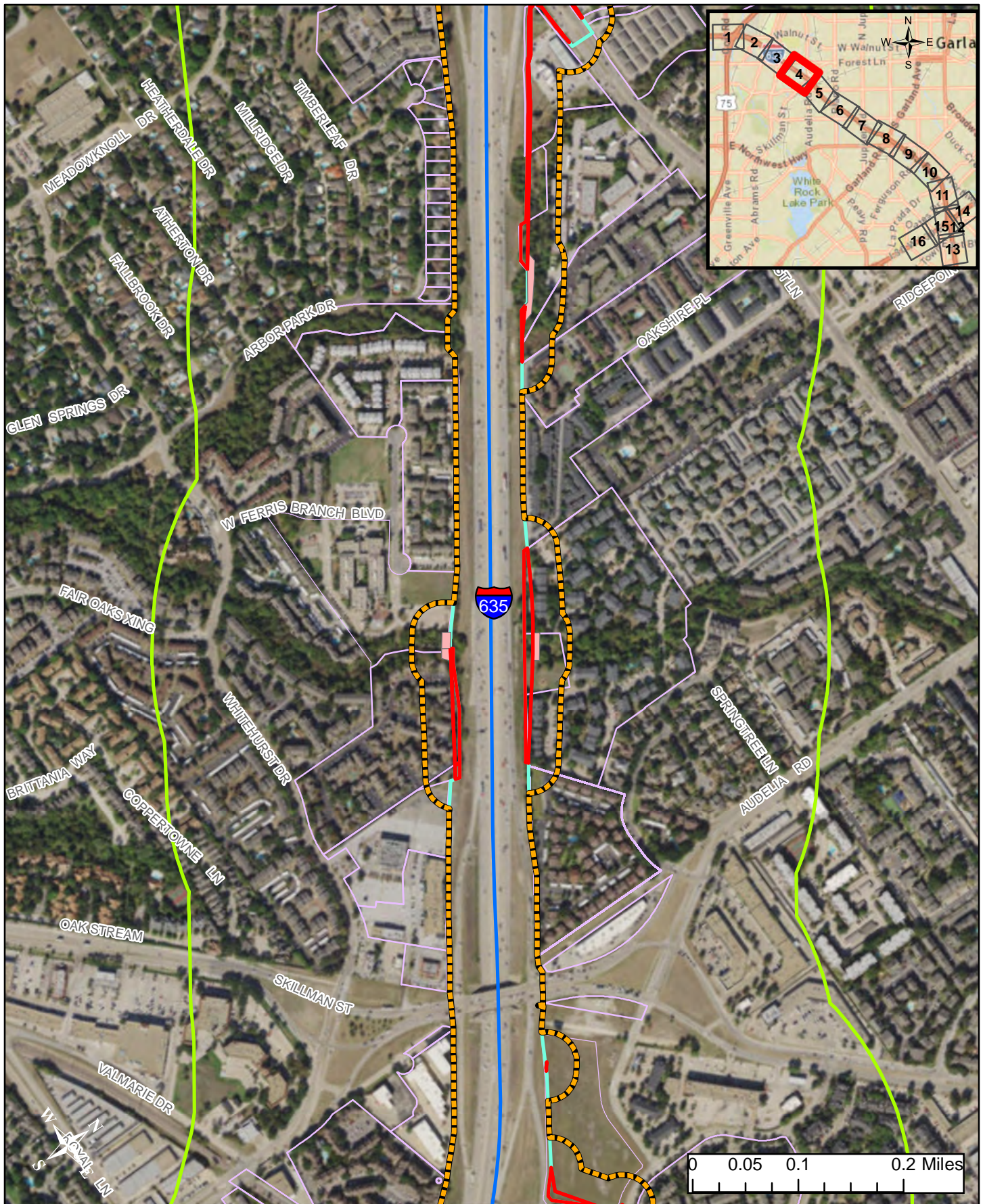
IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



- | | |
|--|---|
|  Center Line |  Parcels |
|  Existing ROW |  APE |
|  Proposed ROW |  1/4-mile Study Area |

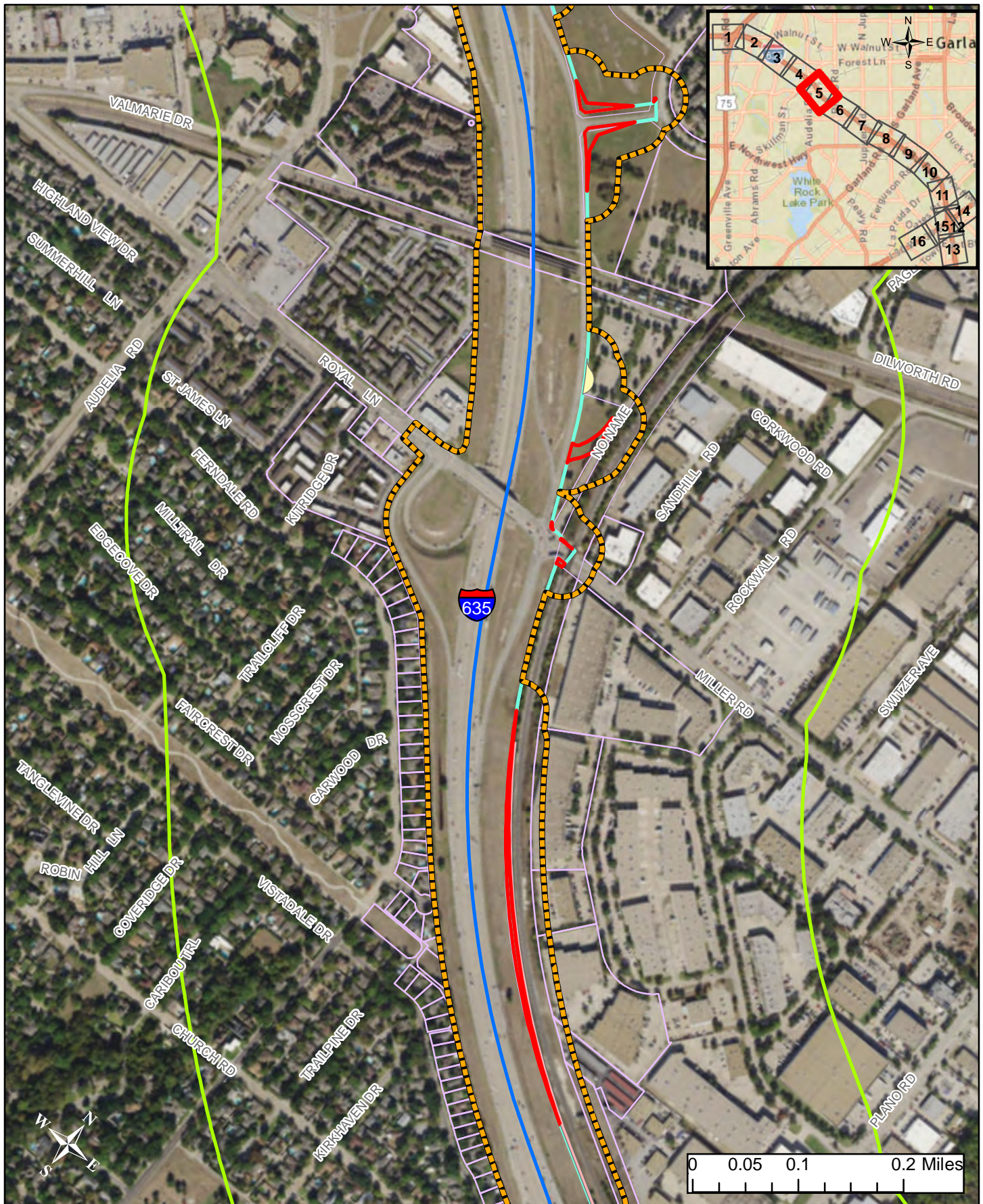
Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053 & 2374-02-144



**Historic Properties and
APE on Aerial Photograph**

IH 635 from US 75 to IH 30
Cities of Dallas, Garland and
Mesquite, Dallas County, Texas
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053 & 2374-02-144

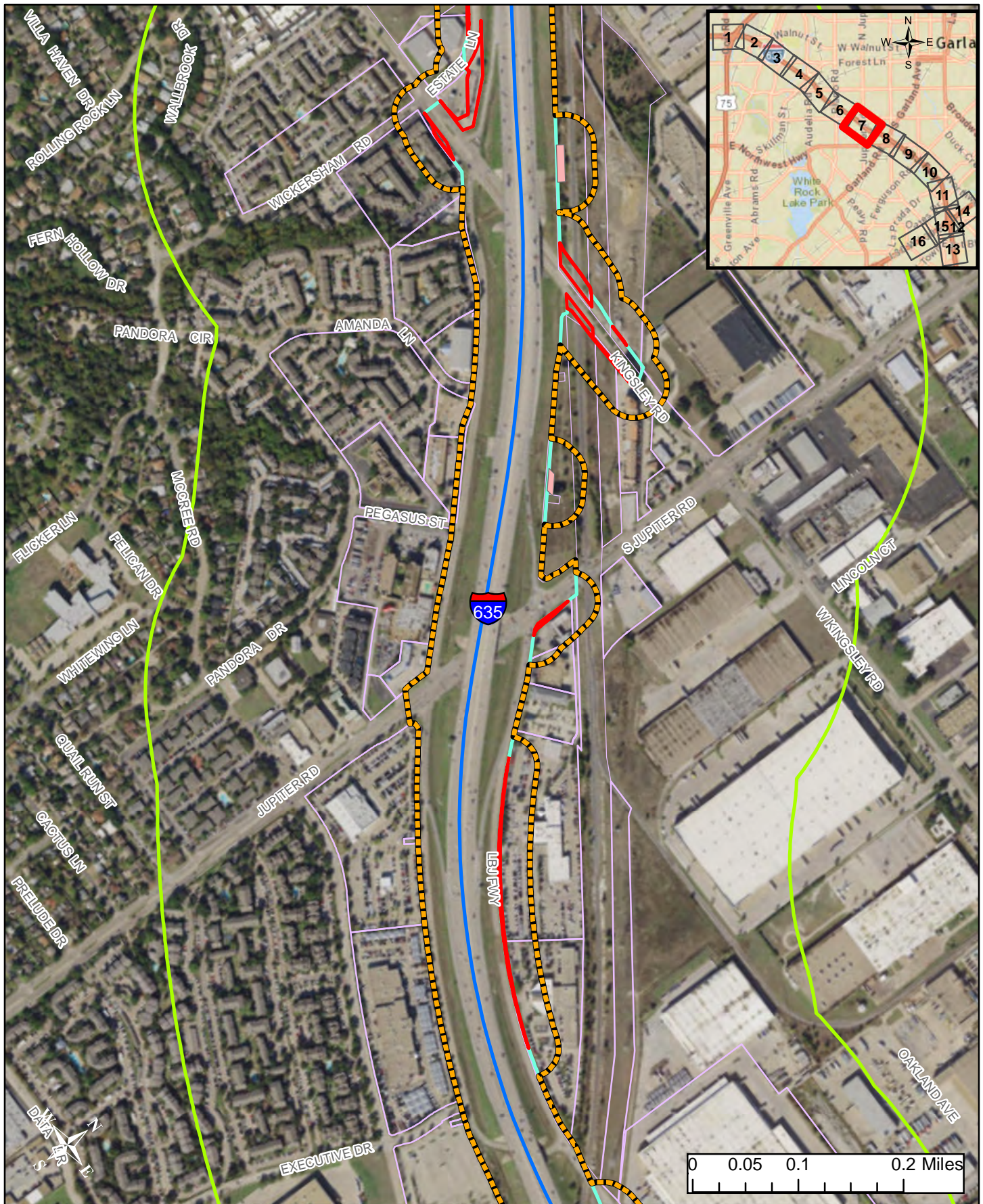


Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



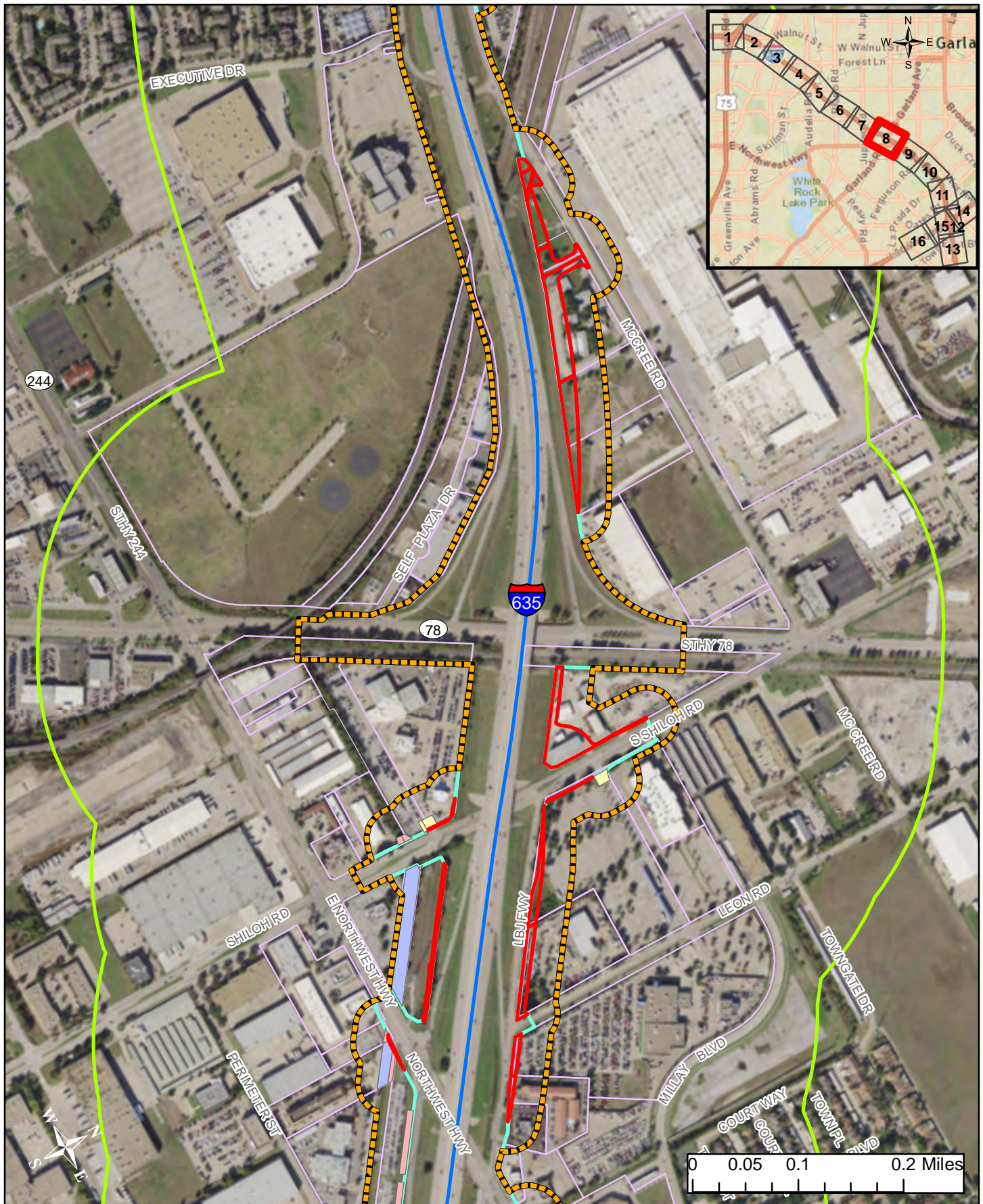
| | |
|-------------------|---------------------|
| Center Line | Parcels |
| Existing ROW | APE |
| Proposed ROW | 1/4-mile Study Area |
| Proposed Easement | Historical Marker |

Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



Historic Properties and APE on Aerial Photograph

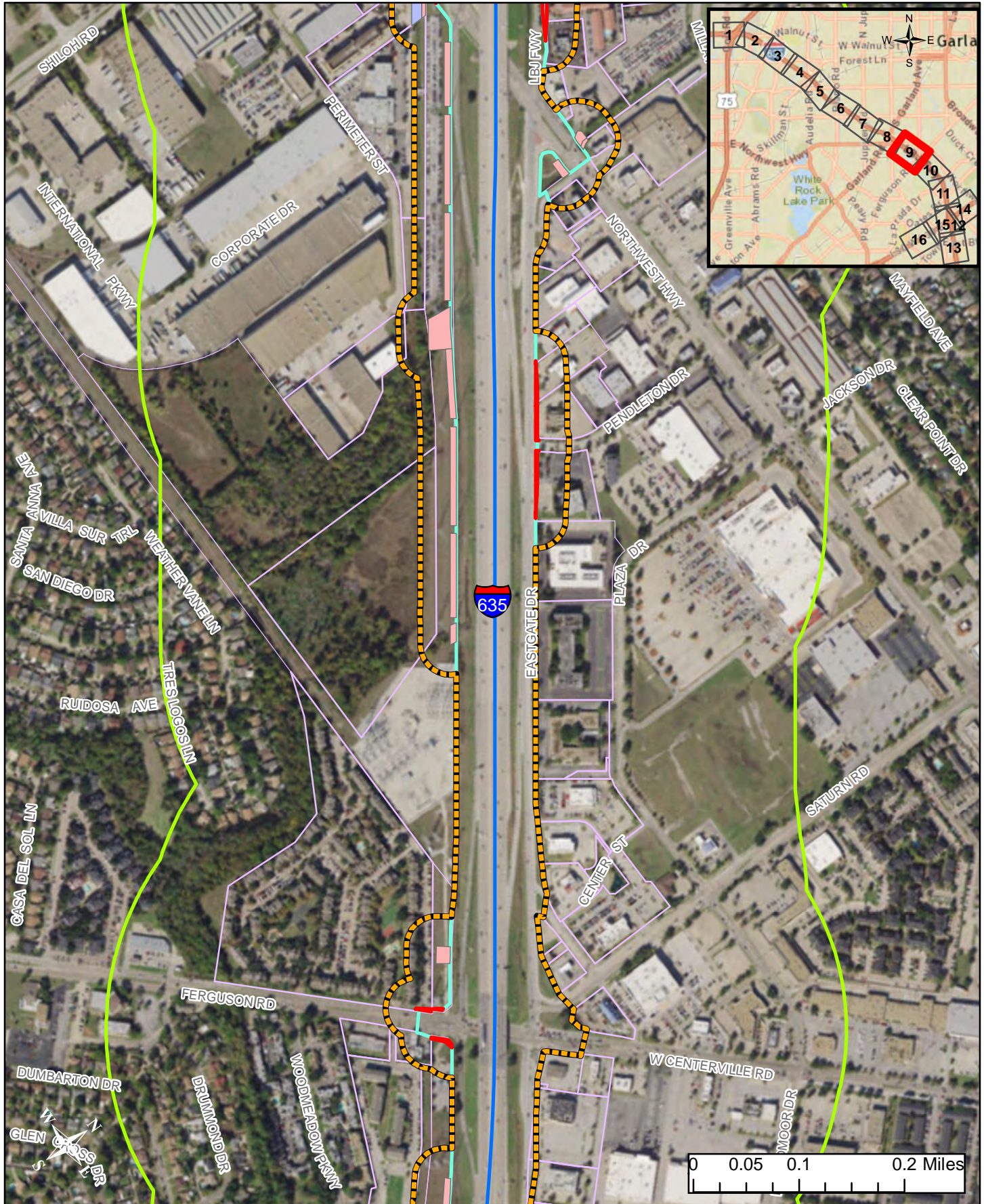
IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
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 2374-01-183, 2374-02-053 & 2374-02-144



| | | | |
|--|--------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Existing Easement | | 1/4-mile Study Area |
| | Proposed ROW | | |
| | Temporary Easement | | |
| | Proposed Easement | | |

Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
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 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053 & 2374-02-144



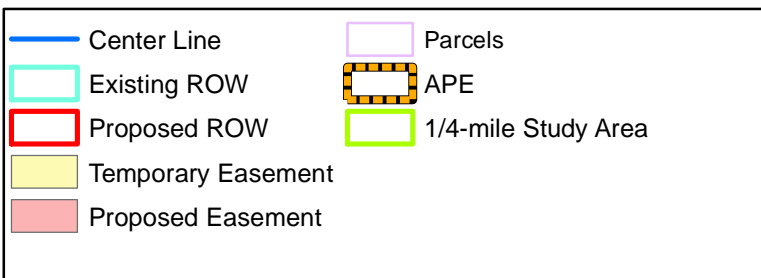
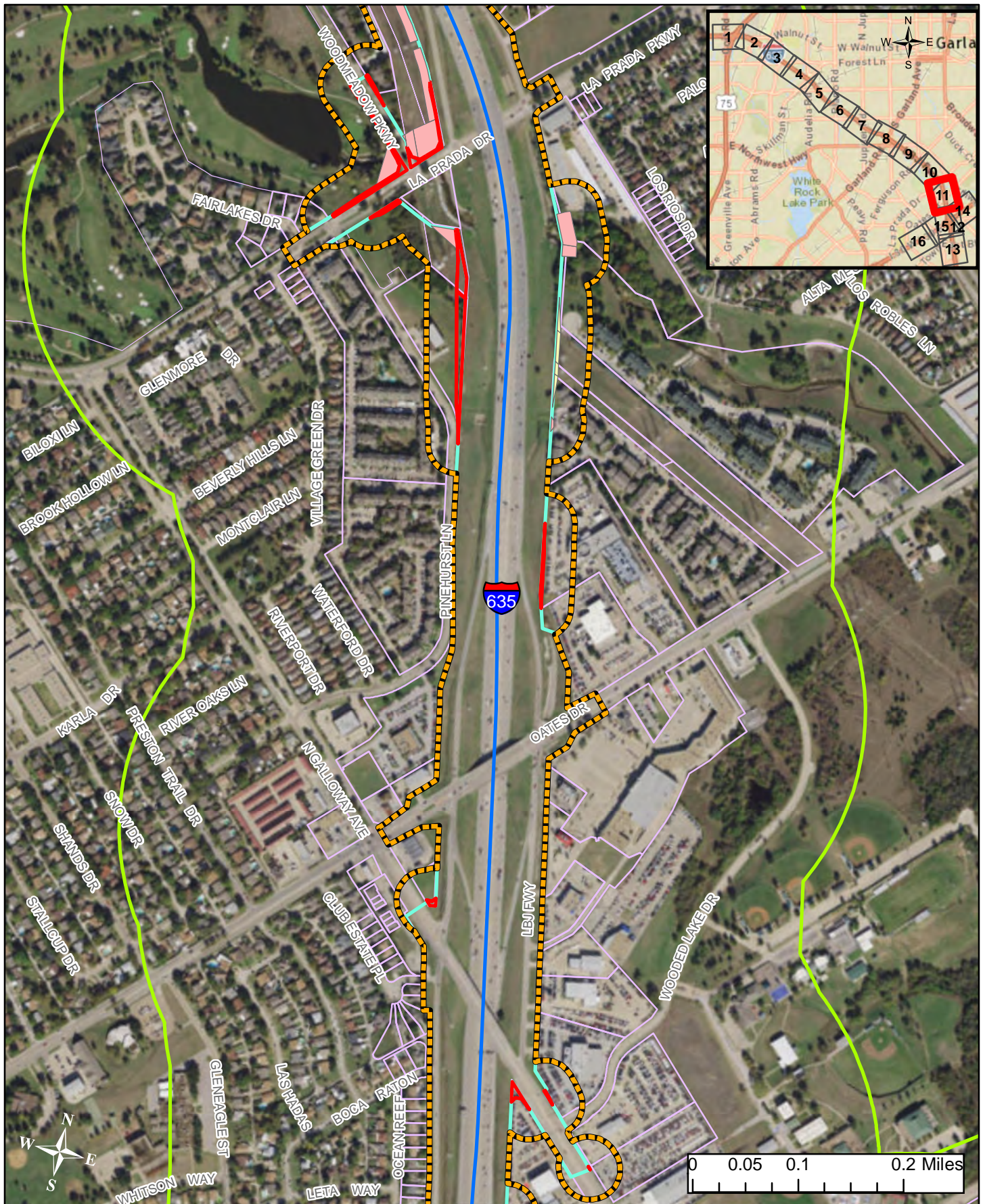
| | | | |
|--|-------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Existing Easement | | 1/4-mile Study Area |
| | Proposed ROW | | |
| | Proposed Easement | | |

Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144

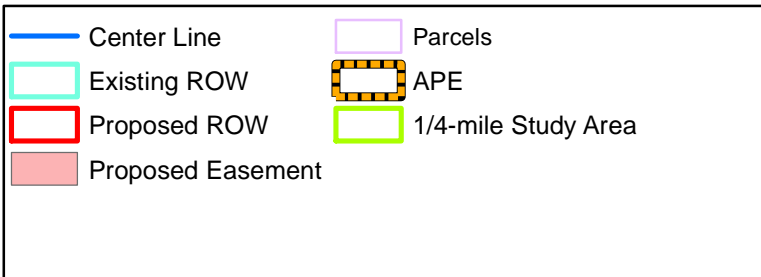


| | | | |
|--|--------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Proposed ROW | | 1/4-mile Study Area |
| | Temporary Easement | | |
| | Proposed Easement | | |

**Historic Properties and
APE on Aerial Photograph**
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
 Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180,
 2374-01-183, 2374-02-053 & 2374-02-144



Historic Properties and APE on Aerial Photograph
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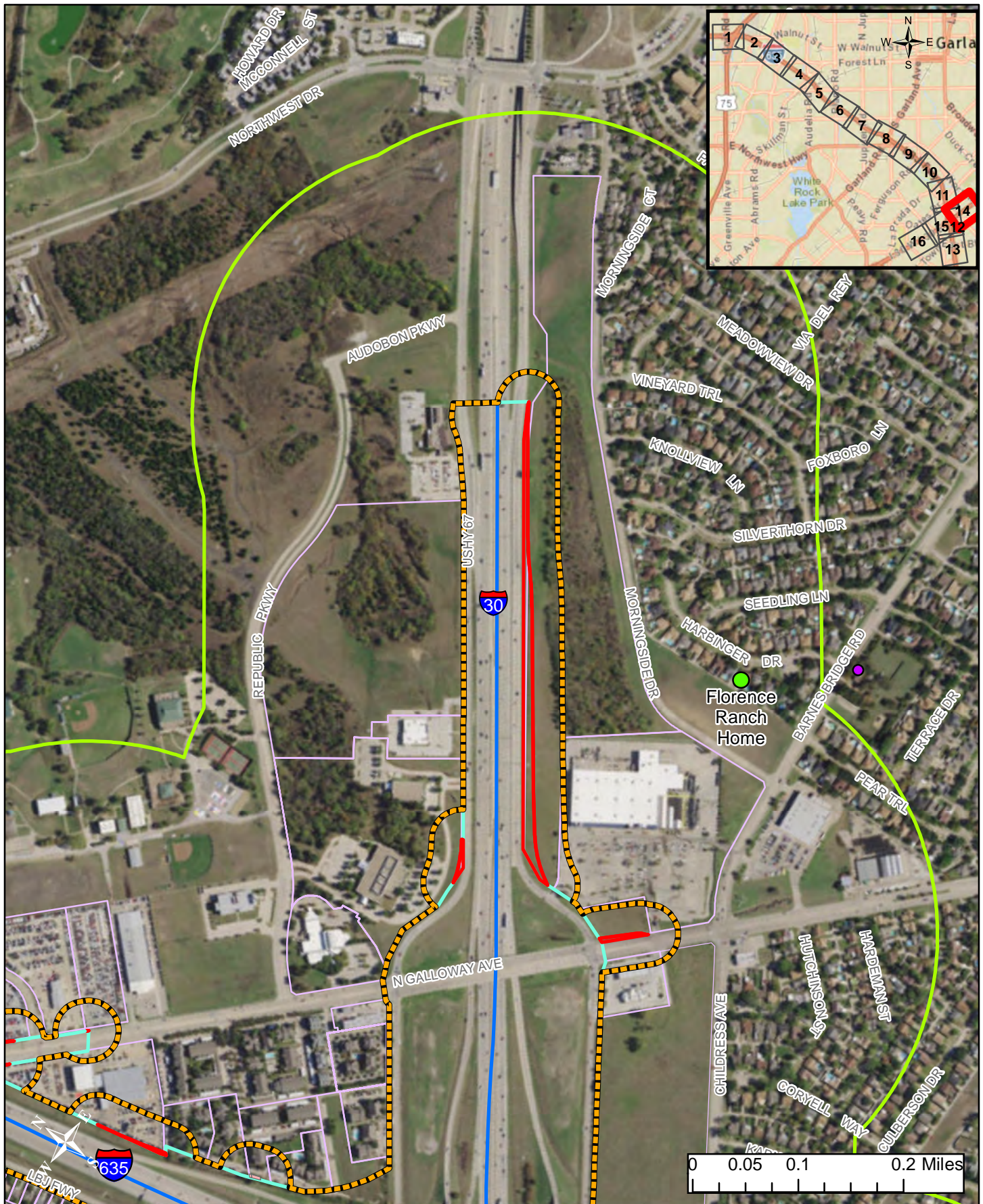


Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144

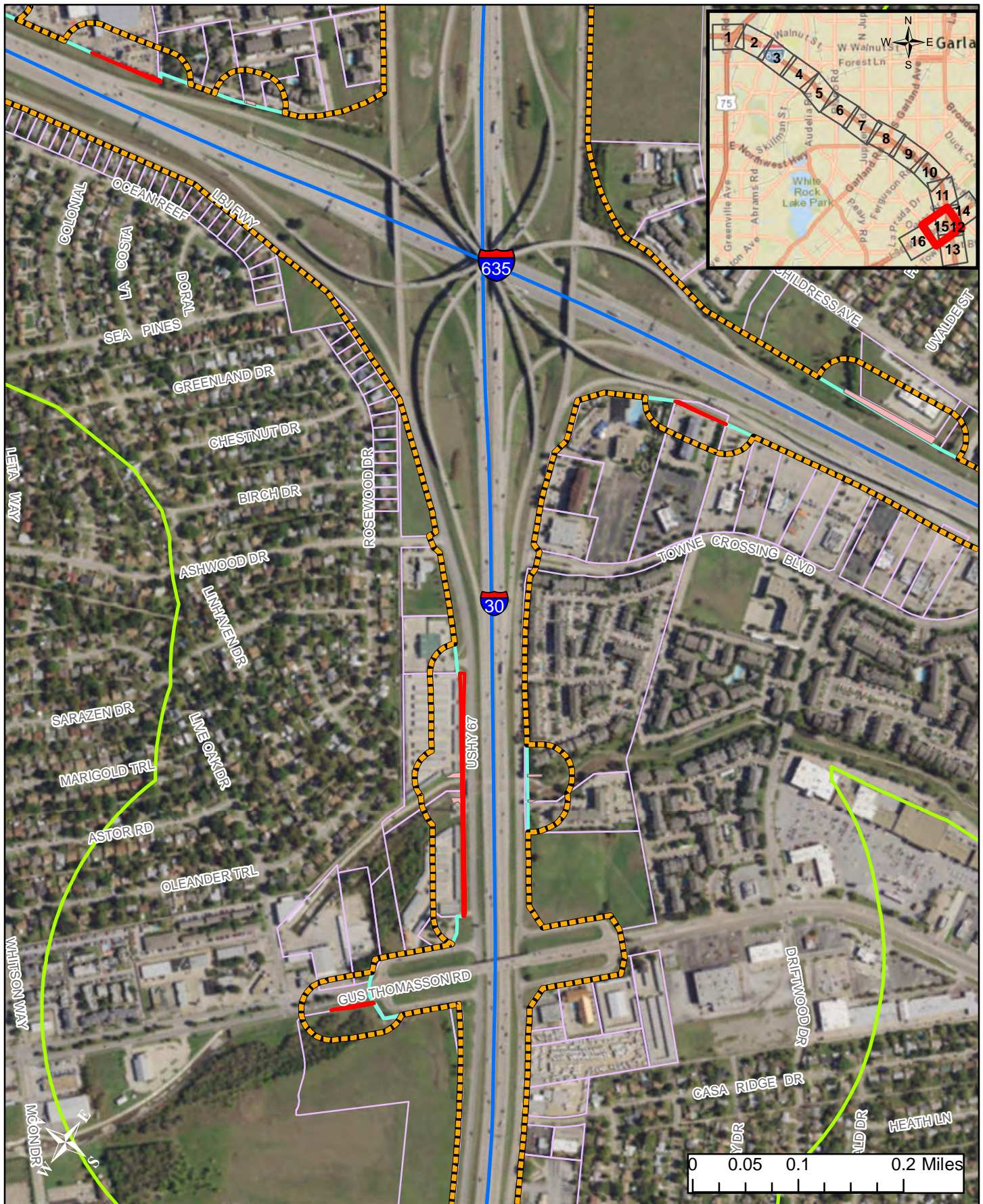


Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144

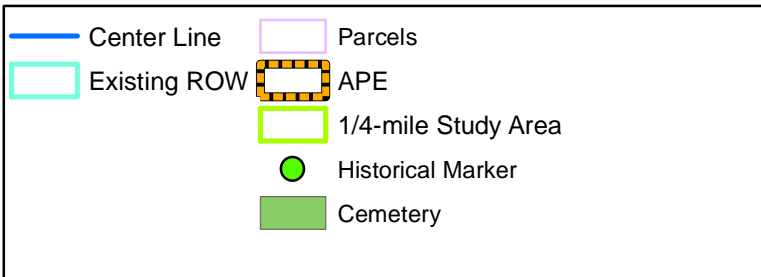
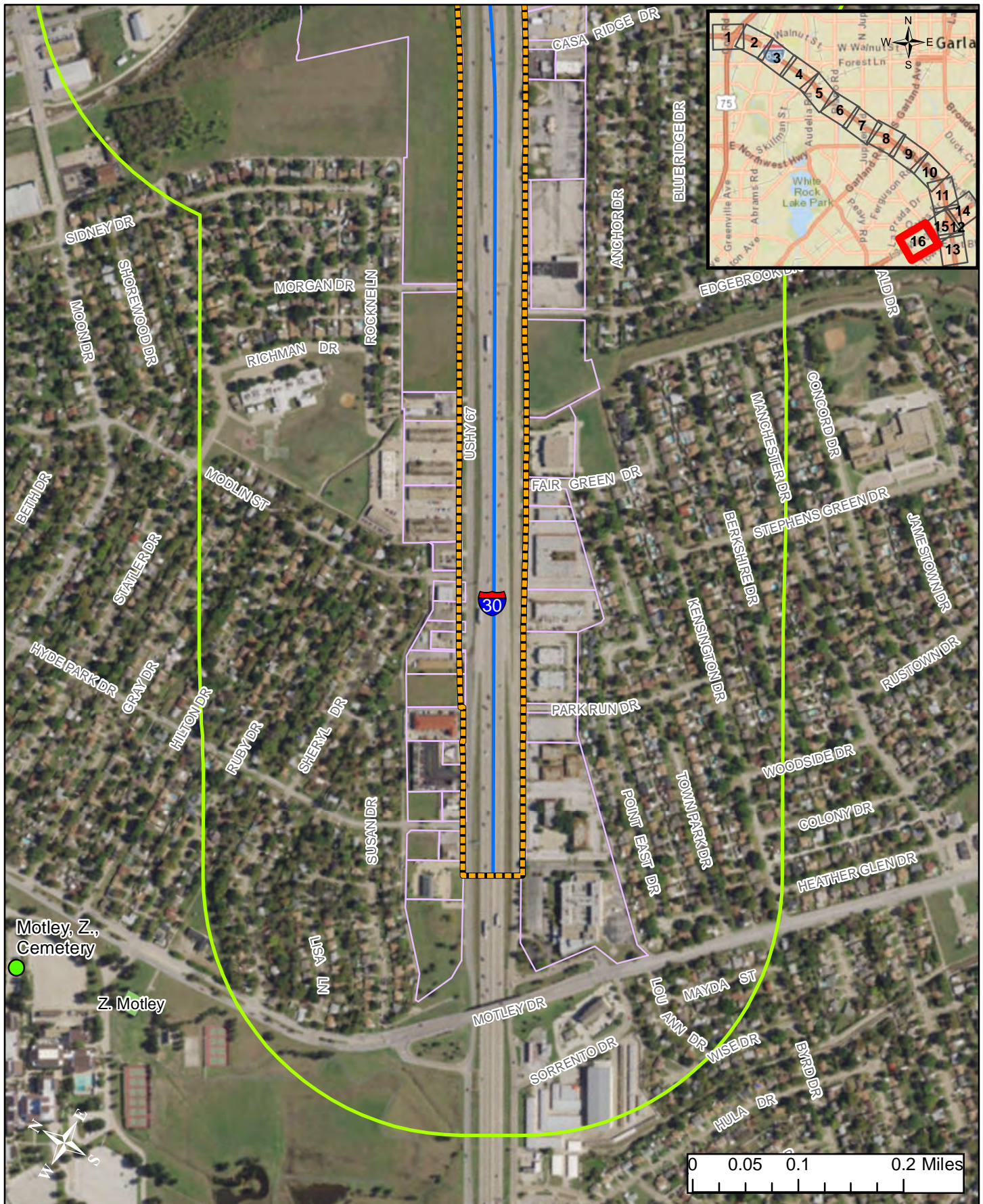


Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



| | | | |
|--|-------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Proposed ROW | | 1/4-mile Study Area |
| | Proposed Easement | | |

Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144

Appendix D – Project Photos



Photograph 1. Looking west toward the northern project terminus on IH 635 near the exit at US 75. Signage for the converted Express Lanes on the west side of US 75 is shown.



Photograph 2. Looking east on IH 635 from TI Boulevard at the current high occupancy vehicle (HOV) lanes.

Project Area Photographs (Page 1 of 8)

IH 635 from US 75 to IH 30

Cities of Dallas, Garland, and Mesquite, Dallas County, Texas

CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144



Photograph 3. Metroplex Piano, located at 9292 LBJ Freeway, is a proposed displacement due to construction of the proposed project.



Photograph 4. Title Max, located at 11919 Abrams Road, is a proposed displacement due to construction of the proposed project.



Photograph 5. Looking east from eastbound IH 635 lanes near the overpass of Abrams Road.



Photograph 6. Villas on Forest Springs Apartments, located at 9450 Forest Springs Drive, is an apartment complex located adjacent to the proposed project.

Project Area Photographs (Page 3 of 8)

IH 635 from US 75 to IH 30

Cities of Dallas, Garland, and Mesquite, Dallas County, Texas

CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144



Photograph 7. Looking southwest towards westbound IH 635 lanes.



Photograph 8. This building at Summerwood Cove Apartments, located at 9821 Summerwood Circle, is a proposed displacement due to construction of the proposed project.

Project Area Photographs (Page 4 of 8)

IH 635 from US 75 to IH 30

Cities of Dallas, Garland, and Mesquite, Dallas County, Texas

CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144



Photograph 9. Looking northeast from the eastbound IH 635 lanes near the Skillman Street and Audelia Road exit.



Photograph 10. Looking west on IH 635 near the Plano Road exit.

Project Area Photographs (Page 5 of 8)
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland, and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144



Photograph 11. Looking east from eastbound IH 635 lanes near the exit at Walnut Hill Lane, Kingsley Road and Jupiter Road.



Photograph 12. Looking west from westbound IH 635 lanes from the Garland Road entrance ramp.

Project Area Photographs (Page 6 of 8)
IH 635 from US 75 to IH 30
Cities of Dallas, Garland, and Mesquite, Dallas County, Texas
CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144



Photograph 13. View of various commercial properties located adjacent to IH 635 at Northwest Highway.



Photograph 14. View of residential properties along westbound IH 635, located north of la Prada Drive.

Project Area Photographs (Page 7 of 8)

IH 635 from US 75 to IH 30

Cities of Dallas, Garland, and Mesquite, Dallas County, Texas

CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144



Photograph 15. Looking east on IH 635 toward the IH 30 interchange (southern terminus).



Photograph 16. Looking west toward the eastern project terminus on IH 30 and the IH 635 interchange.

Project Area Photographs (Page 8 of 8)
IH 635 from US 75 to IH 30
Cities of Dallas, Garland, and Mesquite, Dallas County, Texas
CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, & 2374-02-144

Appendix E – Historic-Age Bridge Information

**IH 635 From US 75 to IH 30
Historic-Age Bridges in the Project Area**

| Crossing | Location (see Project Plan View Map in Appendix C) | Bridge Structure | NBI # | Year Built | Eligibility |
|-----------------------------|--|----------------------------------|----------------------|------------|--------------|
| Cross Street Bridges | | | | | |
| Greenville Ave | Approx. IH 635 STA 507+69.17 | Greenville Ave | 18-057-0-2374-01-050 | 1967 | Not eligible |
| Abrams Rd | Approx. IH 635 STA 538+87.91 | Abrams Road | 18-057-0-2374-01-051 | 1967 | Not eligible |
| Utility Bridge | Approx. IH 635 STA 550+50 | Utility Bridge | 18-057-0-2374-01-174 | 1967 | Not eligible |
| Forest Lane | Approx. IH 635 STA 551+88.96 | Forest Lane | 18-057-0-2374-01-052 | 1967 | Not eligible |
| Royal Ln/Miller Rd | Approx. IH 635 STA 635+50.29 | Royal Lane/Miller Road | 18-057-0-2374-01-057 | 1967 | Not eligible |
| La Prada | Approx. La Prada STA 20+00 | La Prada (city bridge) | unknown | 1966 | unknown |
| Woodmeadow Parkway | Approx. Woodmeadow Parkway STA 15+00 | Woodmeadow Parkway (city bridge) | unknown | unknown | unknown |
| Oates Dr | Approx. IH 635 STA 971+23.83 | Oates Dr. | 18-057-0-2374-02-072 | 1968 | Not eligible |
| Galloway Ave | Approx. IH 635 STA 938+65.10 | Galloway Ave | 18-057-0-2374-02-073 | 1968 | Not eligible |
| Galloway Ave | Approx. IH 30 STA 21+20.00 | Galloway Ave | 18-057-0-0009-11-452 | 1999 | Not eligible |
| Gus Thomasson Rd NB | Approx. IH 30 STA 59+32.52 | Gus Thomasson Rd NB | 18-057-0-0009-11-199 | 1960 | Not eligible |
| Gus Thomasson Rd SB | Approx. IH 30 STA 59+32.52 | Gus Thomasson Rd SB | 18-057-0-0009-11-375 | 1960 | Not eligible |
| Interstate Bridges | | | | | |
| Floyd Branch | Approx. IH 635 STA 485+00 | IH 635 | 18-057-0-2374-01-049 | 1967 | Not eligible |
| DRAW (Ferris Creek) | Approx. IH 635 STA 572+00 | IH 635 | 18-057-0-2374-01-053 | 1967 | Not eligible |
| DRAW (Audelia Branch) | Approx. IH 635 STA 589+00 | IH 635 | 18-057-0-2374-01-054 | 1967 | Not eligible |
| Plano Rd | Approx. IH 635 STA 683+11.26 | IH 635 WB | 18-057-02374-01-058 | 1967 | Not eligible |
| Plano Rd | Approx. IH 635 STA 683+11.26 | IH 635 EB | 18-057-02374-01-081 | 1967 | Not eligible |
| Dixon Branch | Approx. IH 635 STA 691+50 | IH 635 | 18-057-0-2374-01-059 | 1967 | Not eligible |
| DRAW | Approx. IH 635 STA 724+00 | IH 635 WB | 18-057-0-2374-01-060 | 1967 | Not eligible |
| Kingsley Rd / Walnut Hill | Approx. IH 635 STA 727+89.57 | IH 635 EB | 18-057-0-2374-01-104 | 1967 | Not eligible |
| DRAW | Approx. EB Frontage Road STA 726+50 | IH 635 EB FR | 18-057-0-2374-01-062 | 1967 | Not eligible |
| Kingsley Rd / Walnut Hill | Approx. IH 635 STA 727+89.57 | IH 635 WB | 18-057-0-2374-01-061 | 1967 | Not eligible |
| DRAW | Approx. IH 635 STA 740+00 | IH 635 | 18-057-0-2374-01-063 | 1967 | Not eligible |
| Jupiter Rd | Approx. IH 635 STA 750+05.81 | IH 635 WB | 18-057-0-2374-01-064 | 1967 | Not eligible |
| Jupiter Rd | Approx. IH 635 STA 750+05.81 | IH 635 EB | 18-057-0-2374-01-105 | 1967 | Not eligible |
| McCree Road U Turn | Approx. IH 635 STA 775+00 | IH 635 EB | 18-057-0-2374-01-065 | 1967 | Not eligible |
| TAP RR Bridge | Approx. IH 635 STA 780+00 | IH 635 EB | 18-057-0-2374-02-110 | 1967 | Not eligible |
| TAP RR Bridge | Approx. IH 635 STA 780+00 | IH 635 WB | 18-057-0-2374-02-066 | 1967 | Not eligible |
| Garland Ave (SH 78 @ SF RR) | Approx. IH 635 STA 802+39.38 | IH 635 EB | 18-057-0-2374-02-444 | 1967 | Not eligible |
| Garland Ave (SH 78 @ SF RR) | Approx. IH 635 STA 802+39.38 | IH 635 WB | 18-057-0-2374-02-354 | 1967 | Not eligible |
| Shiloh Rd | Approx. IH 635 STA 812+17.59 | IH 635 EB | 18-057-0-2374-02-434 | 1967 | Not eligible |
| Shiloh Rd | Approx. IH 635 STA 812+17.59 | IH 635 WB | 18-057-0-2374-02-074 | 1967 | Not eligible |
| Northwest Hwy | Approx. IH 635 STA 828+38.12 | IH 635 EB | 18-057-0-2374-02-433 | 1967 | Not eligible |
| Northwest Hwy | Approx. IH 635 STA 828+38.12 | IH 635 WB | 18-057-0-2374-02-075 | 1967 | Not eligible |
| Long Branch Creek | Approx. IH 635 STA 840+00 | IH 635 | 18-057-0-2374-02-175 | 1967 | Not eligible |
| Drainage Ditch | Approx. IH 635 STA 855+60 | IH 635 ML | 18-057-0-2374-02-176 | 1967 | Not eligible |
| Centerville Rd | Approx. IH 635 STA 877+15.76 | IH 635 EB | 18-057-0-2374-02-432 | 1967 | Not eligible |
| Centerville Rd | Approx. IH 635 STA 877+15.76 | IH 635 WB | 18-057-0-2374-02-076 | 1967 | Not eligible |
| La Prada/Wynn Joice | Approx. IH 635 STA 939+65.10 | IH 635 NB | 18-057-0-2374-02-102 | 1968 | Not eligible |
| La Prada/Wynn Joice | Approx. IH 635 STA 939+65.10 | IH 635 SB | 18-057-0-2374-02-103 | 1968 | Not eligible |
| Long Branch Creek | Approx. IH 635 STA 945+00 | IH 635 ML | 18-057-0-2374-02-071 | 1968 | Not eligible |
| Drainage Ditch | Approx. IH 635 STA 1010+00 and 1015+00 | IH 30 WB FR | 18-057-0-0009-11-305 | 1971 | Not eligible |
| Conn. A (IH 30) | Approx. IH 635 STA 1015+64.34 | IH 30 EB to IH 635 NB | 18-057-0-0009-11-356 | 1971 | Not eligible |
| Conn. B | Approx. IH 635 STA 1015+64.35 | IH 30 WB to IH 635 SB | 18-057-0-0009-11-357 | 1971 | Not eligible |
| Conn. C | Approx. IH 635 STA 1015+64.36 | IH 635 NB to IH 30 WB | 18-057-0-0009-11-358 | 1971 | Not eligible |
| Conn. D | Approx. IH 635 STA 1015+64.37 | IH 635 SB to IH 30 EB | 18-057-0-0009-11-355 | 1971 | Not eligible |
| IH 635 & Conn. E & G | Approx. IH 635 STA 1015+64.34 | IH 30 WB FR | 18-057-0-0009-11-359 | 1971 | Not eligible |
| IH 635 & Conn F & H | Approx. IH 635 STA 1020+00 | IH 30 EB FR | 18-057-0-0009-11-351 | 1971 | Not eligible |
| IH 635 | Approx. IH 635 STA 1015+64.34 | IH 30 WB ML | 18-057-0-0009-11-353 | 1971 | Not eligible |
| IH 635 | Approx. IH 635 STA 1015+64.34 | IH 30 EB ML | 18-057-0-0009-11-352 | 1971 | Not eligible |
| Drainage Ditch | Approx. IH 635 STA 1050+00 | IH 635 ML & SB FR | 18-057-0-2374-02-114 | 1971 | Not eligible |
| Mesquite Creek | Approx. IH 30 STA 68+00 | IH 30 ML & FR | 18-057-0-0009-11-080 | 1948 | Not eligible |
| Drainage Ditch | Approx. IH 30 EB Frontage Road STA 77+50 | IH 30 EB FR | 18-057-0-0009-11-304 | 1971 | Not eligible |

Report for Historical Studies Survey



Report for Historical Studies Survey

CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053 and
2374-02-144

IH 635 LBJ East Ultimate Project
Dallas County, Dallas District

Terri Asendorf Hyde
January 26, 2016

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

This technical report is produced for the purposes of meeting requirements under Section 106 of the National Historic Preservation Act, the Antiquities Code of Texas, and other cultural resource legislation related to environmental clearance as applicable.

Abstract

Re-Evaluation consultation is being conducted for the proposed reconstruction and improvement to an approximately 11.2-mile section of IH 635. The proposed project would reconstruct IH 635 to provide five general-purpose lanes, two tolled-managed lanes in each direction, and two to three frontage road lanes in each direction, from US 75 to near Royal Lane/Miller Road. From near Royal Lane/Miller Road to La Prada Drive, the proposed project would reconstruct IH 635 to provide five general-purpose lanes (one additional general purpose lane in each direction), two non-tolled express lanes in each direction, and two to three frontage road lanes in each direction. From La Prada Drive to south of IH 30, the project would reconstruct IH 635 to provide lane balance transitions between the general purpose lanes, proposed express lanes, and IH 30 interchange. Additionally, two to three frontage road lanes would be constructed in each direction from La Prada to Town East Boulevard. Improvements to IH 30 would include reconstructing the main lanes to provide at least three lanes in each direction through the IH 635 interchange with allowances provided for potential future managed/express lanes. Auxiliary lanes would also be constructed for the IH 635 interchange. The IH 30 improvements would include two to three lane frontage roads in each direction. The project would require approximately 16.3 acres of proposed right-of-way (ROW) as well as 0.5 acre of temporary construction easements and 9.1 acres of drainage easements.

The historic resources reconnaissance survey identified 13 historic-age properties in the Area of Potential Effects (APE) including commercial/industrial, funerary, educational, and domestic properties. None of these are recommended as eligible for the National Register of Historic Places. Because there are no historic properties in the APE, there would be no direct, indirect or cumulative effects by the proposed project on historic resources.

Project Identification

- **Date:** 12/14/2016
- **Date(s) of Fieldwork:** from 01/03/2017 to 01/05/2017
- **Historical Studies Survey Type:** Constraints Analysis ☐ Reconnaissance ☒
Intensive ☐
- **Report Version:** Draft ☒ Final ☐
- **Regulatory Jurisdiction:** Federal ☒ State ☐
- **TxDOT Contract Number:** 31-318P5009
- **District:** Dallas
- **County or Counties:** Dallas
- **Highway:** IH 635 LBJ East from US 75 to IH 30
- **CSJ:** 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053, 2374-02-144
- **Report Author(s):** Terri Asendorf Hyde
- **Principal Investigator:** Terri Asendorf Hyde

Project Description

Project Type: Reconstruction and Improvements

Total Project Length: 11.2 miles

New Right of Way (ROW) Acreage: 16.3 acres

Easement Acreage: 9.6 acres

Project Description and Impacts: Re-Evaluation consultation is being conducted for the proposed reconstruction and improvement to an approximately 11.2-mile section of IH 635. The proposed project would reconstruct IH 635 to provide five general-purpose lanes, two tolled-managed lanes in each direction, and two to three frontage road lanes in each direction, from US 75 to near Royal Lane/Miller Road. From near Royal Lane/Miller Road to La Prada Drive, the proposed project would reconstruct IH 635 to provide five general-purpose lanes (one additional general purpose lane in each direction), two non-tolled express lanes in each direction, and two to three frontage road lanes in each direction. From La Prada Drive to south of IH 30, the project would reconstruct IH 635 to provide lane balance transitions between the general purpose lanes, proposed express lanes, and IH 30 interchange. Additionally, two to three frontage road lanes would be constructed in each direction from La Prada to Town East Boulevard. Improvements to IH 30 would include reconstructing the main lanes to provide at least three lanes in each direction through the IH 635 interchange with allowances provided for potential future managed/express lanes. Auxiliary lanes would also be constructed for the IH 635 interchange. The IH 30 improvements would include two to three lane frontage roads in each direction. The proposed project includes the construction of multiple noise walls located along the project corridor, where reasonable and feasible. The project would require approximately 16.3 acres of proposed right-of-way (ROW) as well as 0.5 acre of temporary construction easements and 9.1 acres of drainage easements. Existing and proposed typical sections are included in **Appendix A**.

The previously evaluated Environmental Assessment (EA) received a Finding of No Significant Impact in 2003. The historic resources assessment for the EA found no historic-age resources within an Area of Potential Effects (APE) of 500 feet from the proposed ROW. The current historic resources survey found no properties eligible for the National Register of Historic Places (NRHP) within the current APE; therefore, there would be no impacts to historic properties.

Area of Potential Effects (APE):

The existing ROW ☐

150' from proposed ROW and easements ☐

300' from proposed ROW and easements ☐

Custom: feet from proposed ROW and easements ☐

Custom: The APE is 150 feet from all proposed ROW and easements, and is the existing ROW where project activities are confined to the existing ROW. ☒

Project Setting

Study Area: A study area of 1,300 feet from the proposed ROW was established to assist in developing historic context. A review was conducted of the Texas Historical Commission (THC) survey files and Historic Sites Atlas; the NRHP; the list of State Antiquities Landmarks (SAL); the list of Recorded Texas Historic Landmarks (RTHL); and the TxDOT Environmental Affairs (ENV) Google Earth database of historic resources. The following resources were identified:

1. Restland Cemetery (Cemetery), 13005 Greenville Avenue, Dallas, TX. Restland Cemetery was founded in 1925 with the intent to serve as a landscaped park with the use of in-ground memorial markers. It is the burial ground for many prominent figures in the Dallas area including politicians and professional athletes.
2. Highland Oaks Church of Christ (Official Texas Historical Marker, 2005), 10805 Kingsley Road, Dallas, TX. The church originally formed in 1885 when Dallas was a small town, and met in private homes, the county courthouse and other places until they built their first building in 1867 at Ross and Market streets. Civil War veteran Gen. Richard M. Gano served the congregation as minister and church elder for many years after returning from the war. The church split in 1877 over the use of musical instruments. The congregation moved several times throughout its history, finally establishing at Kingsley Road in 1986.
3. Florence Ranch Home (Recorded Texas Historic Landmark, 1978), 1424 Barnes Bridge Road, Mesquite, TX. David and Julia (Beaty) Florence built the first portion of this ranch house in 1871-72 after moving to Dallas from Van Zandt County. It is a simple frame structure with elaborate wood trim. By 1890, the house and grounds were enlarged, occupying 730 acres. After Florence retired in 1908, his son Emet and his wife Perle (Curtis) continued to run the ranch, called Meadow View Farm.

Historic Land Use: Historic maps from 1886, 1900, and 1918 from the Texas Historic Overlay reveal that northeast Dallas County was mostly comprised of large farms and ranches. Three railroads traverse the project corridor including the Houston and Texas Central (1872), the Missouri Kansas and Texas (1881), and the Gulf Colorado and Santa Fe (1886). Historic aerials from 1952-58¹ show continued agricultural/ranch land use along the corridor until IH 635 was constructed in 1968.² Several neighborhoods, including Hamilton Park, Lake Highlands, and Casa View were developed along the corridor between 1955 and 1965. Texas Instruments was built near US 75 and IH 635 in 1958, and a large Safeway Distribution Center (Resource ID 8) was built on a railroad spur at McCree Road and SH 78 between 1952 and 1956. Infill has occurred over the past several decades, but several neighborhoods and commercial/industrial buildings that appear on the 1972 aerial appear to be extant.

¹ <http://www.historicaerials.net> (accessed January 25, 2017)

² http://www.interstate-guide.com/i-635_tx.html

Current Land Use: The IH 635 project area is highly urbanized with commercial and industrial development as well as residential neighborhoods located adjacent to the existing roadway. Future land use and zoning maps for the cities of Dallas, Garland, and Mesquite reflect the majority of land used as single/multi-family residential, commercial, industrial, and mixed use. There is minimal undeveloped space within the project area. The project corridor is comprised almost entirely of paved surfaces, mowed turf grass, and other urban landscapes. Non-urban areas are primarily small patches of riparian hardwood forests associated with stream crossings of IH 635 and IH 30.

Historic Period: Based on research to-date including a review of historic aerials and parcel data, the anticipated period of significance for historic resources begins in 1955 with the construction of the first neighborhoods built in proximity to the corridor. It ends in 1973 with the survey construction cut-off date. Properties built before 1955 are possible, but unlikely to be discovered.

Comments on Project Setting: Based on a review of historic aerials and parcel data from Dallas Central Appraisal District online, it appears that a great deal of development along the corridor was completed by 1972. Historic aerials from 1952-58³ show mostly large ranch properties along the corridor before IH 635 was constructed in 1968. Several neighborhoods, including Hamilton Park, Lake Highlands, and Casa View, were developed along the corridor between 1955 and 1965. Infill has occurred over the past several decades, but neighborhoods and commercial/industrial buildings that appear on the 1972⁴ aerial appear to be extant. It is highly likely intact historic-age resources will be discovered within the APE. A reconnaissance survey is necessary to determine if any historic-age resources are NRHP eligible, and if so, what the impacts by the proposed project would be.

Consulting Parties:

- Fred Durham, Dallas County Historical Commission, 214-669-4586, fldurhamjr@gmail.com
- Linda Henderson, Texas Historical Commission, 512-463-5851, Linda.Henderson@thc.state.tx.us
- Mark Doty, City of Dallas Historic Preservation Office, mark.doty@dallascityhall.com
- David Preziosi, Preservation Dallas, director@preservationdallas.org

No public involvement plan (PIP) was included for this project. A copy of the Report for Historical Studies Survey will be forwarded to consulting parties when it is submitted to the THC.

³ <http://www.historicaerials.net> (accessed January 25, 2017)

⁴ Ibid.

Survey Methods

Surveyors: Terri Asendorf Hyde

Methodological Description: A reconnaissance survey is necessary to identify any NRHP-eligible properties within the proposed APE and assess the proposed project's potential effects on historic properties.

Comments on Methods: A qualified historian identified and documented all resources built in 1973 or earlier within the proposed APE according to TxDOT's Documentation Standard for a Reconnaissance Survey Report (April 2014). Each historic-age resource in the proposed APE was mapped, photographed, and documented for style, architectural features, age, and other pertinent information related to eligibility and/or effects determinations. The report includes a historic context; the identification of any potential historic districts; recommendations of eligibility and a justification statement for each resource; effects determinations; and recommendations for any additional survey or research work necessary to complete a determination. Additionally, because of the large number of mid-20th century resources in the APE, the historian also adhered to TxDOT ENV's Documentation Standards for Mid-20th Century Developments in a Historical Studies Report. Mid-20th century properties were documented individually when they fell within the APE and otherwise were documented as a whole within the neighborhood or development.

Survey Results

Project Area Description: The reconnaissance survey revealed the project area to be much as described in the research design, consisting of a dense and highly urbanized commercial and industrial corridor with several neighborhoods located adjacent to the IH 635. Most historic-age resources were constructed after 1970, but some are remnants of the mid to late 1950s. The context, *Parks and Cemeteries in Northeast Dallas, 1956 –1973*, was added below to encompass the Dallas Athletic Club, established in 1956, which was not identified prior to the research design.

Literature Review/Context:

Settlement and Early Development in Northeast Dallas, 1840 - 1920

The project area is located in the northeast section of Dallas County, Texas. American explorers arrived in the area around 1840 after Native American groups (mostly the Anadarkos) had settled along the Trinity River and Europeans had explored the area, sometimes establishing missions. The area was further settled by Peters colony efforts and became mainly rural and agricultural (*Handbook of Texas Online*, Dallas County). During the Civil War, agriculture in the area experienced setbacks but regained momentum beginning in 1870: Dallas County's cotton crop rose from 3,834 bales in 1870 to 21,649 in 1880, a gain of 465 percent (Campbell 2003). By 1873, because of the rich soils of the Blackland Prairies, the fordability of the Trinity River, and the eventual confluence of railroads, Dallas had become the trading and transportation hub for North Texas (City of Dallas 2012).

By 1886, three railroads traversed the project corridor including the Houston and Texas Central (HTC) (1872), the Missouri Kansas and Texas (MKT) (1881), and the Gulf Colorado and Santa Fe (GC&SF) (1886). Communities near the project corridor including Richardson, Garland, and Mesquite were established along these railroad lines. Along with improved transportation came an increase in industry and manufacturing: between 1870 and 1890, growth occurred in milling, lumbering, and mining, mainly for processing agricultural products (Campbell 2003). Flour milling became the state's largest industry through the 1880s and was concentrated in North Texas (Campbell 2003). Alongside growth in agriculture and industry was a rise in farm tenancy, which was largely caused by a move from semi-subsistence farming to large-scale, mechanized farming (Campbell 2003) and could not be solved until Texas joined the industrial revolution with the rest of the United States. The Federal Reserve Bank established a branch office in Dallas in 1913, making it a leader city in banking and finance.

Transportation and Industry in Northeast Dallas, 1930 - 1968

Dallas was slightly less affected by the Great Depression due to the city's role as chief petroleum financier of oil which had been discovered in East Texas in 1930. During WWII, Dallas experienced an economic boom as lead manufacturer of military goods (City of Dallas 2012). The project area, however, remained agricultural through the 1950s. Historic aerials from 1952-58⁵ show large agricultural/ranch properties along the corridor before IH 635 was constructed in 1968. The Central Expressway (US 75) was built in 1949 and the project area was annexed during the 1960s (City of Dallas 2012).

Agricultural trade maintained prominence in the local and state economy, bolstered by new road construction connecting towns to new markets until the Great Depression. The railroads continued to service local and diversified industry which included military goods, aviation products, steel fabrication, oil field equipment, aluminium die casting, and food processors. Within the project area, the land along the GC&SF between IH 635 and SH 78 (Garland Road) saw increased numbers of distribution centers, warehouses, and manufacturers between 1956 and 1972. In particular, a large Safeway distribution center (Resource ID 8) was built at McCree Road and SH 78 between 1952 and 1956 along a railroad spur of the GC&SF. This approximately one million-square-foot structure is located on 66 acres and served as the regional distribution center for 112 grocery stores operated by Safeway according to an article in *D Magazine* published in June 2014.⁶ The International Paper building (Resource ID 7) was constructed at the northwest corner of Jupiter and Kingsley in 1958. Founded in 1898, in northeast U.S. as a pulp and paper company, mills were eventually opened nationwide including in Texas. The Kingsley Plant was on a railroad spur of the MKT.

Historic aerials show the Garland Municipal Airport, also called the Dallas Hudson Airport, at the southwest corner of IH 635 and SH 78. It was built between 1937 and 1940 and appears to have been abandoned circa (ca.) 1980. It had two paved runways and several hangars. According to the

⁵ <http://www.historicaerials.net> (accessed January 25, 2017)

⁶ <http://www.dmagazine.com/commercial-real-estate/2014/06/garden-ridge-to-open-new-distribution-center-in-garland/>

website, *Abandoned and Little Known Airfields*,⁷ the airport had a moment in history as part of the events surrounding the assassination of President Kennedy in Dallas when Robert Plumlee (a veteran CIA pilot) testified before the Senate Foreign Relations Committee in 1990 and 1991 that on November 22, 1963, he had co-piloted a top secret flight for the CIA that carried Philippe Socko (a known Mafia/CIA double operative) from Florida to Dallas. He testified that their assignment was to stop the assassination attempt on Kennedy's life and that Socko departed the plane at Dallas Garland Airport on the morning of November 22.



Photo 1: 1957 aerial of Dallas Hudson/Garland Airport, courtesy of http://www.airfields-freeman.com/TX/Airfields_TX_Dallas_E.htm (accessed January 15, 2017).

With US 75 already built from central to north Dallas, it was only a matter of time that an east-west connection would be constructed. The Carrollton Chronicle (Vol. 61, No. 21, Ed. 1, Thursday, April 8, 1965) reported that “a contract was [also] awarded for 7.494 miles of construction on IH 635 to Austin Bridge Co. and M.C. Winters on a low bid of \$5[million] for grading, drainage and large structures from east of U.S. Hwy. 75 to west of Centerville Road...resident engineer W. E. Williams will be in charge.” By 1968, the section of IH 635 from US 75 to SH 30 had been built, and the suburbanization of northeast Dallas would quickly follow.

⁷ http://www.airfields-freeman.com/TX/Airfields_TX_Dallas_E.htm

The Post War Boom and Suburban Northeast Dallas, 1950 - 1965

A post war boom in technology placed Dallas as third largest technology center in the nation with new businesses and manufacturing plants arriving on a monthly basis (City of Dallas 2012). According to the company's website,⁸ in 1956, Texas Instruments was built on 300 acres at the northeast corner of US 75 and IH 635 (not yet built). Over the next 10 years, the TI campus would grow to include several major buildings and three large semiconductors on the site, including the Dallas Metal Oxide Semiconductor Six (DMOS 6), a 1 million square-foot wafer fabrication semiconductor.



Photo 2: Texas Instruments, taken January 3, 2017.

During this time, several neighborhoods, including Hamilton Park, Lake Highlands, and Casa View were developed along the corridor. Hamilton Park, south of IH 635 and east of US 75, was established in 1953 and was originally designed as an all-black neighborhood named after African-American civic leader and physician, Dr. Richard T. Hamilton. The neighborhood included 742 single-family houses, an apartment complex, a shopping center, a park, a school, and several churches. It is still a predominantly black neighborhood (*Handbook of Texas Online*, Hamilton Park).

The larger Lake Highlands neighborhood was also established in the mid-1950s. A more diversified neighborhood, it contained many subdivisions, several schools and parks. Further south in the project area toward IH 30, Casa View was established as a predominantly white, Roman Catholic

⁸ <http://www.ti.com/corp/docs/company/history/timeline/key/1950/docs/55site.htm>

neighborhood with schools and a shopping center, churches, parks and schools. It is now referred to as Greater Casa View.

Within the APE, the New World Neighborhood (Resource ID 12) was established slightly later, in 1964. The neighborhood consists of mostly one-story, brick Ranch-style homes with varying finishes and designs, four small parks, and the Montclair Elementary School which opened in 1968. The majority of houses were constructed between 1966 and 1972.

Parks and Cemeteries in Northeast Dallas, 1956 – 1973

In the project area (but not in the APE), the Samuel Garland Park at the southwest corner of SH 78 and IH 635 (directly east of the former airport) was established in 1938 on approximately 45 acres and appears to have also served as a heliport (it still features two helipads). Further south, at the southwest corner of IH 635 and La Prada Drive, The Dallas Athletic Club (Resource ID 13) was built in 1956 by Ralph Plummer, a productive golf course designer and student of noted golf course architect, Joseph Bredemus. The club is situated on approximately 300 acres with 36 holes, recreational buildings, a pool and tennis courts. Plummer designed both the blue and gold courses (two total courses on the club). Jack Nicklaus won the 45th PGA Championship played on the Blue Course in 1963. Both courses were renovated by Nicklaus in 1980.

Restland Cemetery was established on the north side of IH 635 in 1957 on 357 acres. The cemetery was designed as a park with in-ground memorial markers, chapels, and mausoleums throughout the landscape.

Continued Expansion along IH 635

During the 1970s and 80s, expansion in aviation, electronics, and oil and gas industries encouraged more corporations to move to Dallas to take advantage of the business-friendly, lower-tax economic climate (City of Dallas 2012). The IH 635 corridor continued to grow, with the continued expansion of the TI campus, the addition of numerous distribution warehouses, the Remington College campus (Resource ID 11), business parks, restaurants, hotels, and neighborhoods.

APE Integrity: IH 635 was built through the area by 1968 along with several neighborhoods adjacent to the corridor. Some commercial and industrial buildings had also occurred by then, including Texas Instruments at US 75 and IH 635, and the Safeway Distribution Center (Resource ID 8) in Garland. Thus, by 1970, much of the farmland along the corridor had been consumed. Since 1973, the end of the historic period for this project, much infill has occurred, the majority between approximately 1975 and 1990. However, most of the historic neighborhoods and much of the commercial and industrial stock is extant.

Recommendations - Historic Property Evaluations (including historic districts):

Properties not eligible for the NRHP

Historic-age property types within the APE that are *not eligible* for the NRHP include commercial/industrial, funerary educational and domestic structures. Commercial buildings comprise the bulk of the APE and consist mostly of buildings constructed in 1970 or later, and two built in the mid- to late 1950s.

Commercial/Industrial

Resource ID 1, Metroplex Piano, Inc.

Metroplex Piano, Inc. is a commercial retail property specializing in piano sales, built in 1972. The building is one and two-stories with a flat roof and large recessed windows. The main block is one-story concrete and the rear block is two story brick. The building is Modern with Brutalist elements including the use of sculptural exposed concrete, heavy massing and deeply-recessed windows. The building does not appear to have any integrity issues: location, materials, design, workmanship, setting, feeling and association are all intact. Based on reconnaissance-level research, the property does not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. While the building exhibits some features of Modern architecture such as its flat roof, lack of ornament, use of exposed concrete and large windows, it is modest in size and does not clearly illustrate enough of the distinctive characteristics of Modernism including a light and airy structure, and exposed structural elements, in order to be a good representation of that style. Nor does it contain enough of the salient features of Brutalism including exposed wood form, recessed windows that read as voids (this window is too large), repeating patterns and strong right angles (Brown 2010). Metroplex Piano is recommended *not eligible* under Criterion C.

Resource ID 2, Kazy's Gourmet Restaurant

Kazy's Gourmet is a restaurant built in 1971. This vernacular modern, two-story, brick building has a flat roof, rows of large fixed windows on the façade, and a yellow awning. The building appears to have integrity of location, setting, feeling and association; however, a one-story addition may have been made later on the rear elevation and it also appears that windows on the first story façade have been partially filled in with brick, compromising integrity of materials, design and workmanship. Based on reconnaissance-level research, the property does not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. The building, while exhibiting some characteristics of Modern architecture such as a flat roof and large, fixed windows, is modest in size and does not clearly illustrate enough of the distinctive characteristics of the Modern style, design, construction or form such as the absence of ornamentation and exposed structural elements to be eligible under Criterion C.

Resource ID 3, 5 and 6, Gas Stations (and/or former gas stations)

Resource ID 3, 5 and 6 are all commercial automotive buildings. Resource 3, Richland Auto Repair, was built in 1972 and appears to have been a gas station with an oblong box and front-facing canopy, flat roof, and concrete block construction. The southeast elevation features four garage bays. The building does not appear to have any integrity issues: location, materials, design, workmanship, setting, feeling and association are all intact. It is unclear whether original signage is extant.

Resource 5, Ted's Automotive, was built in 1970 and is also an auto repair shop in the form of an oblong box and front-facing canopy with a side-gabled metal roof and front-gabled metal canopy. It is a steel building with five garage bays on the southwest façade. The building does not appear to have any integrity issues: location, materials, design, workmanship, setting, feeling and association are all intact.

Resource 6 is Miller Grass, an office building, which also appears to have been a gas station in the past as it too has an oblong box and canopy form with a flat mansard-style roof, brick siding and one garage bay on the south façade. It also features a canopy on the south elevation which appears to have been added ca. 2004 and a metal one-story addition made on the west elevation in phases between 2007 and 2014. The building retains integrity of location, workmanship, setting, feeling and association; however, due to the canopy and one-story additions to the west and south elevations over the years, integrity of materials and design are compromised.

Based on reconnaissance-level research, the properties do not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. None of the buildings are particularly noteworthy in design, nor are they rare or unusual examples of a type, or clearly illustrate enough of the distinctive characteristics of any style (Jones 2003). Therefore, they are recommended as *not eligible* under Criterion C.

Resource ID 4, Strip Center

Resource 4 is a retail strip center built in 1973. It is a vernacular modern brick building featuring stone veneer, fixed retail windows, and a flat roof with mansard-style awnings. The current awning may cover an original awning. The building does not appear to have any integrity issues: location, materials, design, workmanship, setting, feeling and association are all intact. Based on reconnaissance-level research, the property does not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. The building does not exhibit any distinctive characteristics of style, design, construction or form to be eligible under Criterion C.

Resource ID 7, 8 and 9, Commercial Warehouses

Three properties in the APE are commercial warehouses. Resource 7 is the International Paper building, constructed in 1958. This is a large (approximately 171,000 square feet) concrete block, one and two-story building with articulated bays, a flat roof, and rows of fixed windows. The building does not appear to have received any additions over the years. It retains integrity of location, materials, design, workmanship, setting, feeling and association.

Resource 8 is a former Safeway Distribution Center built between 1952 and 1956 along the railroad spur at the Whiterock Station on the Gulf, Colorado & Santa Fe Railway. This building is an enormous (approximately 900,000 square feet) rectangular, concrete structure with over forty garage bays. The building is currently a distribution center for several businesses. The building appears to have received a large addition on the west elevation, and smaller additions on the north and east elevations between 1956 and 1972. The building has retained integrity of location, setting, feeling and association. The additions do not detract from the building's integrity of materials, design, or workmanship because they were added during the historic period of significance for the building (before 1973) and they are done in scale and proportion to the rest of the building. Therefore, the building retains integrity of all seven aspects.

Resource 9 is a U-Haul storage building constructed in 1971. The building is a concrete aggregate, three-story structure with articulated bays and a flat roof. It has very few windows. The building does not appear to have received any additions or major changes to materials. It has retained integrity of location, materials, design, workmanship, setting, feeling and association.

Based on reconnaissance-level research, the properties do not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. The buildings are vernacular modern and do not clearly illustrate enough of the distinctive characteristics of any style, design, form or construction to be *eligible* under Criterion C.

Funerary

Resource ID 10, Eastgate Funeral Home

The Eastgate Funeral Home was built in 1973. It is a one story, limestone building with a mansard-type roof, a porte cochère on the east elevation and a canopy on the south façade. The porte cochère appears to have been added ca. 2000. Windows vary but are mostly single-hung with 9 lights. The stand-alone Modern sign is extant. The entry door is wood with a large sidelight. The building has retained integrity in location, materials, workmanship, setting, feeling and association. There has been a slight loss of integrity in design as the porte cochère was added in 2000. Based on reconnaissance-level research, the property does not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. The building, while exhibiting some characteristics of Modern architecture such as a flat roof and large, fixed windows, does not clearly illustrate enough of the distinctive characteristics of the style, such as the absence of ornamentation and exposed structural elements, and therefore is recommended *not eligible* under Criterion C.

Educational

Resource ID 11, Remington College

The Remington College building was built in 1972. This vernacular modern building is a rectangular, two-story, blonde brick building with a flat concrete cantilevered roof. Windows are full-façade and fixed. The building appears to have received a small addition to the northwest corner ca. 2002. The building has retained integrity in location, materials, workmanship, setting, feeling and association. Based on reconnaissance-level research, the property does not appear to have any association with significant events or persons of importance to be eligible under Criteria A or B. The building, while exhibiting some characteristics of Modern architecture such as a flat roof and large, fixed windows, does not clearly illustrate enough of the distinctive characteristics of the Modern period, such as the absence of ornamentation and exposed structural elements, and therefore is recommended *not eligible* under Criterion C.

Domestic

Resource ID 12a/12b, Domestic Properties in the New World Neighborhood

Two domestic properties in the APE are part of a mid-twentieth century neighborhood: resources 12a and b are part of the New World Neighborhood, established in 1964. The two properties are 5301 (12a) and 5305 (12b) La Jolla Drive, both built in 1966. They are modest, one-story Ranch-style homes. Resource 12a is a brick veneer, gable front-and-wing building with a two car garage facing



Photo 3: View southeast of La Jolla Drive Note sidewalks.

the street. Resource 12b is also a brick veneer, gable front-and-wing building with a two-car garage facing the street. The gable end on 12b features white fish-scale shingles.

The neighborhood was platted as Montclair 1, 2 and 3, and New World 1, with boundaries roughly consisting of IH 635 on the southwest, Northwest Highway on the northeast, Springlake Drive on the southeast, and Chevy Chase Drive on the northwest. It appears the bulk of homes were built between 1966 and 1970. In approximately 1970, Northwest Highway was widened to a four-lane road, and the Meadowcreek Village and Meadowcreek Park neighborhoods were platted north of it. By 1970, boundaries of New World had extended southward to La Prada. See map of boundaries in Appendix B.

The neighborhood features a hierarchy of roadways consisting of a large bisecting boulevard (Broadmoor Drive) which gives access to smaller interior streets, cul de sacs, and alleyways. Homes are modest Ranch-style, frame construction on concrete slab with brick or stone veneer siding, cross-gabled roofs often with shingled gable ends, deeply recessed entry ways and two-car garages. Houses are both one and two stories and ornamentation varies greatly. Most houses back to alleys where access is provided for driveways and garages; however, houses on the perimeter streets with no alley access have front-facing garages. The average one-story home size is 1,200 square feet with an average lot size of 7,400 square feet and an approximate 30-foot setback from the roadway. Alleyways provide access to the garage and driveway. Original sidewalks are present throughout the neighborhood.

Montclair Elementary School is in the middle of the northwest quadrant of the neighborhood. It received additions to its southeast elevation ca. 1972 and 1979, and to its northwest elevation ca. 1995. The New World United Methodist Church was built along with the neighborhood in 1968 on the northeast side of Northwest Boulevard but has been replaced with a new building ca. 1979. Small parks are scattered throughout the neighborhood and include Dorfman, Armstrong, Grissom, and White. Playgrounds appear to be the only amenities in the parks and they appear to have changed very little over time.



Photo 4: Montclair Elementary built in 1966 with neighborhood.



Photo 5: View southwest of Armstrong Park from Birchwood Drive and Penn Street.



Photo 6: View west of Dorfman Park from Springlake Drive.



Photo 7: View southwest of Grissom Park from Rockledge and Hollow drives.

The neighborhood has uniformity of setbacks, side yards, and lot sizes. The original school is extant and has retained integrity of location, setting, feeling and association; however, it has had several large additions which compromise its integrity of materials, design and workmanship. The other original public building in the neighborhood, the church, was replaced with a newer building in 1979, which is outside of the historic period for this project. Houses vary from one to two stories and many feature unsympathetic replacement materials and non-historic additions such as newer siding materials, out-of-style porch posts, enclosed garages, and sometimes two-story additions.

The neighborhood has retained its circulation patterns and parks. Park signage appears to be original but new street signs have been installed as part of the Neighborhood Vitality Matching Grant Program, which allows neighborhoods to design custom street sign toppers to be installed by the city. There are no real displays of significant or distinctive landscape features designed by architects or engineers that form a pattern or uniformity that separates the neighborhood from surrounding areas. Other neighborhoods built later, such as Meadowcreek Village and Meadowcreek Park, look very similar and it is difficult to determine the neighborhood's boundaries. While the neighborhood has integrity of location and setting, its integrity of materials, design and workmanship is compromised due to non-historic siding and additions, the new street signs, the large additions made to the school, and the replaced church building. Feeling and association are also slightly compromised due to the unsympathetic materials used freely throughout the neighborhood, and unsympathetic additions, which detract from the uniformity of the original plan. Therefore, it is not recommended eligible under Criterion C.



Photo 6: Typical unaltered New World Neighborhood house on La Jolla Drive. Shutters may have been added later.

While the neighborhood falls within the general definition of mid-century modern neighborhoods, it was built relatively late in that time period (most houses were built between 1966 and 1970) and does not appear to have had association with significant events or persons in the community, based on reconnaissance-level research, to be eligible under Criterion A or B.

Resources 12a and 12 b are not recommended individually eligible under Criterion A or B as they are not associated with any significant events or persons. They are also not recommended eligible under Criterion C as they are modest Ranch-style houses that are not particularly noteworthy in design, nor are they rare or unusual examples of a type, or clearly illustrate enough of the distinctive characteristics of any style (Jones 2003).



Photo 7: View of neighborhood street with sidewalk and homes in varying styles.



Photo 8: Atypical house with unsympathetic siding and porch posts at 1125 Intervale Drive.



Photo 9: View of neighborhood homes. Note two-story addition on right.

Properties not eligible for the NRHP, but had potential for significance prior to full evaluation:

Resource ID 13, Dallas Athletic Club

The Dallas Athletic Club was built in 1956 at the southwest corner of IH 635 and La Prada Drive. It was designed by Ralph Plummer, a productive golf course designer and student of noted golf course architect, Joseph Bredemus. The club is situated on approximately 300 acres with 36 holes, seven buildings, a pool, and tennis courts. Plummer designed both the blue and gold courses. Jack Nicklaus won the 45th PGA Championship played on the Blue Course in 1963. Both courses were renovated by Nicklaus in 1984. The buildings were also updated in the 1980s and in addition to several large utility buildings, a 3,600-square-foot building was constructed in 2004.



Photo 10: View of golf course from Woodmeadow Parkway, looking west. Fairway Estates Neighborhood built in 1985 in the distance.



Photo 11: Main club building, view north.



Photo 12: View east.



Photo 13: View of rear elevation looking west.

Historic photos of the buildings were not available during reconnaissance research; therefore the bulk of analysis is based on historic aerial photos. It appears that the original layout included the large main building, pool, and tennis courts at the south edge of the club property along La Prada Drive; a rectangular building in the middle of the western portion of the property; and a rectangular building at the southeast corner of the property near La Prada Drive and Woodmeadow Parkway.



Photo 14: Historic aerial photo of Dallas Athletic Club from 1972.

The main building is a large (over 50,000 sf) meandering wood frame building with stucco and stone siding, a mansard-style roof, and two porte cocheres. The rear elevations feature rows of large fixed windows. Circa 1979, the main building received an addition to the northwest elevation and the pool deck was increased. Four new tennis courts were constructed west of the original five in 1979 and two more added east of them in 2004. The main building was more recently renovated by Abstract Construction in 2013 including the addition of a porte cochere on its southwest façade.

By 1989, Nicklaus had renovated the golf courses. In reviewing historic aerials, it appears to have been sensitively implemented: while there are some additional ponds and pathways appear to be more pronounced with bridges, overall it appears to have retained its general design. The club staff could not be reached during research to confirm changes made from the redesign.



Photo 15: Aerial photo of Dallas Athletic Club in 1989. Note Fairway Estates and Club Manor neighborhoods in lower half of aerial.



Photo 16: View of golf course.



Photo 17: View northwest of playscape by pool.

In 1989, the rectangular building at the southeast corner of the property was demolished and additional buildings were constructed next to the building in the middle, western portion of the park. Also, that year, the Fairway Estates neighborhood was constructed on the southeast edge of the club along La Prada Drive and the Club Manor Neighborhood was constructed at the southwest corner of the park at La Prada Drive and Barnes Bridge Road. An entry building was added ca. 2000, the parking lot extended west ca. 2004, and a playground/playscape was added south of the pool area.

While there may still be integrity of location, setting and association, the club's integrity of materials, design, and workmanship are all compromised due to renovations to the two courses in the 1980s, updates and major additions made to the main building and the middle building. The construction of the Fairway Estates and Club Manor neighborhoods on club grounds in 1985 demolished an original building and significantly altered the integrity of setting and feeling with the introduction of neighborhoods on golf course property. The golf course could be eligible under Criterion A and B for its association with the 45th PGA Golf Tournament, featuring Jack Nicklaus, however due to the lack of integrity in several aspects it is recommended not eligible. Moreover, the athletic club buildings have changed so significantly over time, that the club is not recommended eligible under Criterion C.

Properties eligible for the NRHP:

None of the properties in the APE are recommended as eligible for the NRHP.

Comments on Evaluations: None

Effects: None of the properties in the APE are recommended eligible to the NRHP. Therefore, there would be no direct, indirect or cumulative effects on historic properties in the APE.

Further Work: No further work recommended.

Justification: N/A

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| Inventory of Surveyed Properties for IH 635 East, Dallas County | | | | | | |
|---|--|------------------------|--|--------------------|---|---|
| Resource ID No. | Resource Name and Address/Latitude-Longitude | Property Type: Subtype | Stylistic Influence and Form | Construction Dates | Integrity Issues/Notes | NRHP Status and Eligibility Recommendations |
| 1 | Metroplex Plano Inc. 9292 LBJ Fwy., Dallas, TX 75243 32.913924°/-96.740443° | Commercial: Retail | Modern/ Flat | 1972 | None | Not Eligible |
| 2 | Kazy's Gourmet 9256 Markville Drive, Dallas, TX 75243 32.913519°/-96.740162° | Commercial: Restaurant | Modern/ Flat with awnings | 1971 | Infill on first-story façade compromising integrity of materials, design and workmanship. | Not Eligible |
| 3 | Richland Auto Repair 9301 LBJ Fwy., Dallas, TX 75243 32.914370°/-96.738419° | Commercial: Automotive | Modern/Oblong box with canopy | 1972 | None | Not Eligible |
| 4 | Retail Strip Center 9730 Abrams Road, Dallas, TX 75205 32.908585°/-96.734967° | Commercial: Business | Modern/ Flat | 1973 | None | Not Eligible |
| 5 | Ted's Automotive 10551 New Church Road, Dallas, TX 75238 32.885914°/-96.702848° | Commercial: Automotive | Modern/Oblong box with canopy | 1970 | None | Not Eligible |
| 6 | Miller Grass Office 10025 Plano Road, Dallas, TX 75238 32.884905°/-96.700773° | Commercial: Business | Modern/Oblong box with mansard roof and canopy | 1970 | Canopy and one-story additions to the west and south elevations compromise integrity of materials and design. | Not Eligible |
| 7 | International Paper 11333 Kingsley Road, Dallas, TX 32.879820°/-96.684920° | Commercial: Warehouse | Modern/Flat | 1958 | None | Not Eligible |
| 8 | Distribution Center 2600 McCree Road, Garland, TX 32.872590°/-96.673077° | Commercial: Warehouse | Modern/Flat | 1952-56 | Addition made to west elevation ca. 1970. | Not Eligible |
| 9 | U-Haul Moving and Storage 12215 LBJ Fwy., Garland, TX 75041 32.870063°/-96.668206° | Commercial: Warehouse | Modern/Flat | Circa 1971 | None | Not Eligible |

| Inventory of Surveyed Properties for IH 635 East, Dallas County | | | | | | |
|---|--|---|------------------------------|--------------------|---|---|
| Resource ID No. | Resource Name and Address/Latitude-Longitude | Property Type: Subtype | Stylistic Influence and Form | Construction Dates | Integrity Issues/Notes | NRHP Status and Eligibility Recommendations |
| 10 | Eastgate Funeral Home 1910 Eastgate Drive, Garland, TX 75041 32.862241° / -96.656671° | Funerary: Mortuary | Modern/Mansard with canopy | 1973 | Added porte cochere to east elevation between 1995 and 2001. Integrity of materials and design compromised. | Not Eligible |
| 11 | Remington College 1800 Eastgate Drive, Garland, TX 75041 32.860636° / -96.654022° | Education: College | Modern/ Flat | 1972 | Small addition to the northwest corner ca. 2002. | Not Eligible |
| 12A | 5301 La Jolla Drive, Garland, TX 75041 32.853684° / -96.644171° | Domestic: Single- family Dwelling | Ranch/Gable front and wing | 1966 | None | Not Eligible |
| 12B | 5305 La Jolla Drive, Garland, TX 75041 32.853529° / -96.644012° | Domestic: Single- family Dwelling | Ranch/Gable front and wing | 1966 | None | Not Eligible |
| 13 | Dallas Athletic Club 4111 Athletic Club Drive, Dallas, TX 75228 32.843195° / -96.636146° | Recreation and Culture: Sports Facility | Mediterranean/ Landscape | 1956 | Blue and Gold courses both redesigned in 1980. Buildings demolished, updated and added in 1980s/2004. Integrity of materials, design, workmanship, setting and feeling all compromised. | Not Eligible |

Survey Forms for All Surveyed Properties

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. **1**

Name and Address: **Metroplex Piano Inc., 9292 LBJ Fwy., Dallas, TX 75243**

Lat./Long.: **32.913924°/-96.740443°**

Architectural Style/Form: **Modern**

Construction Dates: **1972**

Resource Physical Description and Integrity Issues: **One-story concrete building with a rear two-story brick addition, flat roof and large recessed windows with rounded corners. The building is Modern with Brutalist elements including the use of sculptural exposed concrete, heavy massing and deeply-recessed windows. No apparent integrity issues.**

NRHP Eligibility Determination: **Not eligible.**

Investigative Limitations Encountered During Survey: **None**



Photo 1: View north of Metroplex Piano.



Photo 2: View south.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 2

Name and Address: Kazy's Gourmet, 9256 Markville Dr., Dallas, TX 75243

Lat./Long.: 32.913519°/-96.740162°

Architectural Style/Form: Modern

Construction Dates: 1971

Resource Physical Description and Integrity Issues: A vernacular modern, two-story, brick building with a flat roof, rows of large fixed windows on the façade, and a yellow awning and sign. A one-story addition may have been made later on the rear elevation, and windows on the first story façade have been partially filled in with brick, compromising integrity of materials, design and workmanship.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 3: View southwest of Kazy's Gourmet.



Photo 4: View south.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 3

Name and Address: Richland Auto Repair, 9301 LBJ Fwy., Dallas, TX 75243

Lat./Long.: 32.914370°/-96.738419°

Architectural Style/Form: No style

Construction Dates: 1972

Resource Physical Description and Integrity Issues: The building is an oblong box with a front-facing canopy, flat roof, and concrete block construction. The southeast elevation features four garage bays.

No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 5: View north of Richland Auto Repair.



Photo 6: View east.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 4

Name and Address: 9730 Abrams Rd., Dallas, TX 75205

Lat./Long.: 32.908585°/-96.734967°

Architectural Style/Form: No style

Construction Dates: 1973

Resource Physical Description and Integrity Issues: A vernacular modern, rectangular brick building featuring stone veneer, fixed retail windows, and a flat roof with mansard-style awnings. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 7: View northeast of strip center.



Photo 8: View southeast.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 5

Address: Ted's Automotive, 10551 New Church Rd., Dallas, TX 75238

Lat./Long.: 32.885914°/-96.702848°

Architectural Style/Form: No style

Construction Dates: 1970

Resource Physical Description and Integrity Issues: An auto repair shop in the form of an oblong box and front-facing canopy, a side-gabled metal roof and front-gabled metal canopy. It is a steel building with five garage bays on the southwest façade and no apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 9: View east of Ted's Automotive.



Photo 10: View southeast.

Resource ID No. 6

Name and Address: Miller Grass Office, 10025 Plano Rd., Dallas, TX 75238

Lat./Long.: 32.884905°/-96.700773°

Architectural Style/Form: No style

Construction Dates: 1970

Resource Physical Description and Integrity Issues: Converted gas station in the form of an oblong box and front-facing canopy with a flat mansard-style roof, brick siding and one garage bay on the south façade. It features a canopy on the south elevation which appears to have been added ca. 2004 and a metal one-story addition made on the west elevation in phases between 2007 and 2014. Due to the canopy and one-story additions to the west and south elevations, integrity of materials and design are compromised.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 11: View east of Miller Grass.



Photo 12: View south.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 7

Name and Address: International Paper, 11333 Kingsley Rd., Dallas, TX

Lat./Long.: 32.879820°/-96.684920°

Architectural Style/Form: Modern

Construction Dates: 1958

Resource Physical Description and Integrity Issues: A large (approximately 171,000 square feet) concrete block, one and two-story building with articulated bays, a flat roof, and rows of fixed windows. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 13: View north of International Paper.



Photo 14: View north.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 8

Name and Address: 2600 McCree Rd., Garland, TX

Lat./Long.: 32.872590°/-96.673077°

Architectural Style/Form: Modern

Construction Dates: Ca. 1952-1956

Resource Physical Description and Integrity Issues: A rectangular, concrete building, approximately 900,000 square feet in size, with over forty garage bays. The building is currently a distribution center for several businesses. The building appears to have received a large addition on the west elevation, and smaller additions on the north and east elevations between 1956 and 1972. The additions do not detract from the building's integrity of materials, design, or workmanship because they were added during the historic period of significance for the building (before 1973) and they are done in scale and proportion to the rest of the building. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 15: View west of distribution center.



Photo 16: View northeast of entrance and building.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 9

Name and Address: Uhaul Moving and Storage, 12251 LBJ Fwy., Garland, TX 75041

Lat./Long.: 32.870063°/-96.668206°

Architectural Style/Form: Modern

Construction Dates: 1971

Resource Physical Description and Integrity Issues: A concrete aggregate, three-story structure with articulated bays and a flat roof and very few windows. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 17: View west of Uhaul Storage.



Photo 18: View northeast.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 10

Address: Eastgate Funeral Home, 1910 Eastgate Dr., Garland, TX 75041

Lat./Long.: 32.862241°/-96.656671°

Architectural Style/Form: Modern

Construction Dates: 1973

Resource Physical Description and Integrity Issues: A one story, limestone building with a mansard-type roof, a porte cochère on the east elevation and a canopy on the south façade. The porte cochère appears to have been added ca. 2000. Windows vary but are mostly single-hung with 9 lights. The stand-alone Moderne sign is extant. The entry door is wood with a large sidelight. Integrity of design is slightly compromised due to the addition of the porte cochère.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 19: View northwest of Eastgate Funeral Home.



Photo 20: View northwest of sign and building.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 11

Name and Address: Remington College, 1800 Eastgate Dr., Garland, TX 75041

Lat./Long.: 32.860636°/ -96.654022°

Architectural Style/Form: Modern

Construction Dates: 1972

Resource Physical Description and Integrity Issues: A rectangular, two-story, blonde brick building with a flat concrete cantilevered roof. Windows are full-façade and fixed. The building appears to have received a small addition to the northwest corner ca. 2002. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 21: View east of Remington College.



Photo 22: View northeast of entrance.

Resource ID No. 12

Name and Address: New World Neighborhood

Resource ID No. 12A, 5301 La Jolla, Garland, TX 75041

Lat./Long.: 32.853684°/-96.644171°

Architectural Style/Form: Ranch

Construction Dates: 1966

Resource Physical Description and Integrity Issues: A modest gable front-and-wing, one-story, Ranch-style home with brick veneer and a two car garage facing the street. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 23: View southwest of 5301 La Jolla.



Photo 24: View south.

IH 635 East, Dallas County

CSJs: 2374-01-137, 01-180, 01-183, 02-053, 02-144

Resource ID No. 12

Name and Address: New World Neighborhood

Resource ID No. 12B, 5305 La Jolla, Garland, TX 75041

Lat./Long.: 32.853529°/-96.644012°

Architectural Style/Form: Ranch

Construction Dates: 1966

Resource Physical Description and Integrity Issues: A modest gable front-and-wing, one-story, Ranch-style home with brick veneer and a two car garage facing the street. The gable end features white fish scale shingles. No apparent integrity issues.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: None



Photo 25: View south of 5305 La Jolla.



Photo 26: View southwest.

Resource ID No. 13

Name and Address: Dallas Athletic Club, 4111 Athletic Club Dr., Dallas, TX 75228

Lat./Long.: 32.843195°/-96.636146°

Architectural Style/Form: Mediterranean/Landscape

Construction Dates: 1956

Resource Physical Description and Integrity Issues: A private athletic club and golf course situated on approximately 300 acres with 36 holes, eight recreational buildings and a pool. Both the blue and gold courses were renovated 1980. The buildings were also updated in the 1980s and in addition to several large utility buildings, a 3,600-square-foot building was constructed in 2004. Integrity of materials, design, and workmanship are all compromised due to renovations to the two courses in the 1980s as well as updates to the buildings. The construction of the Fairway Estates and Club Manor neighborhoods on club grounds in 1985 demolished an original building and diminishes integrity of setting and feeling.

NRHP Eligibility Determination: Not eligible.

Investigative Limitations Encountered During Survey: Did not obtain historic photos of buildings.



Photo 27: View west of Dallas Athletic Club and Long Branch Creek from bridge on Woodmeadow Parkway.

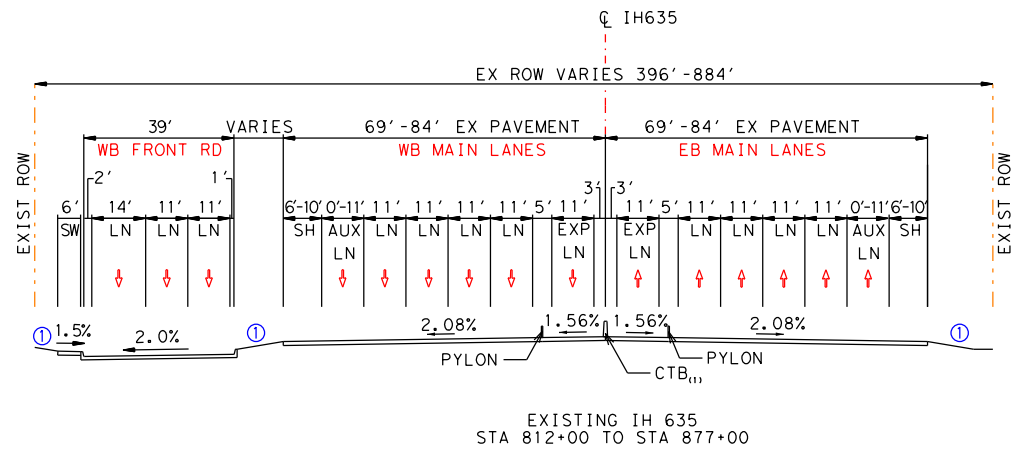
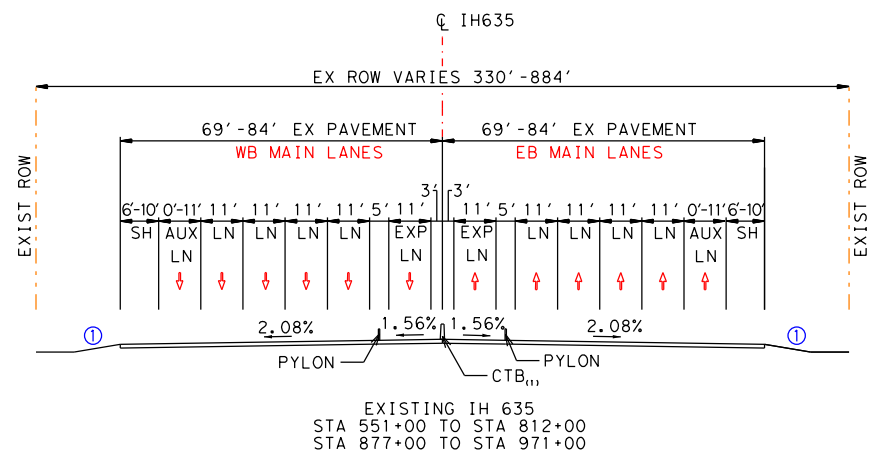
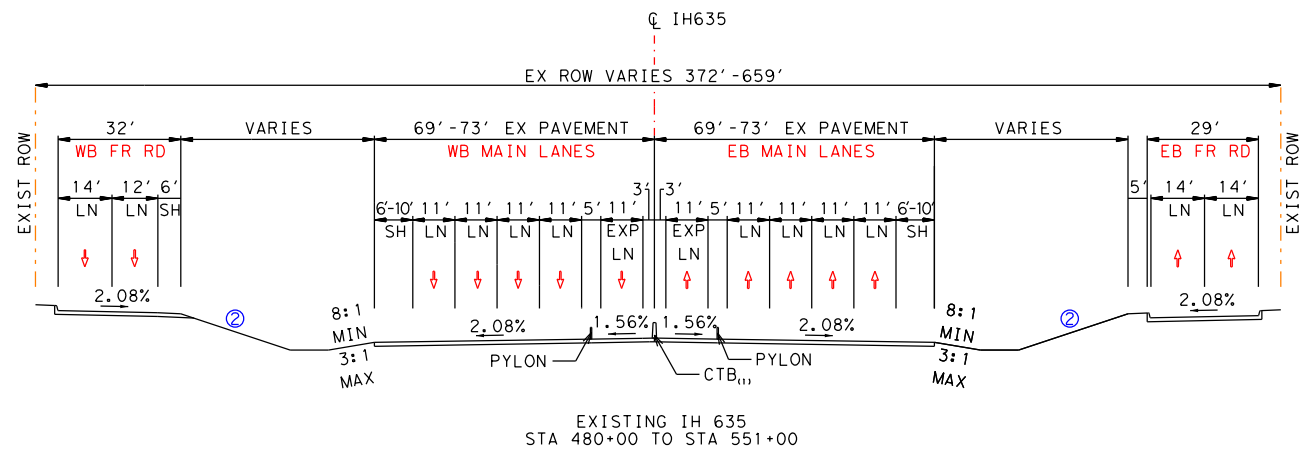


Photo 28: View northeast of main club house.

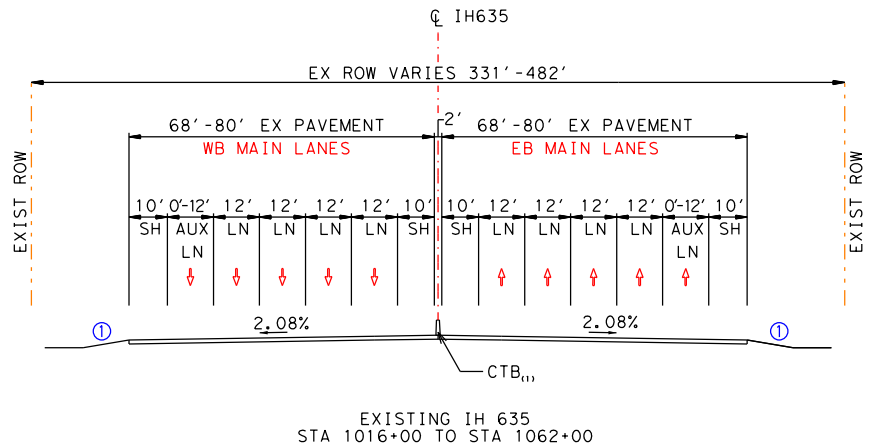
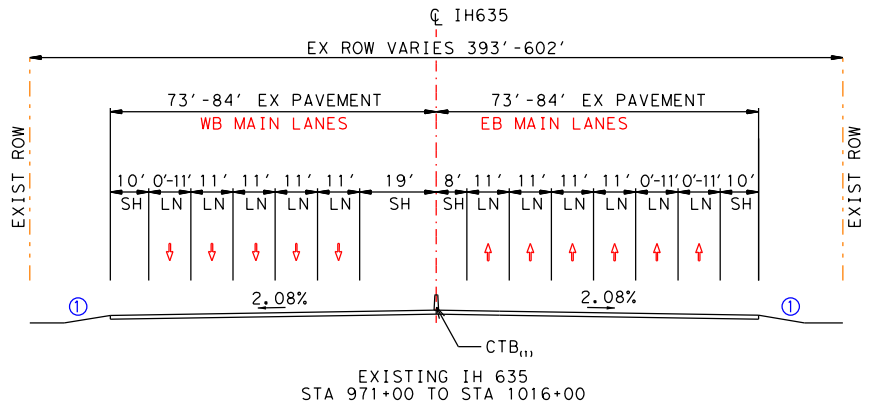
Figures

| | | |
|------------|------------------|----------|
| Appendix A | Typical Sections | 8 Pages |
| Appendix B | Project Maps | 18 Pages |

Appendix A – Typical Sections



- NOTES:
- ① 6:1 DESIRABLE
4:1 MAX
 - ② 4:1 DESIRABLE
3:1 MAX
 - ③ 4' DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS



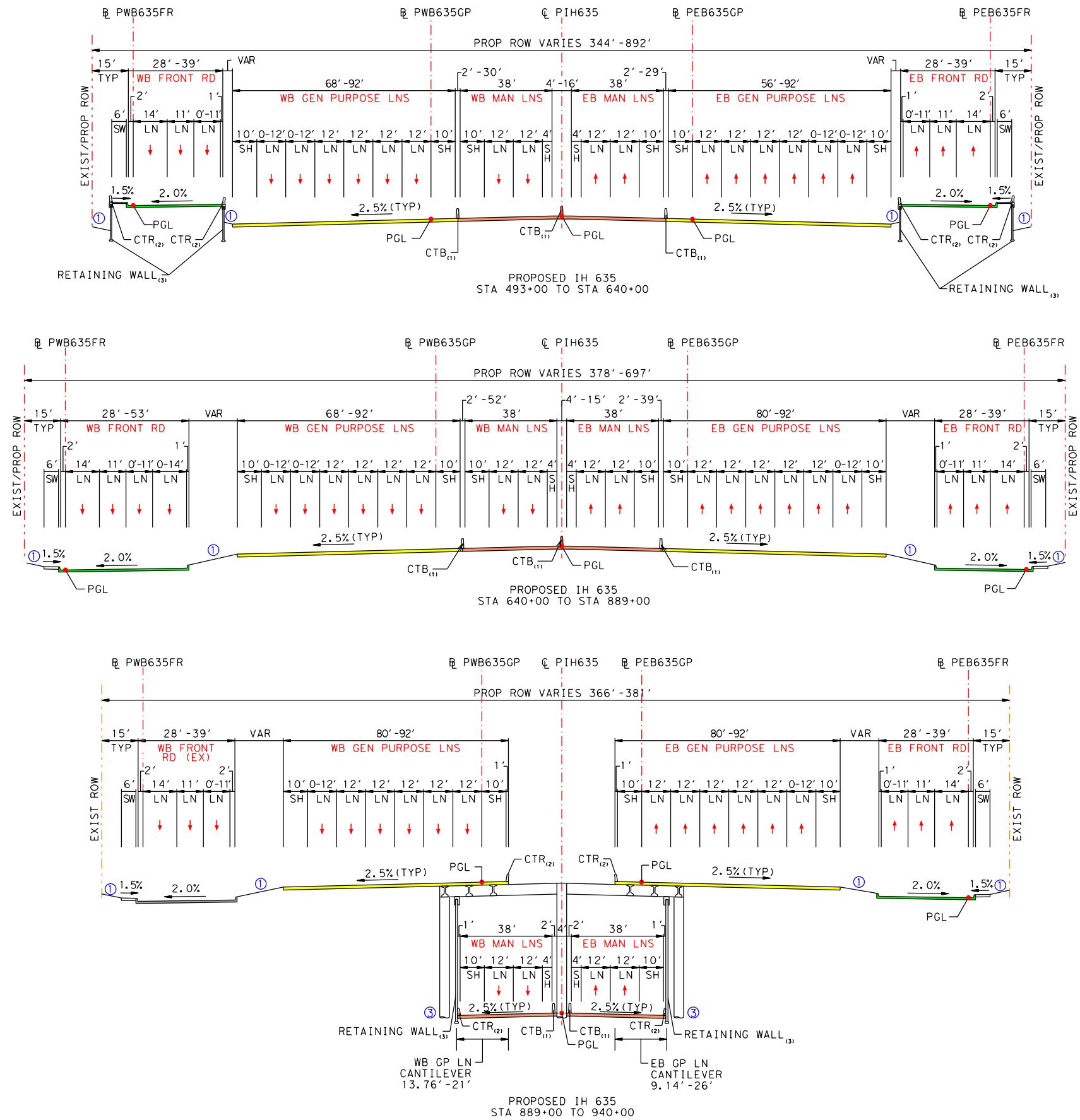
EXISTING IH 635 TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

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PROJECT TYPICAL SECTIONS
PAGE 1 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



NOTES:

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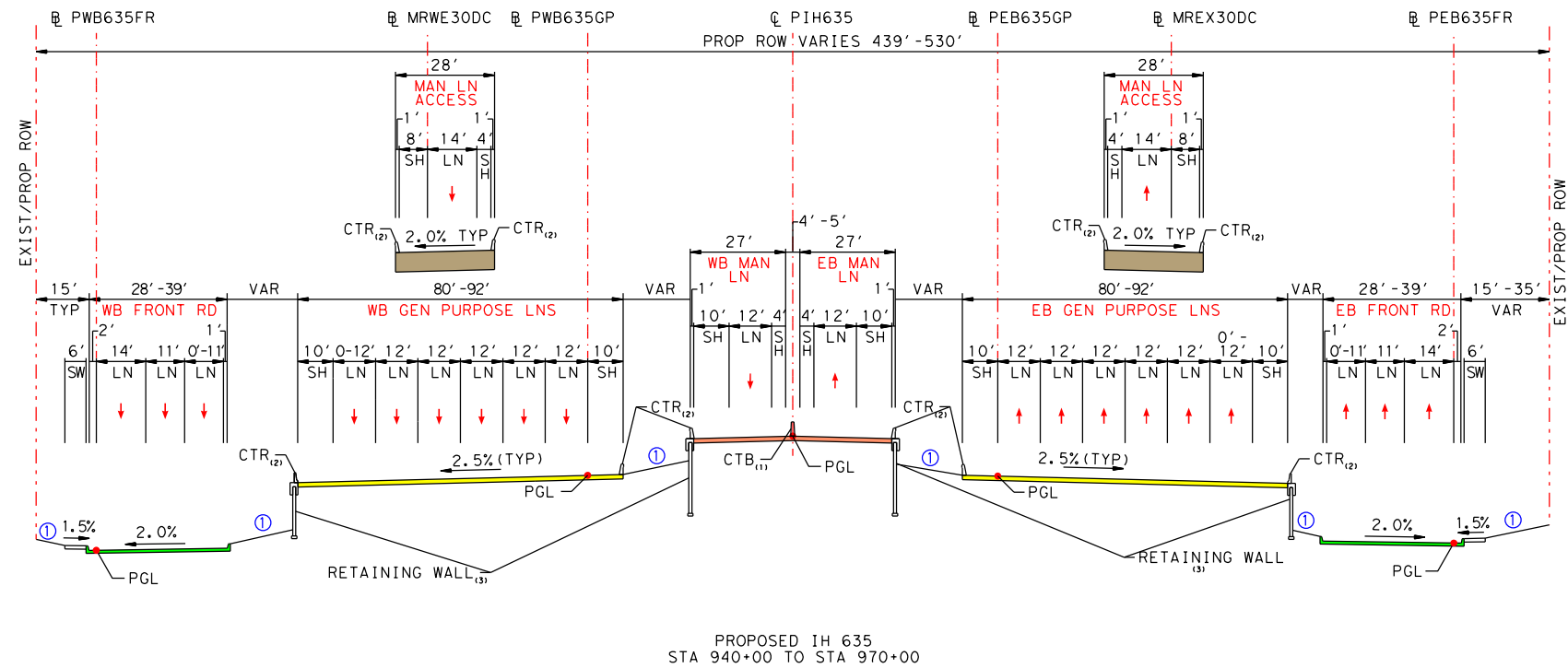
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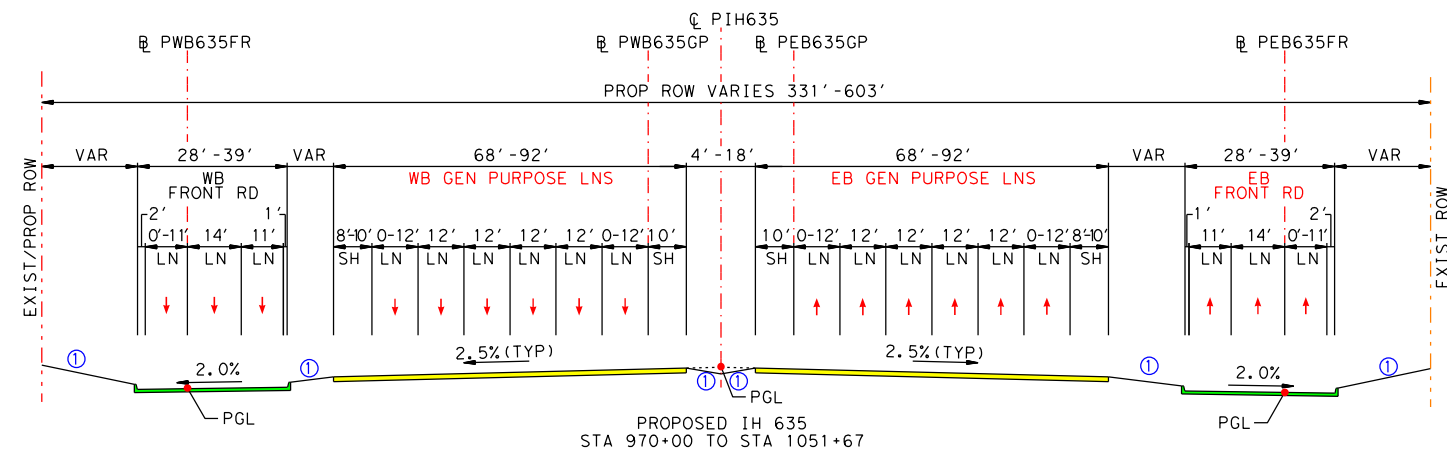
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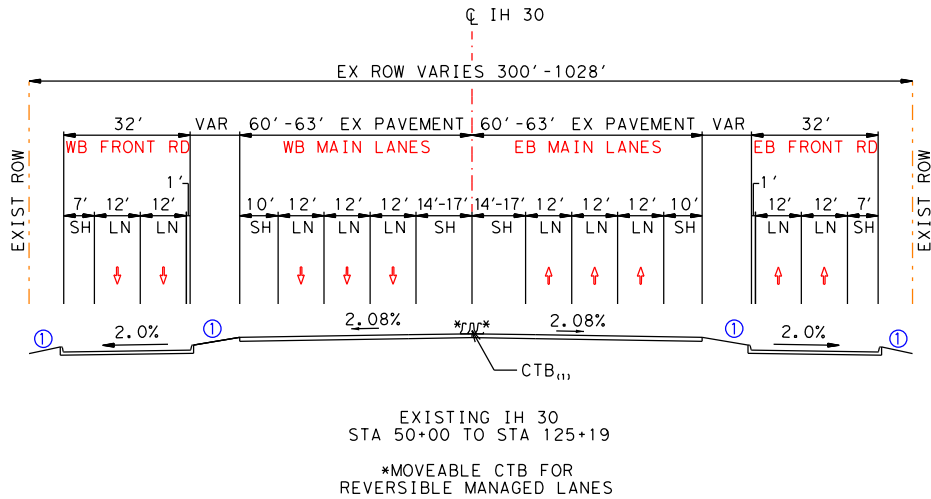
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PROJECT TYPICAL SECTIONS
PAGE 3 OF 8

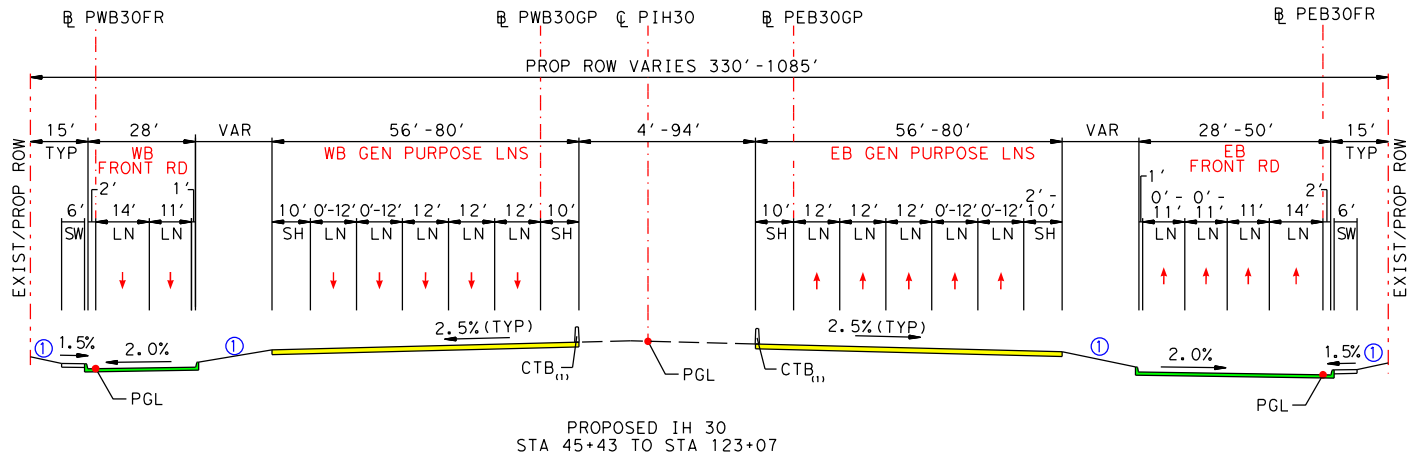
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EXISTING IH 30 TYPICAL SECTION



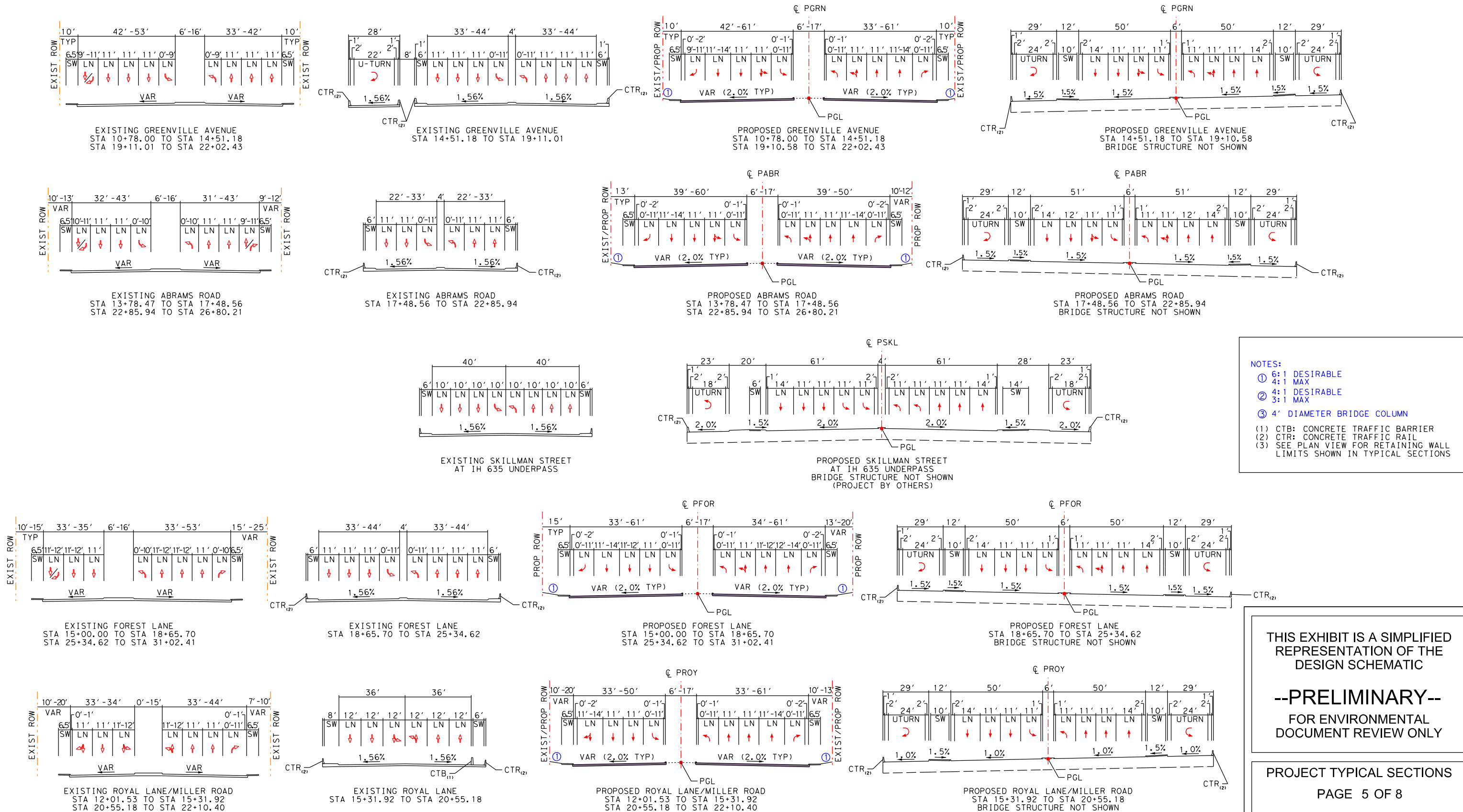
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PROJECT TYPICAL SECTIONS
PAGE 4 OF 8

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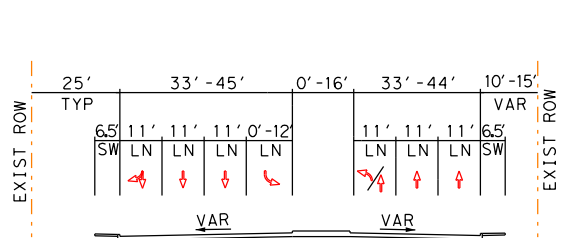
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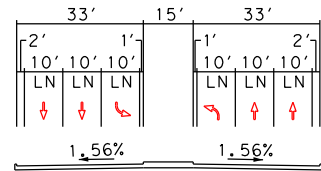
PROJECT TYPICAL SECTIONS
PAGE 5 OF 8

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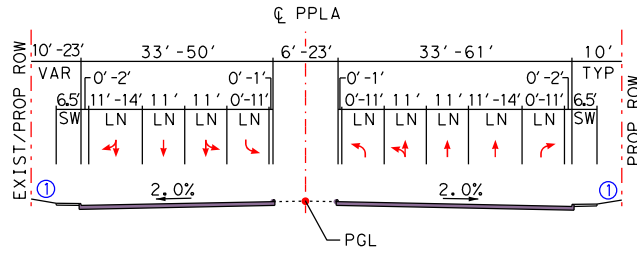
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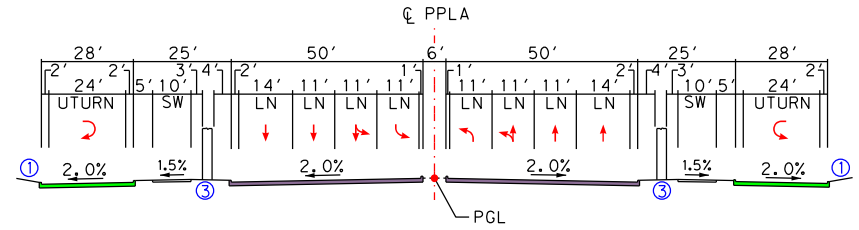
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STA 20+44.61 TO STA 21+50.00



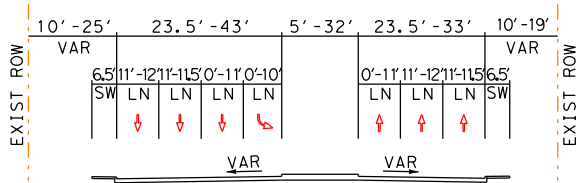
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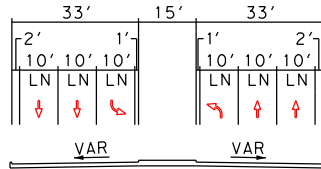
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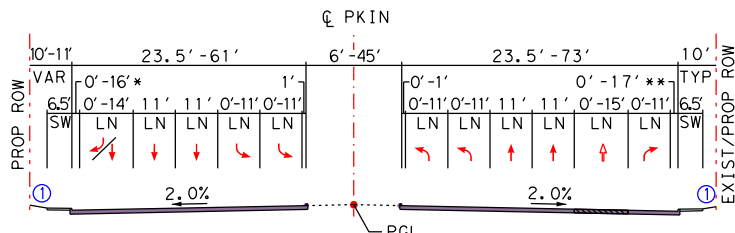
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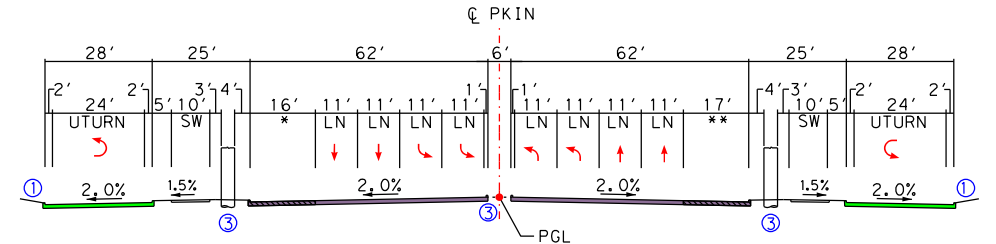
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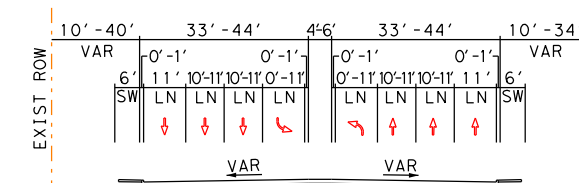
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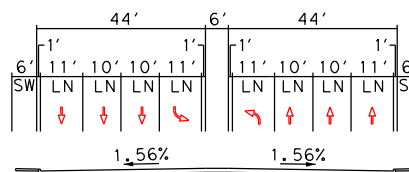
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**OUTSIDE 17' LANE STRIPED OFF FOR FUTURE USE



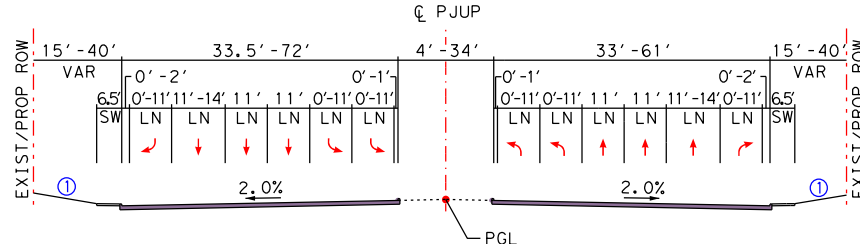
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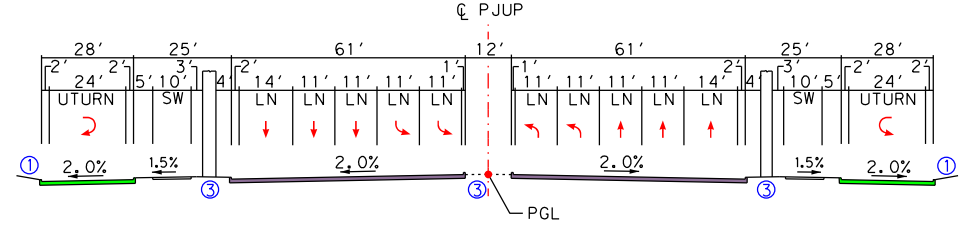
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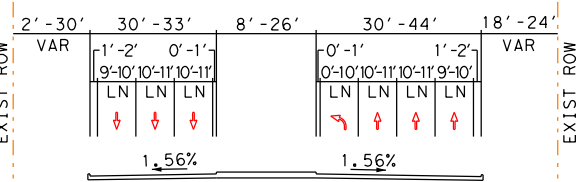
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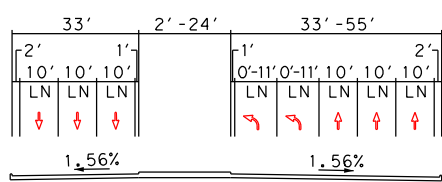
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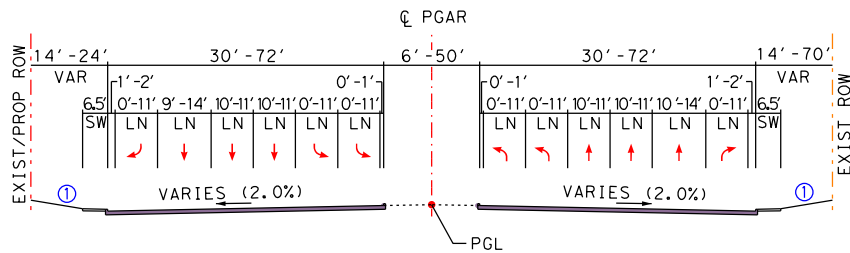
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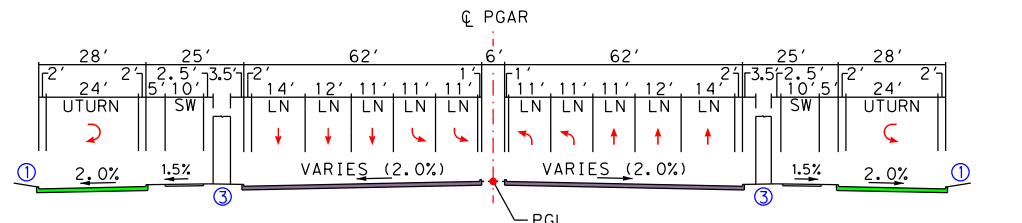
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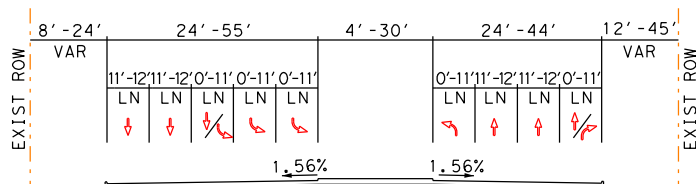
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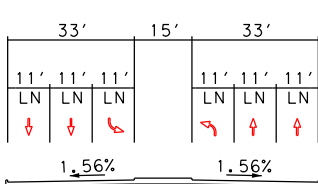
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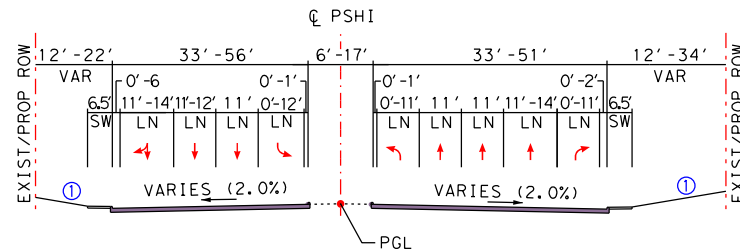
PROPOSED GARLAND AVENUE
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EXISTING SHILOH ROAD
STA 12+00.00 TO STA 14+50.69
STA 20+84.10 TO STA 26+19.86



EXISTING SHILOH ROAD
STA 14+50.69 TO STA 20+84.10



PROPOSED SHILOH ROAD
STA 12+00.00 TO STA 26+19.86

- NOTES:
- ① 6:1 DESIRABLE
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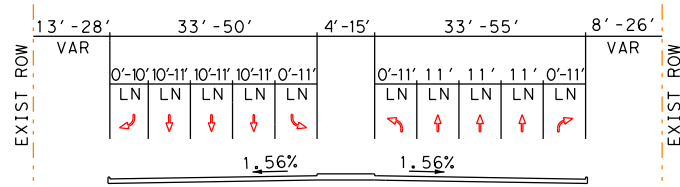
CROSS STREET TYPICAL SECTIONS

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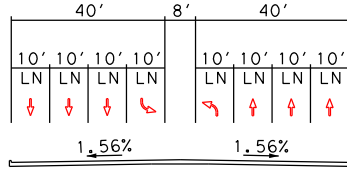
--PRELIMINARY--
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PROJECT TYPICAL SECTIONS
PAGE 6 OF 8

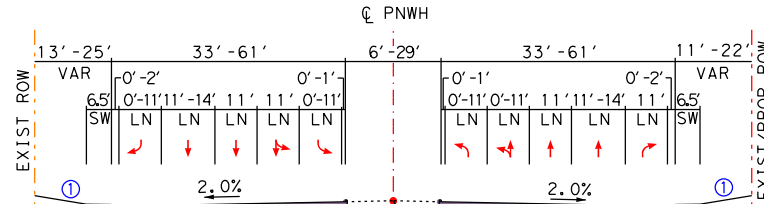
IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



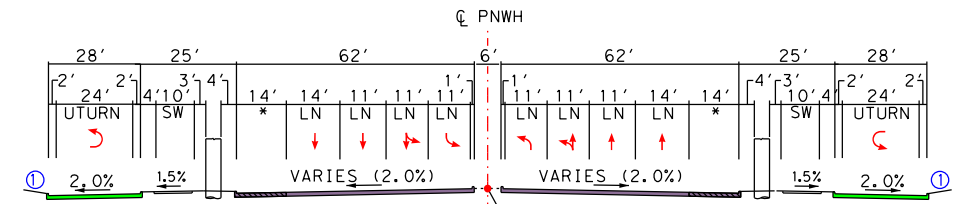
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STA 23+75.96 TO STA 27+23.74



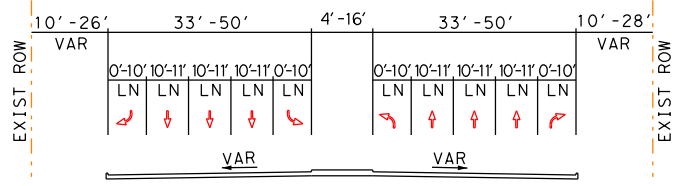
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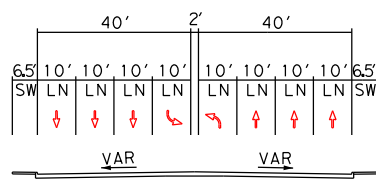
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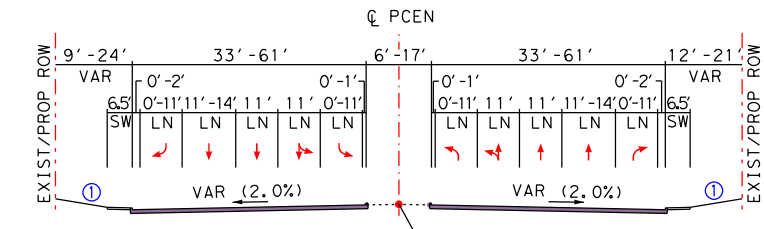
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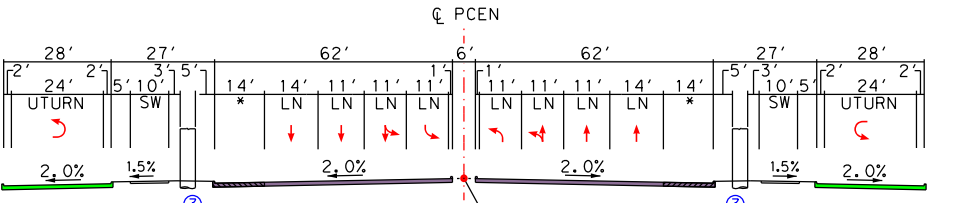
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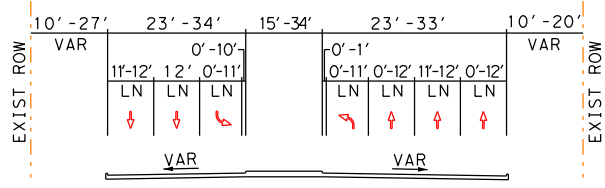
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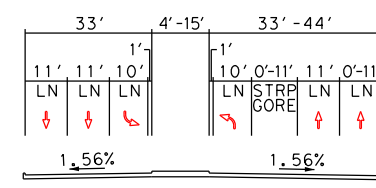
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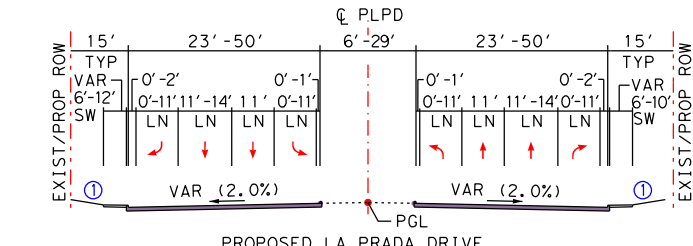
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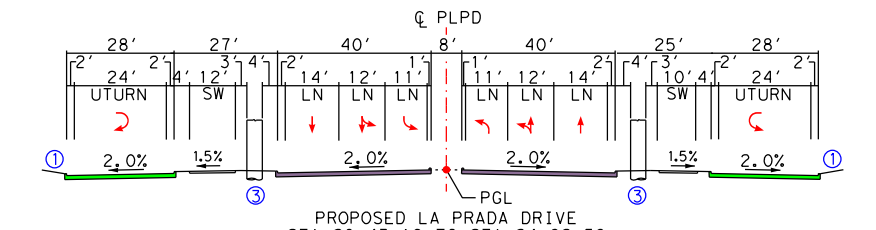
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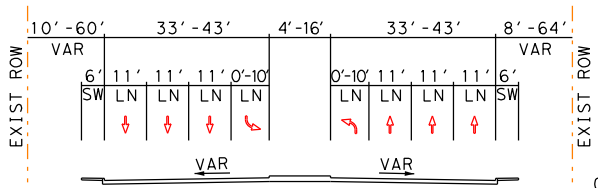
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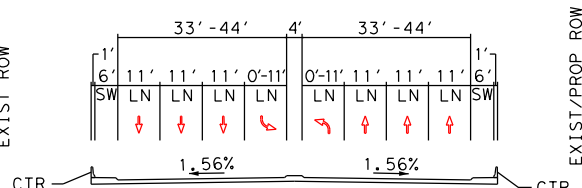
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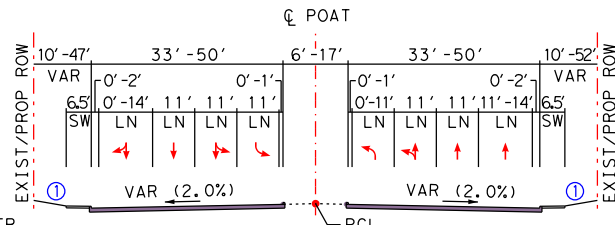
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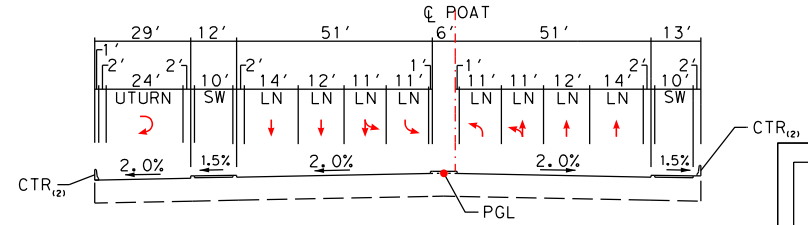
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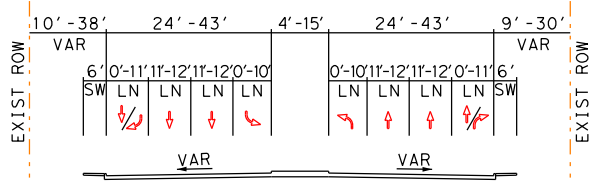
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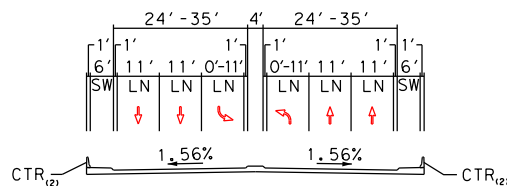
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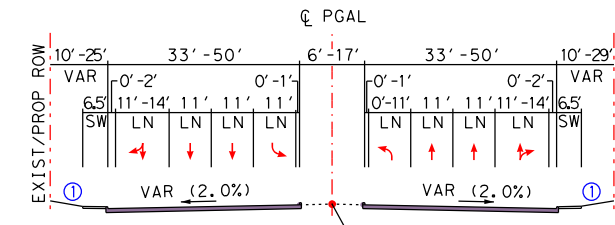
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BRIDGE STRUCTURE NOT SHOWN



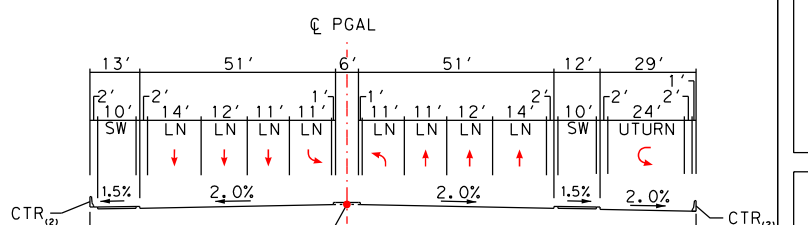
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STA 10+96.20 TO STA 14+74.69
STA 22+47.20 TO STA 28+14.95



EXISTING GALLOWAY AVENUE
STA 14+74.69 TO STA 22+47.20



PROPOSED GALLOWAY AVENUE
STA 10+96.20 TO STA 14+74.69
STA 22+47.20 TO STA 28+14.95



PROPOSED GALLOWAY AVENUE
STA 14+74.69 TO STA 22+47.20
BRIDGE STRUCTURE NOT SHOWN

- NOTES:
- ① 6:1 DESIRABLE
 - ② 4:1 MAX
 - ③ 4:1 DESIRABLE
 - ④ 3:1 MAX
 - ⑤ 4' DIAMETER BRIDGE COLUMN
 - (1) CTB: CONCRETE TRAFFIC BARRIER
 - (2) CTR: CONCRETE TRAFFIC RAIL
 - (3) SEE PLAN VIEW FOR RETAINING WALL LIMITS SHOWN IN TYPICAL SECTIONS

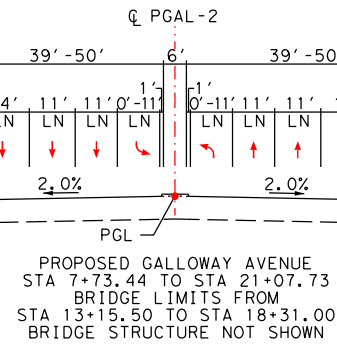
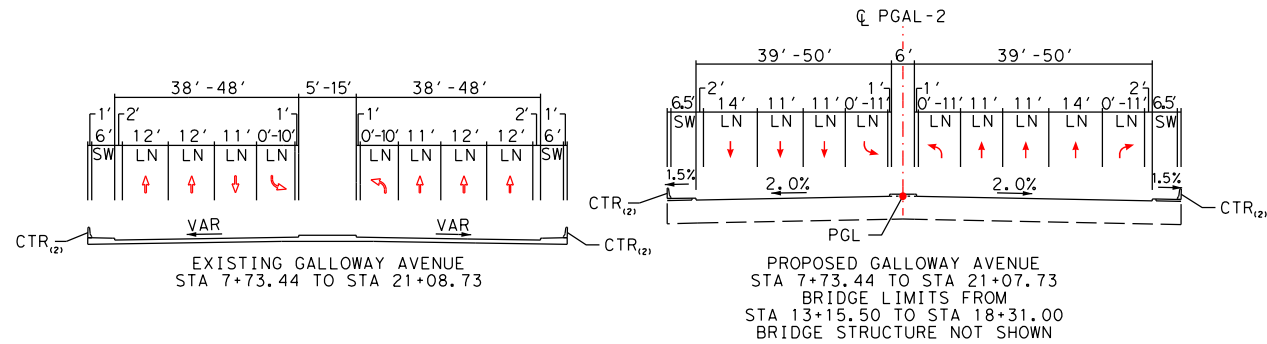
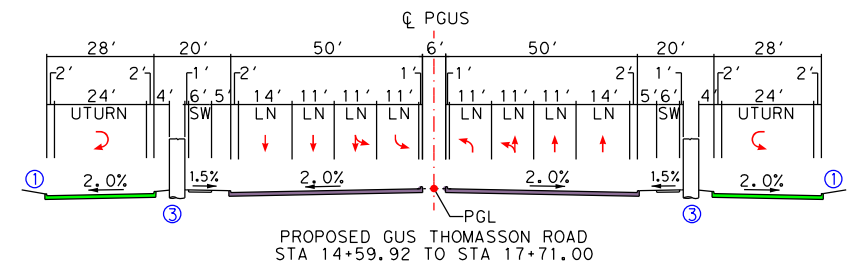
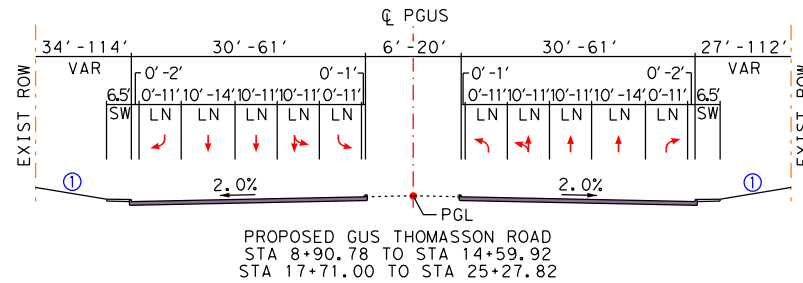
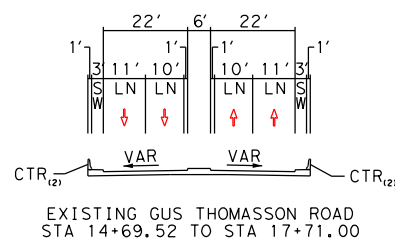
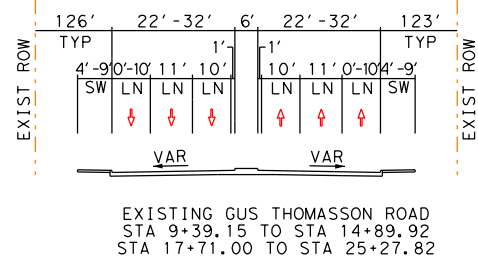
CROSS STREET TYPICAL SECTIONS

THIS EXHIBIT IS A SIMPLIFIED REPRESENTATION OF THE DESIGN SCHEMATIC

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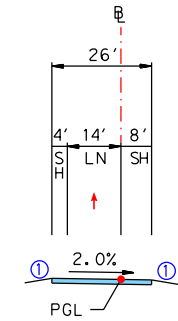
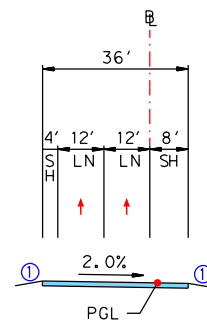
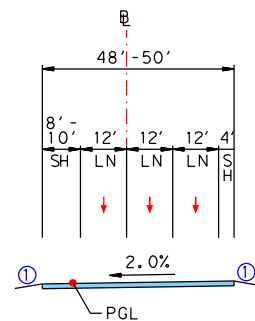
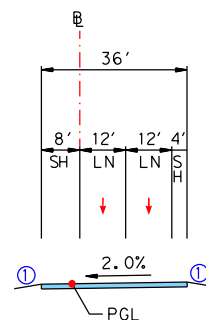
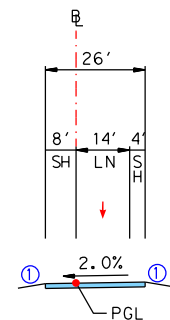
PROJECT TYPICAL SECTIONS
PAGE 7 OF 8

IH 635 FROM US 75 TO IH 30
CITIES OF DALLAS, GARLAND
AND MESQUITE,
DALLAS COUNTY, TEXAS
CSJs: 2374-01-137, 2374-01-180,
2374-01-183, 2374-02-053,
& 2374-02-144



CROSS STREET TYPICAL SECTIONS

- NOTES:
- ① 6:1 DESIRABLE
4:1 MAX
 - ② 4:1 DESIRABLE
3:1 MAX
 - ③ 4' DIAMETER BRIDGE COLUMN
- (1) CTB: CONCRETE TRAFFIC BARRIER
(2) CTR: CONCRETE TRAFFIC RAIL
(3) SEE PLAN VIEW FOR RETAINING WALL
LIMITS SHOWN IN TYPICAL SECTIONS



PROPOSED RAMP TYPICAL SECTIONS

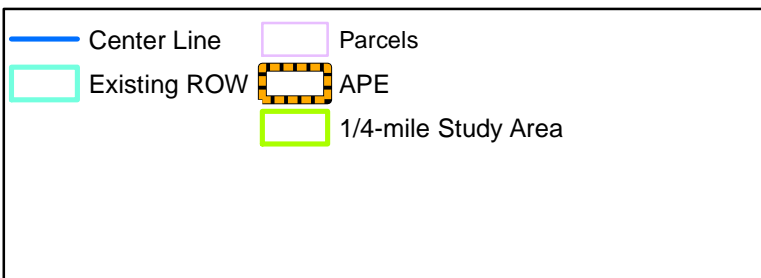
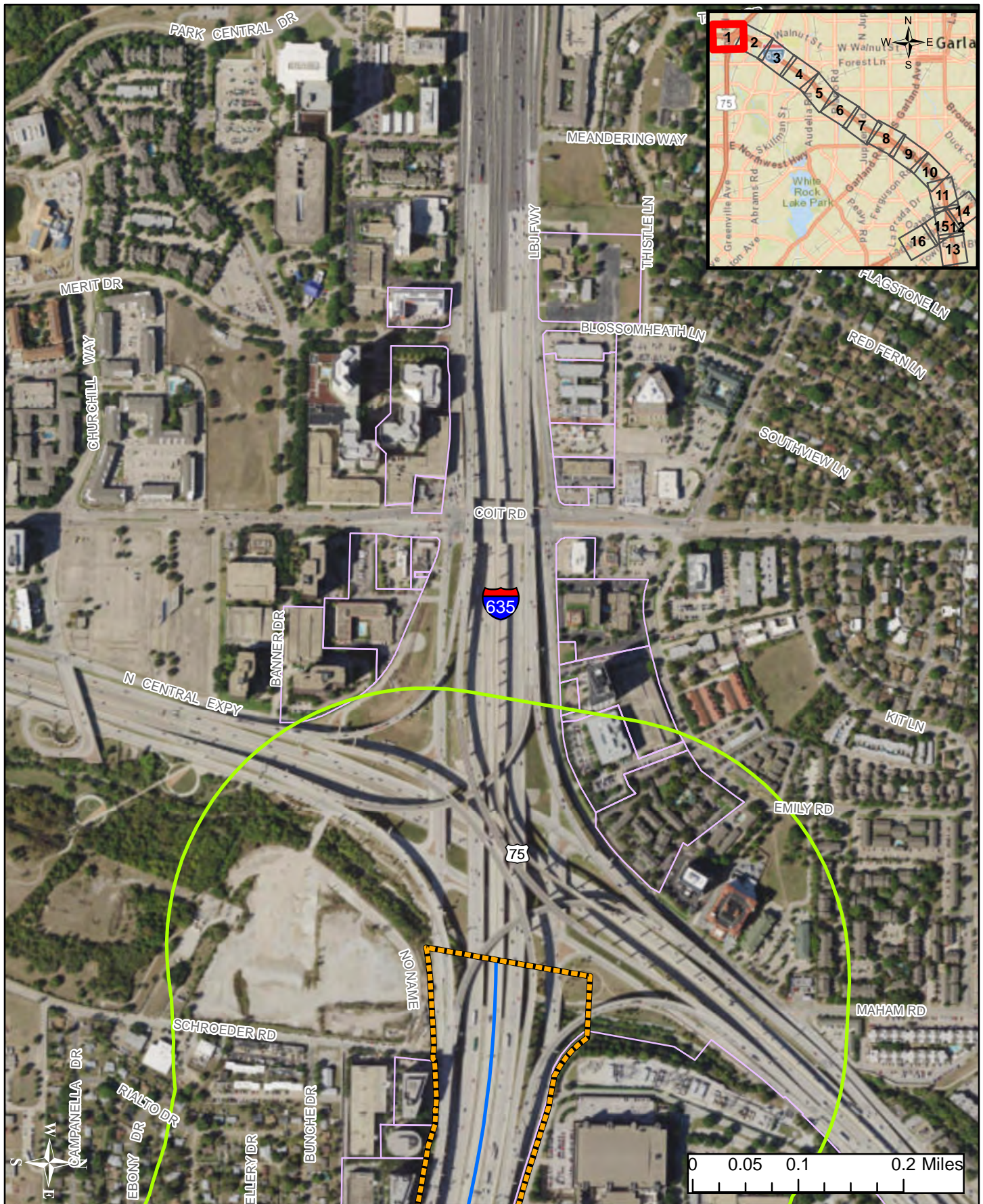
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PROJECT TYPICAL SECTIONS
PAGE 8 OF 8

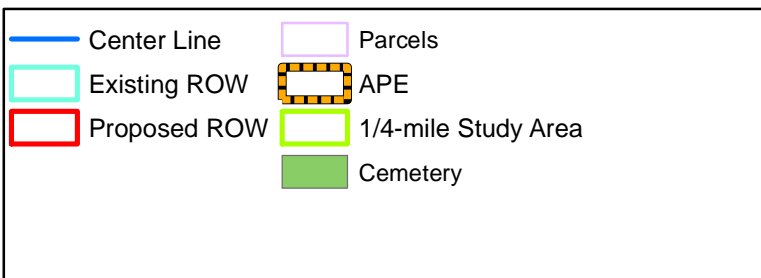
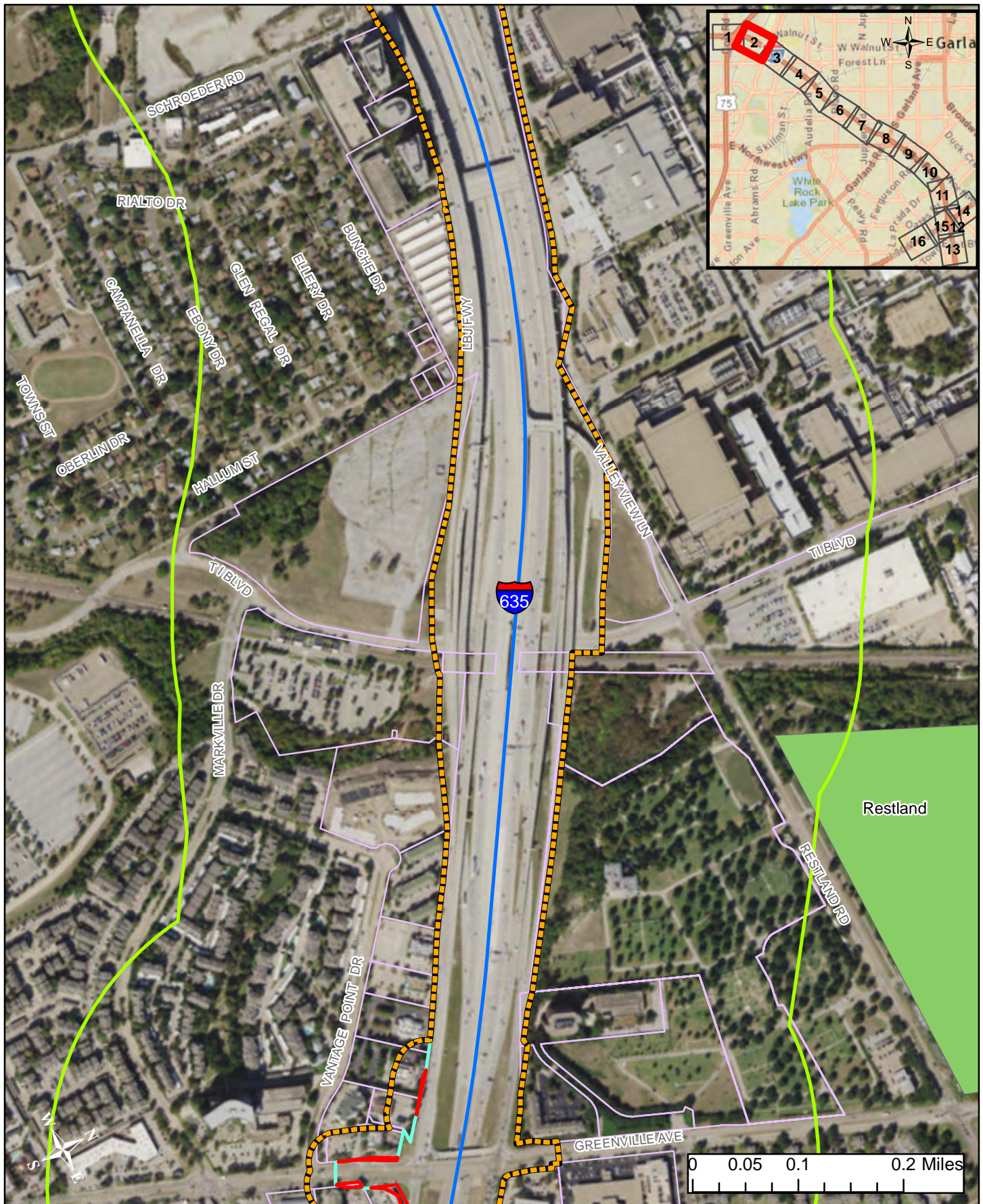
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CITIES OF DALLAS, GARLAND
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Appendix B – Project Maps



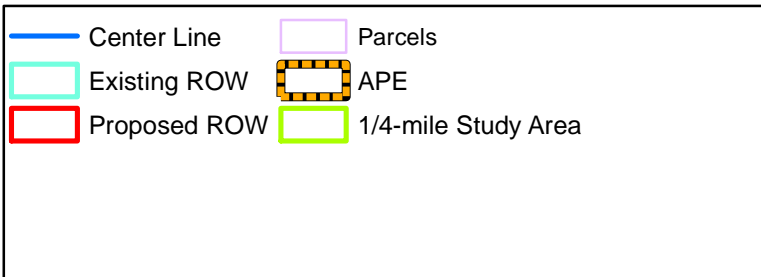
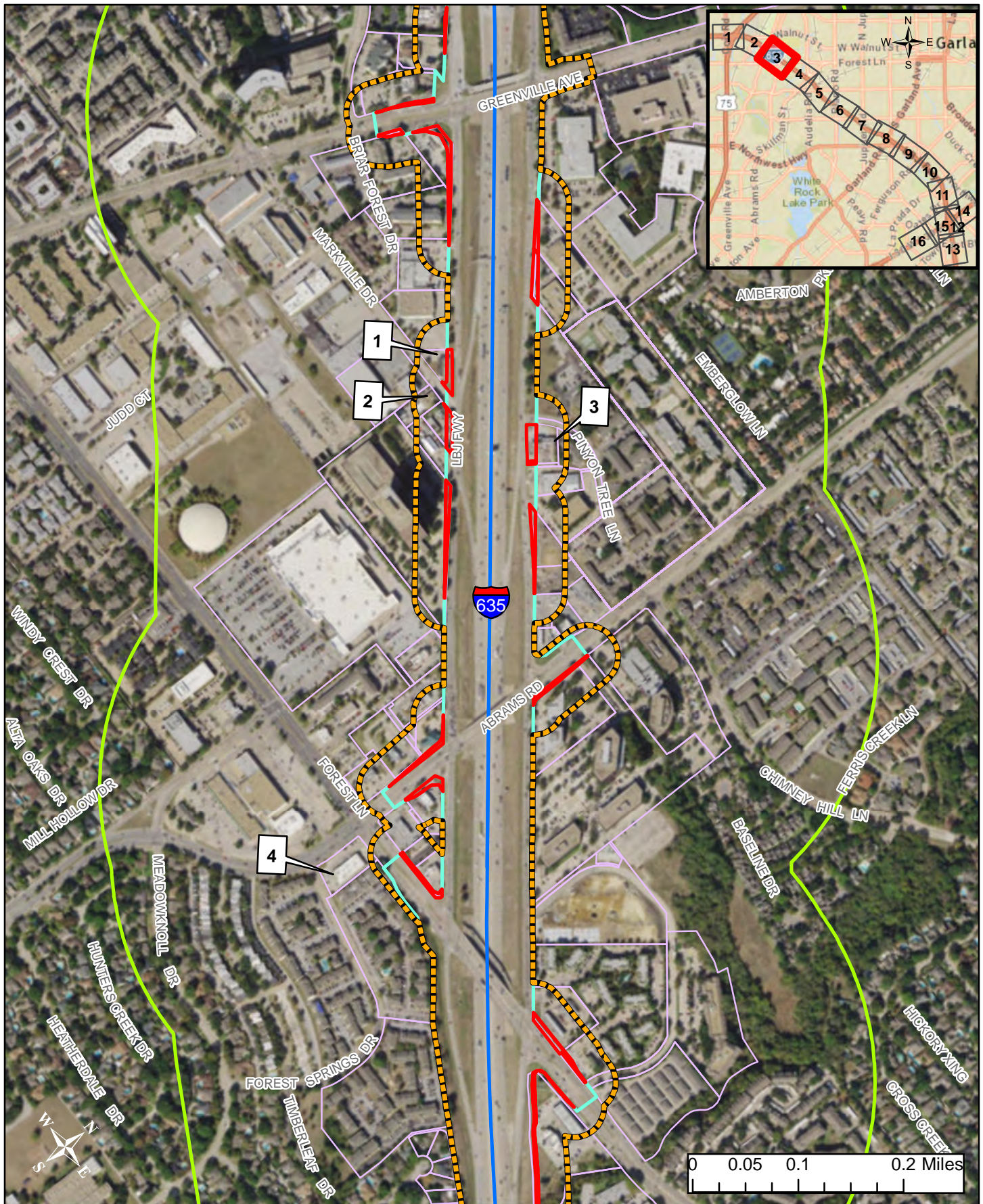
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APE on Aerial Photograph**

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
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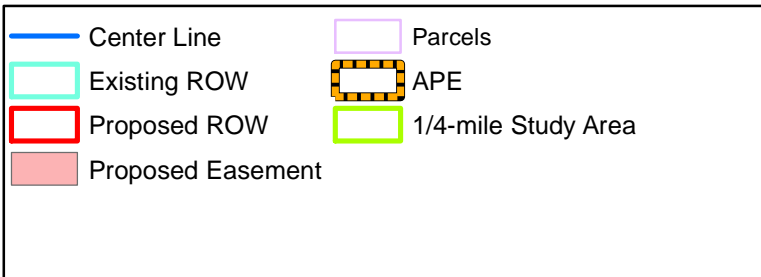
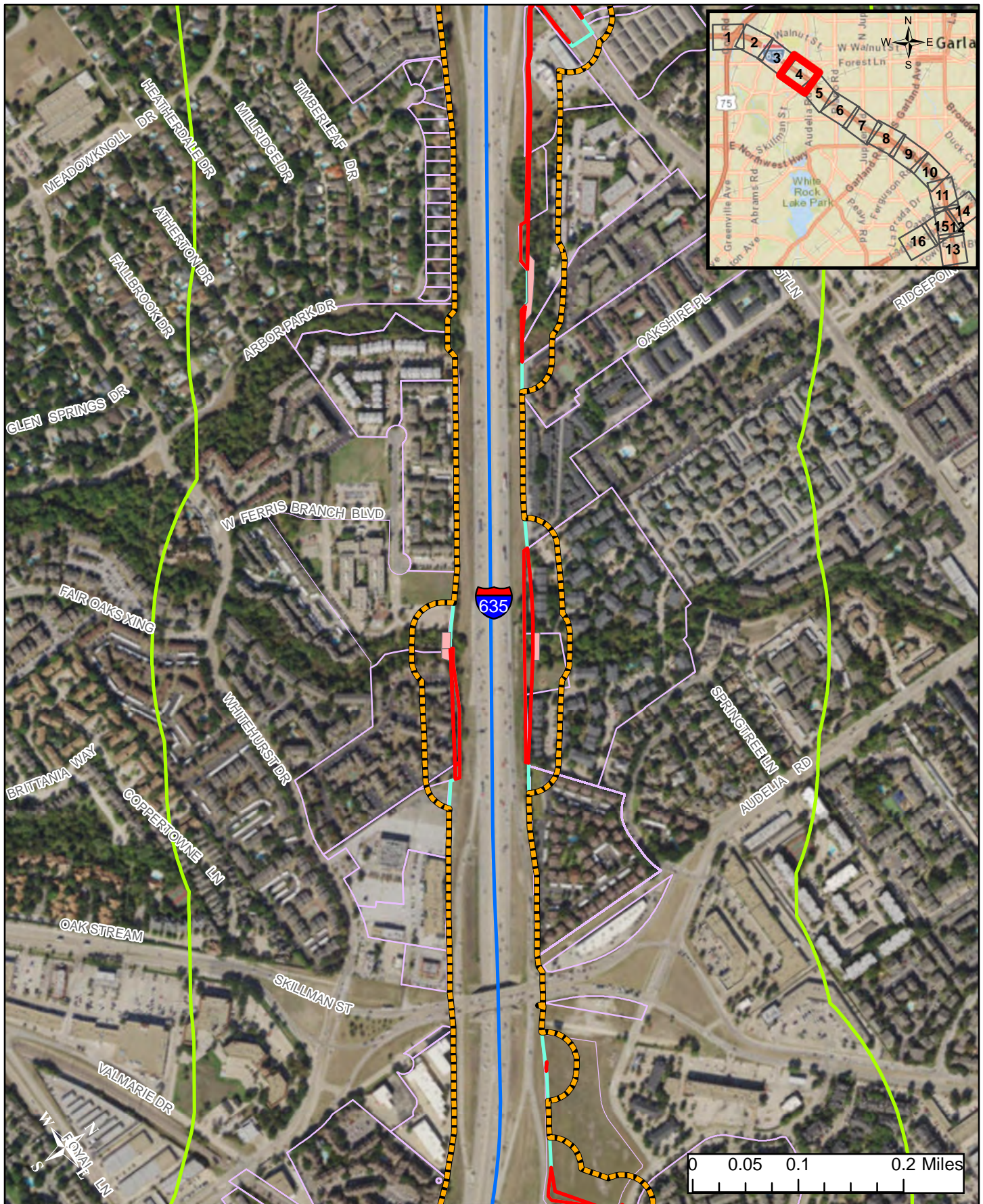


**Historic Properties and
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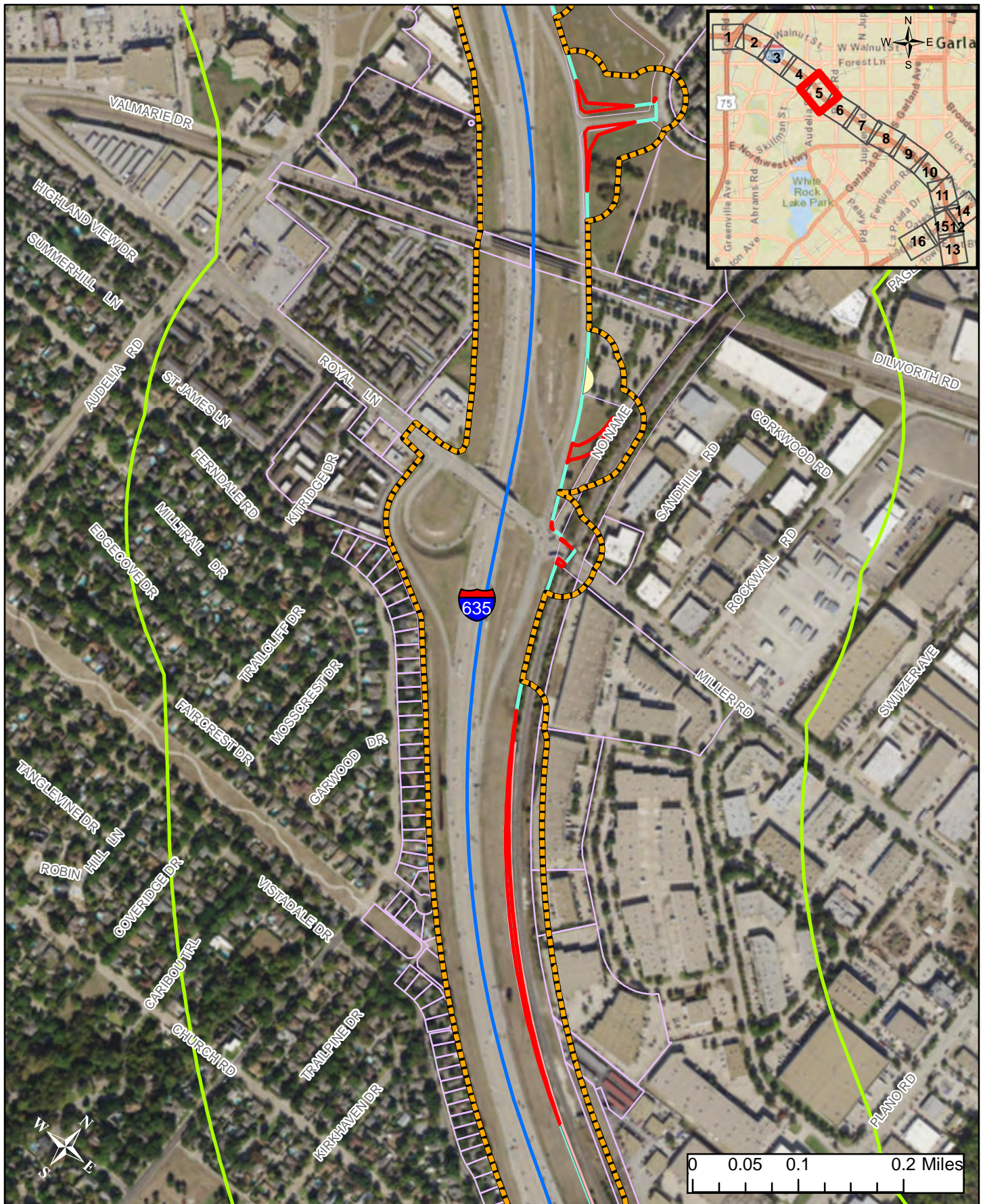
IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
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Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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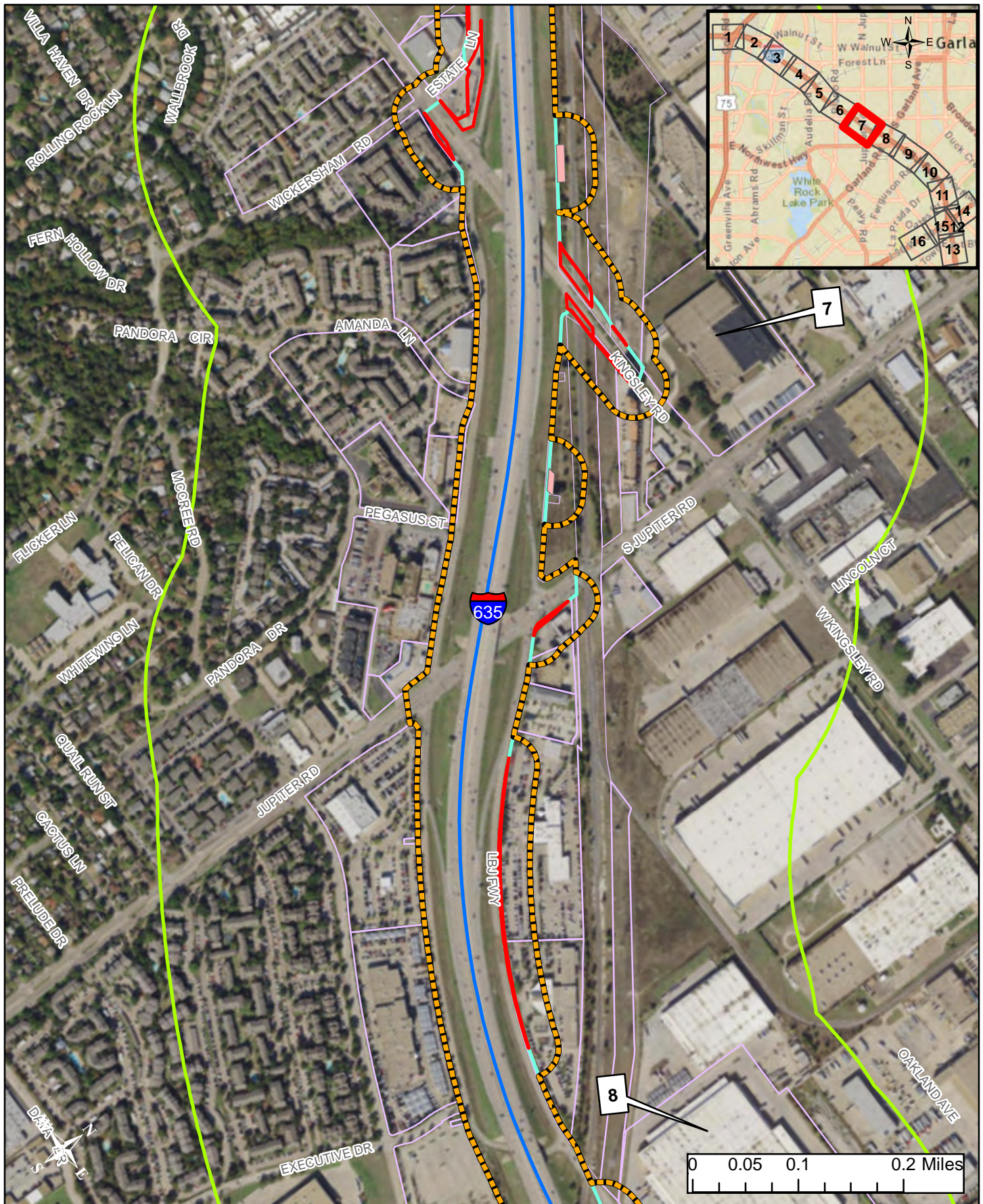


Historic Properties and APE on Aerial Photograph
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 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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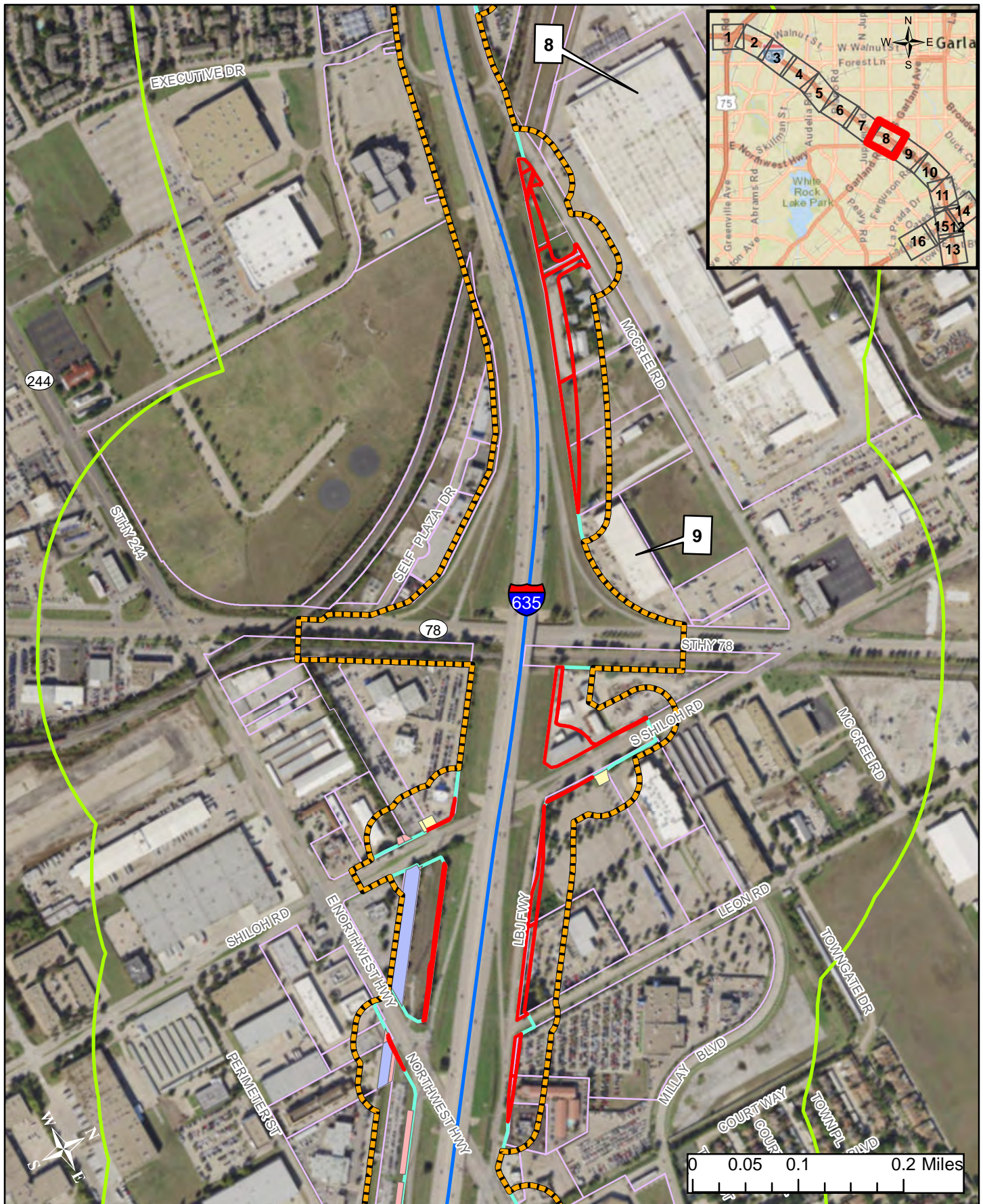
Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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| | | | |
|--|-------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Proposed ROW | | 1/4-mile Study Area |
| | Proposed Easement | | |

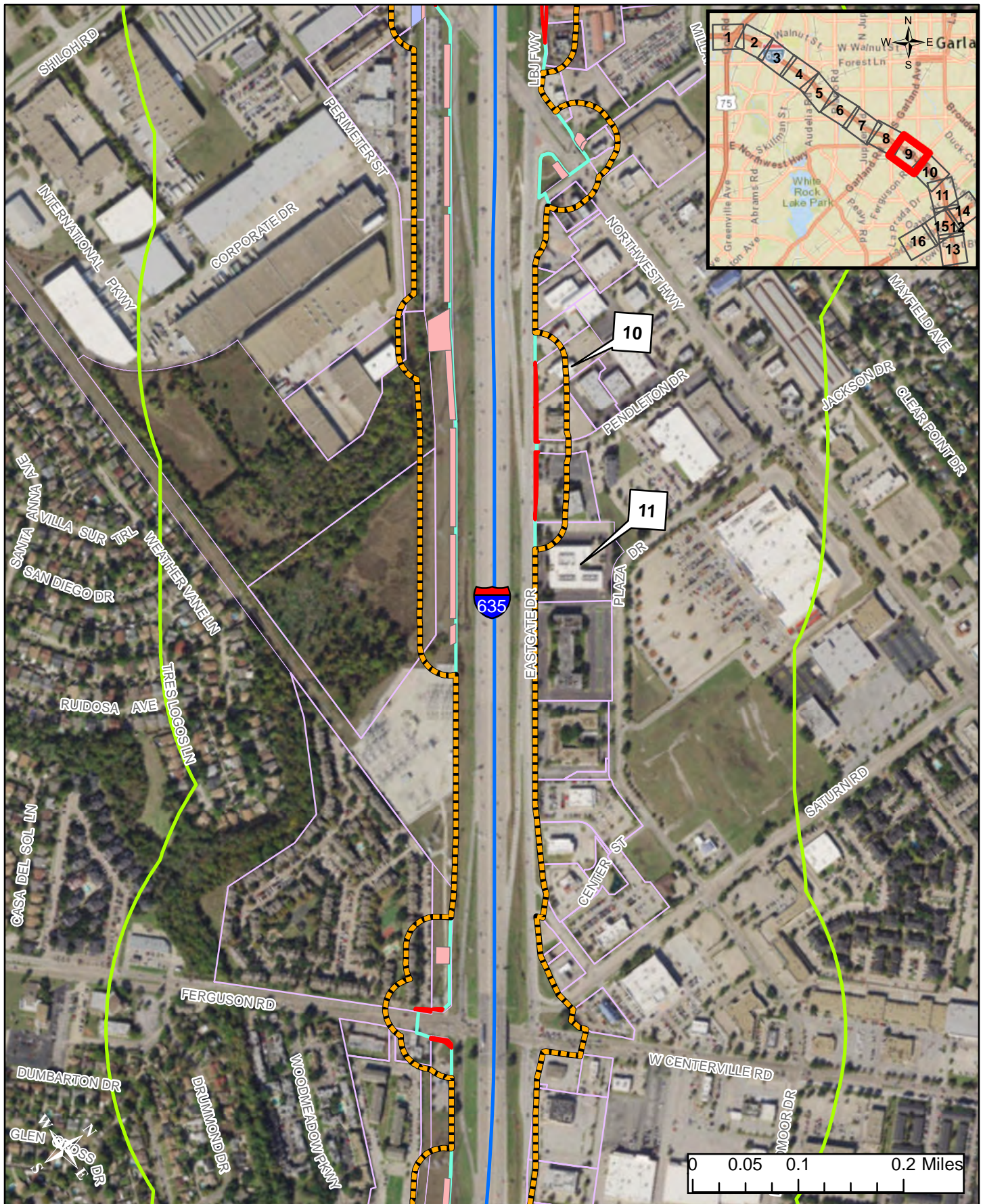
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 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
 CSJs: 2374-01-137, 2374-01-180, 2374-01-183, 2374-02-053 & 2374-02-144



| | | | |
|--|--------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Existing Easement | | 1/4-mile Study Area |
| | Proposed ROW | | |
| | Temporary Easement | | |
| | Proposed Easement | | |

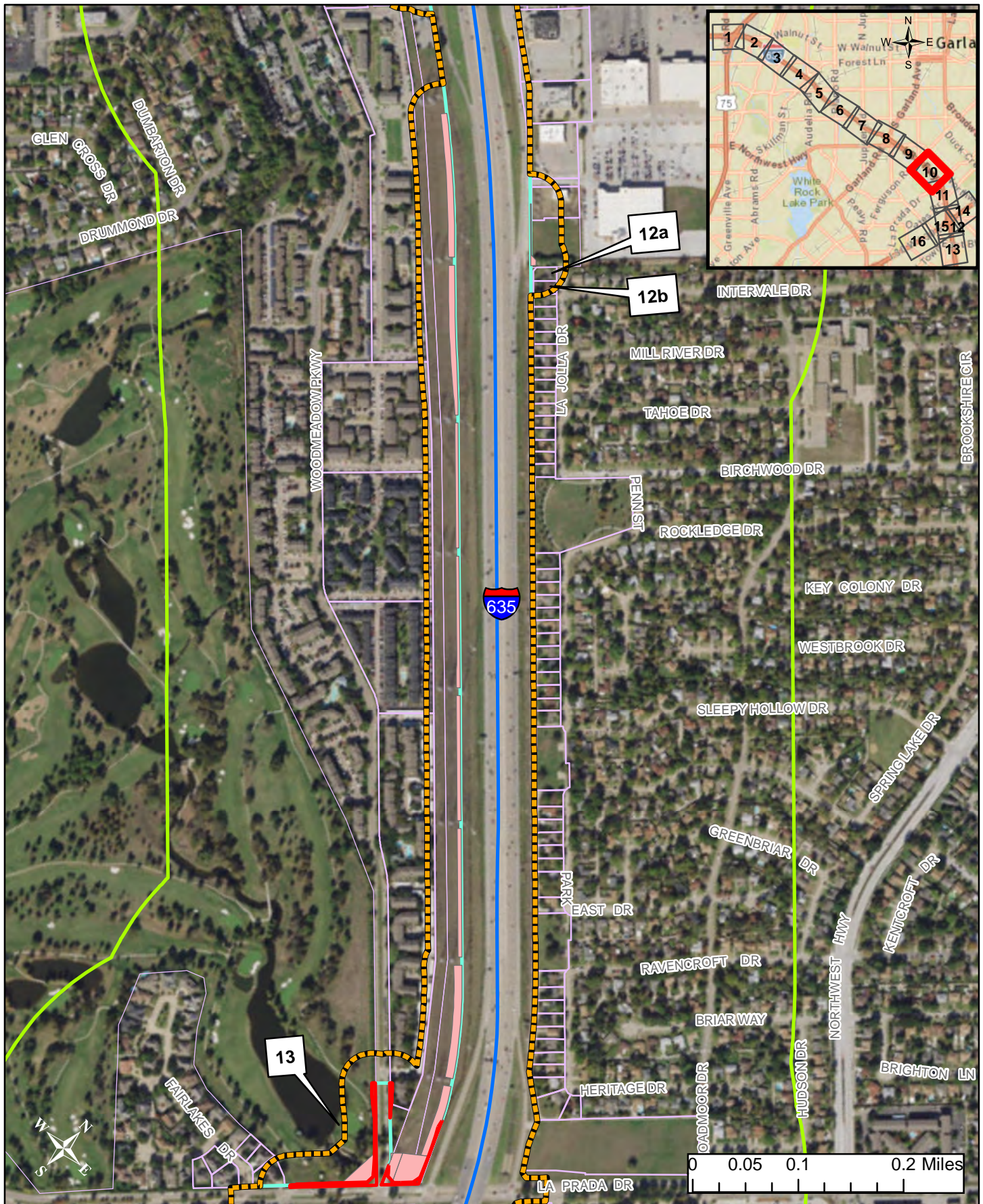
Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and
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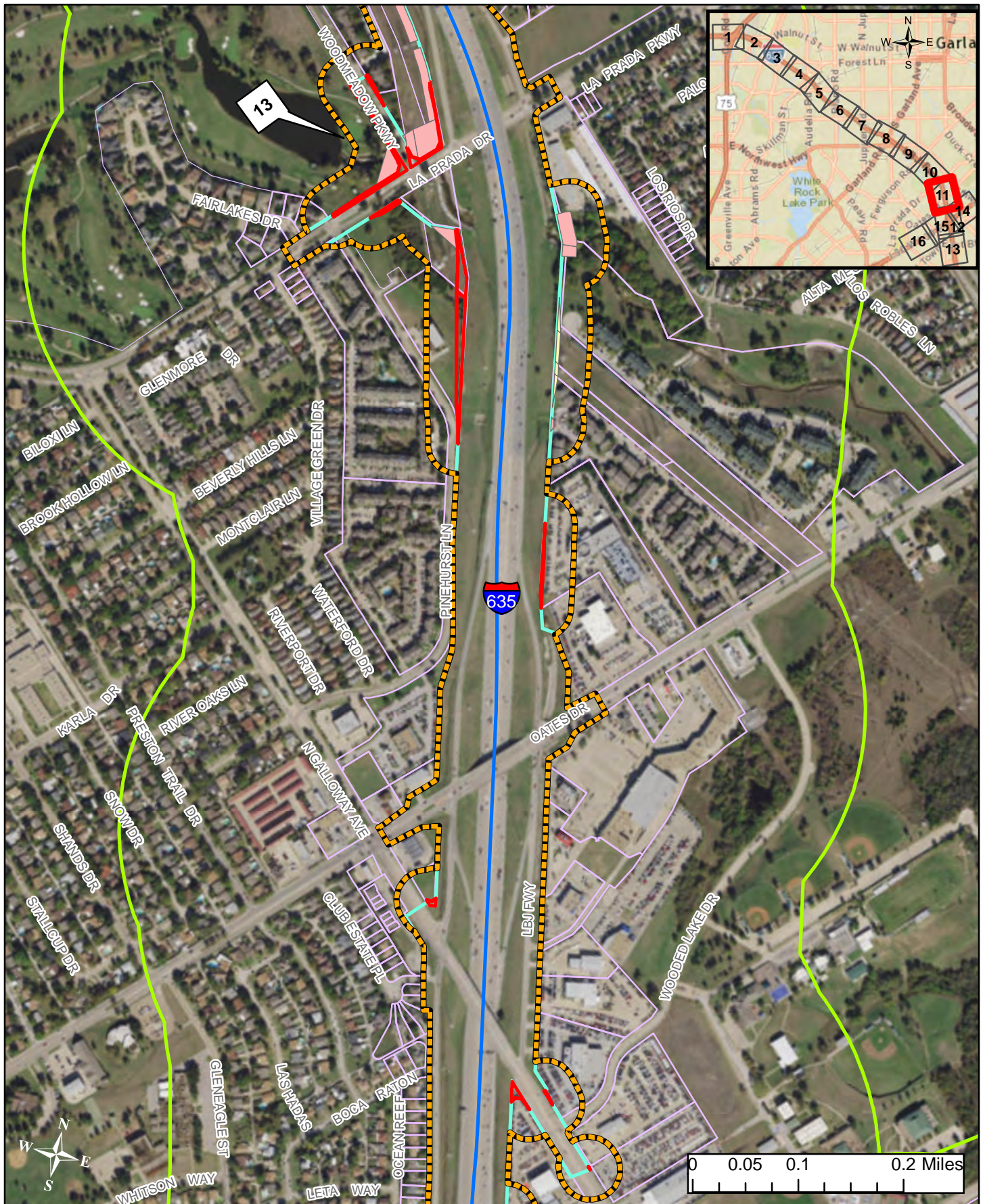
Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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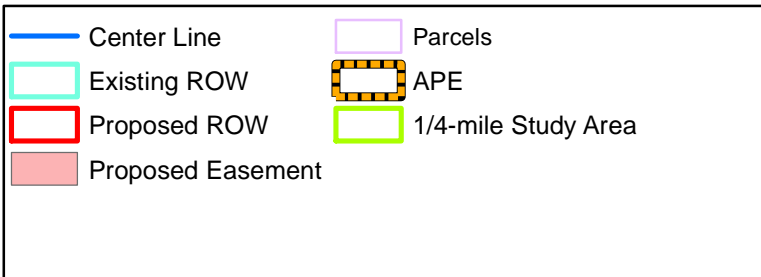
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| | Center Line | | Parcels |
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| | Proposed ROW | | 1/4-mile Study Area |
| | Temporary Easement | | |
| | Proposed Easement | | |

Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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| | | | |
|--|--------------------|--|---------------------|
| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Proposed ROW | | 1/4-mile Study Area |
| | Temporary Easement | | |
| | Proposed Easement | | |

Historic Properties and APE on Aerial Photograph
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 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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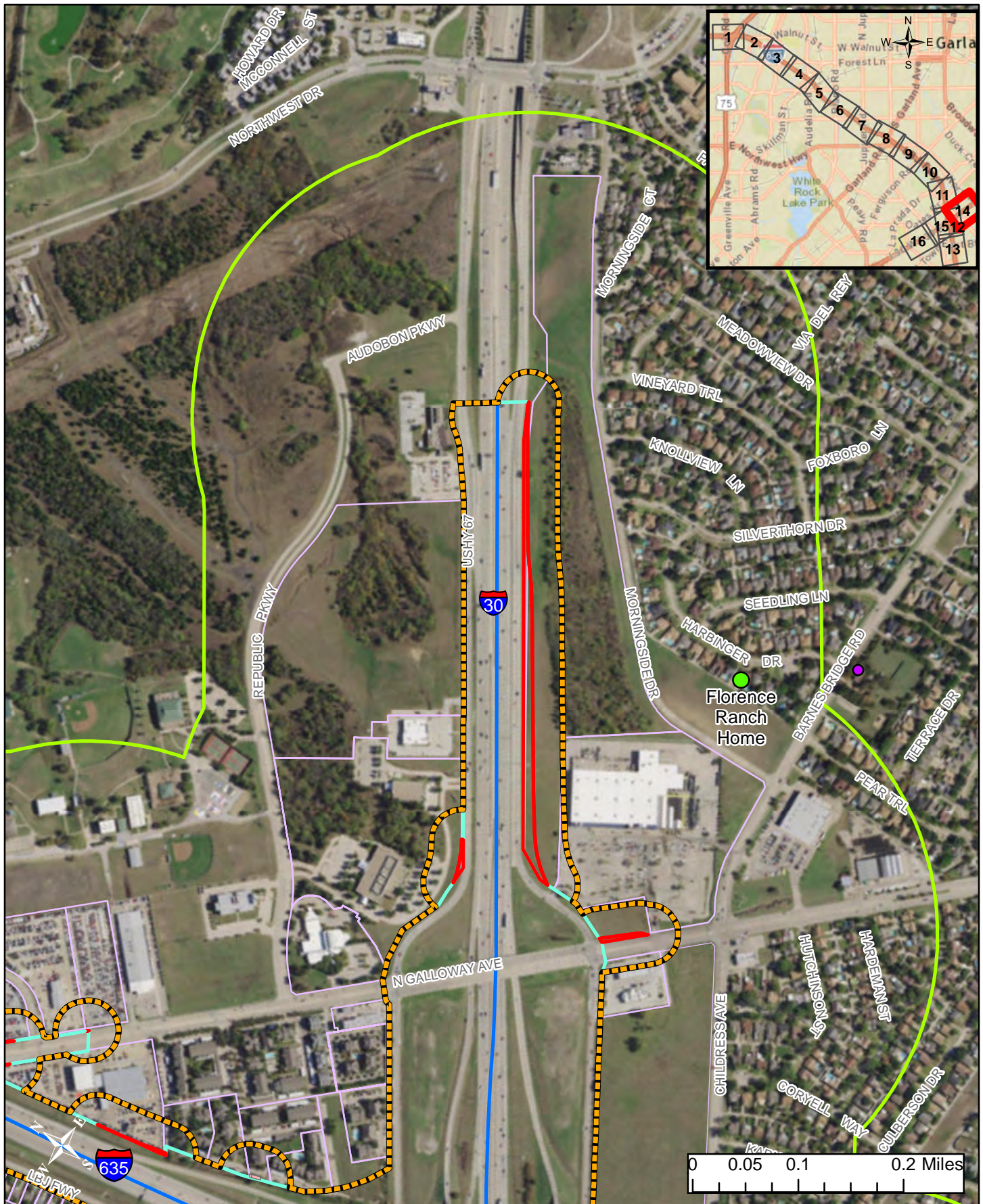
Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
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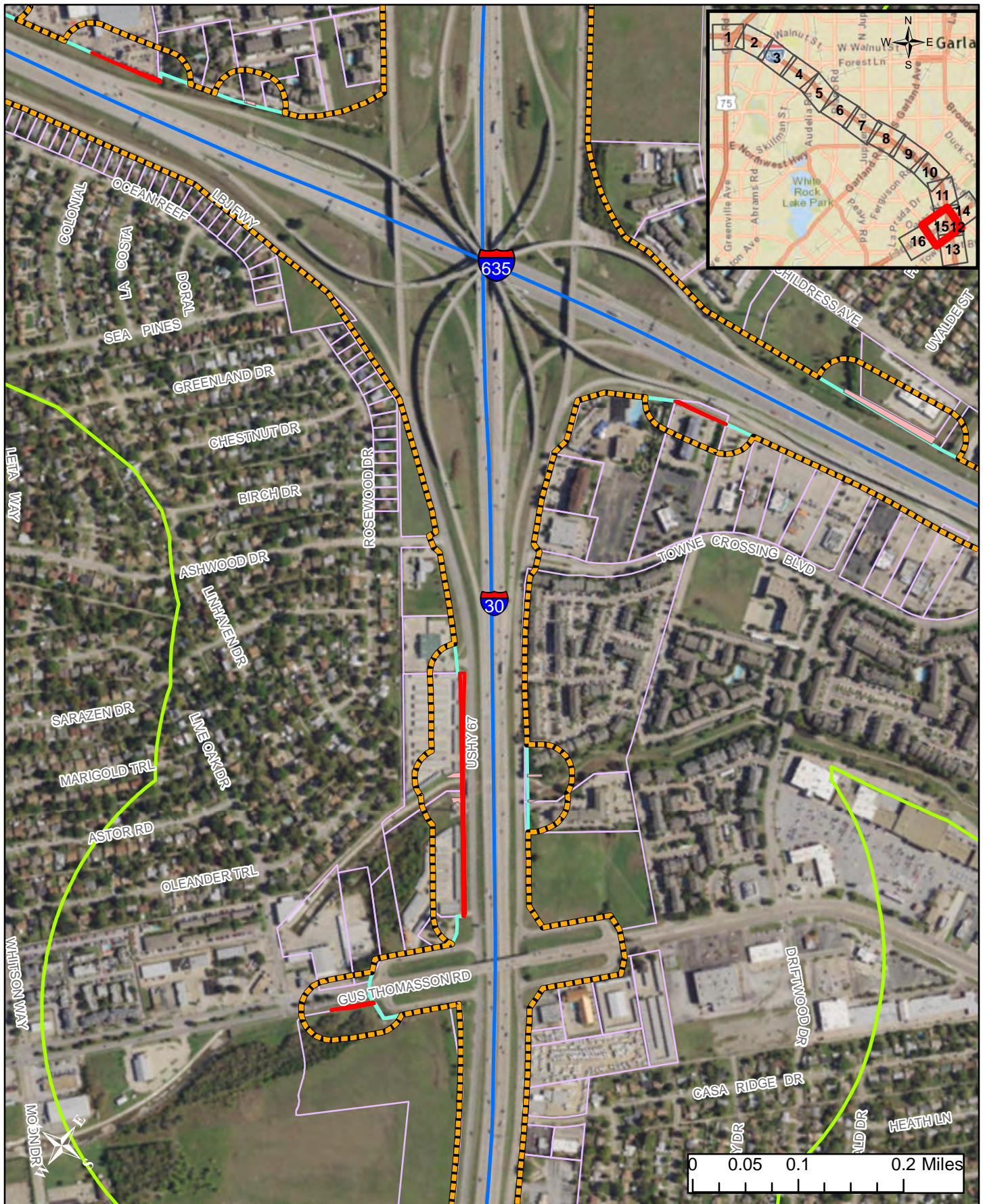


Historic Properties and APE on Aerial Photograph

IH 635 from US 75 to IH 30
 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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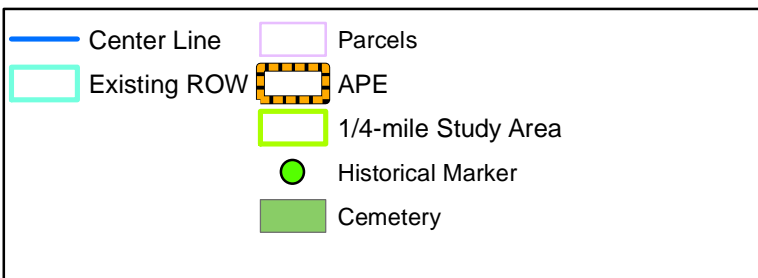
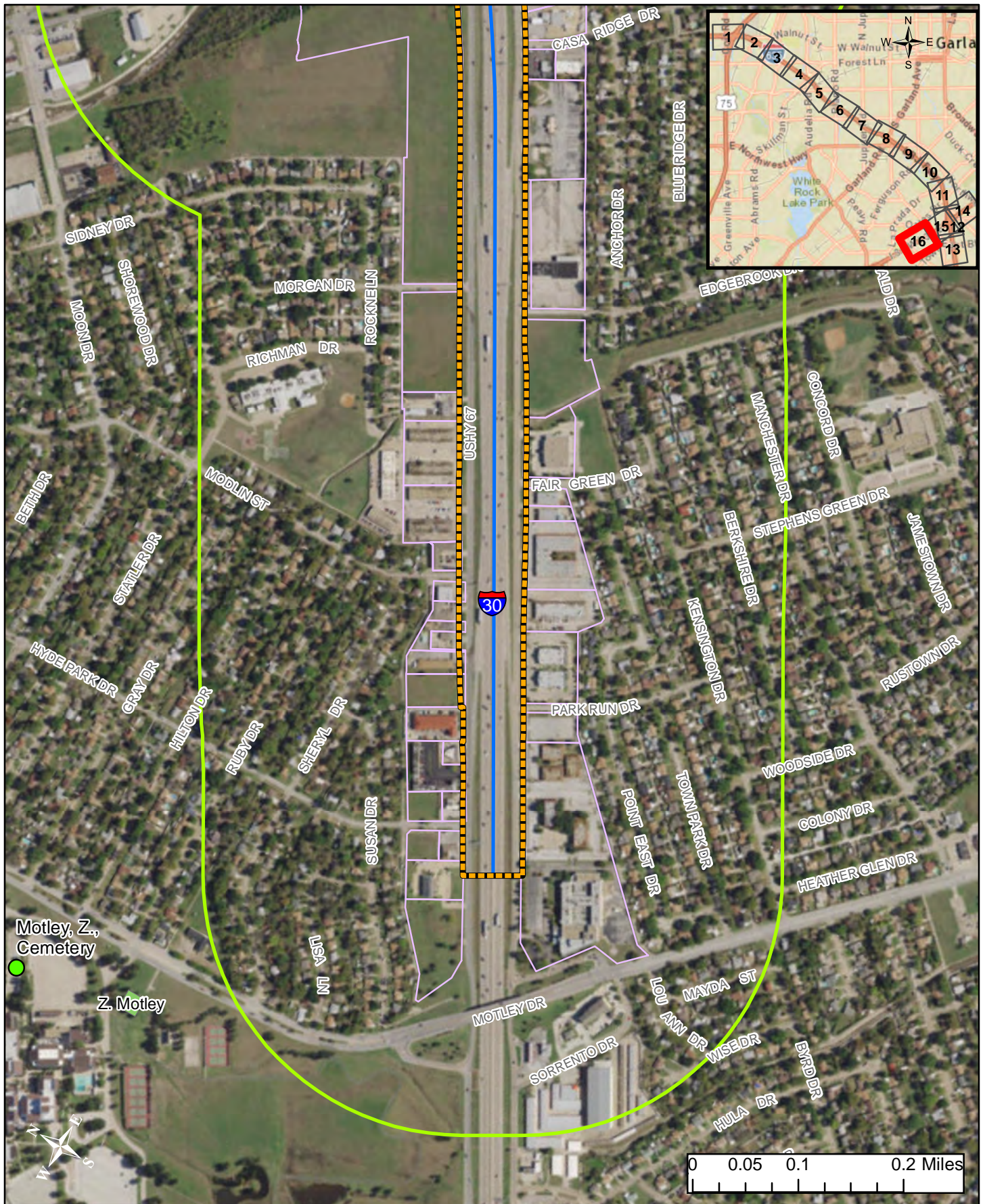


Historic Properties and APE on Aerial Photograph
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 Cities of Dallas, Garland and Mesquite, Dallas County, Texas
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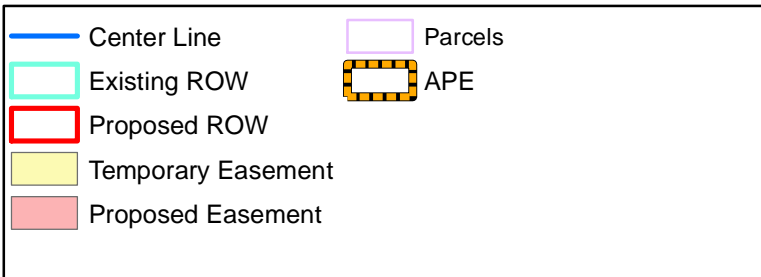
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| | Center Line | | Parcels |
| | Existing ROW | | APE |
| | Proposed ROW | | 1/4-mile Study Area |
| | Proposed Easement | | |

Historic Properties and APE on Aerial Photograph
 IH 635 from US 75 to IH 30
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**Historic Properties and
APE on Aerial Photograph**

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 2374-01-183, 2374-02-053 & 2374-02-144



New World Neighborhood Boundary Map

IH 635 from US 75 to IH 30
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